

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:

Indiana State Road (SR) 65, Gibson

Designation Number(s):

Des. No. 2001922 (Lead Project; Pavement Replacement) & Des. No. 2400688 (Sub Project; Sidewalk and ADA Sidewalk Ramp Construction)

Project Description/Termini:

Pavement Replacement Project at SR 65, 0.26 mile south of SR 168 (South Limits Owensville).

X	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

INDOT DE Signature and Date

INDOT ESD Signature and Date

FHWA Signature and Date

Release for Public Involvement

INDOT DE Initials and Date

INDOT ESD Initials and Date

RF 3/5/2026

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Chad Kelly, Kaskaskia Engineering Group, LLC

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 &
2400688

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*? If No, then:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Survey letters were mailed to potentially affected property owners February 14, 2022, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Survey letter is included in Appendix G, page 1.

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* which requires the project sponsor offer the public an opportunity to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation (INDOT) INDOT District: Vincennes

Local Name of the Facility: SR 65

Funding Source (mark all that apply): Federal State Local Other*

*If other is selected, please identify the funding source: _____

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

INDOT and the Federal Highway Administration (FHWA) intend to proceed with a pavement replacement project in Gibson County, Indiana. The project is located on SR 65, 0.26 mile south of SR 168 (South Limits Owensville). in Section 12, Township 3 South, Range 12 West, Montgomery Township, Gibson County, Indiana (Appendix B, page 1). Per the September 29, 2022, Engineering Assessment Report (EAR), (Appendix I, pages 1 to 18), the need for this project is due to the deterioration of the existing pavement and drainage deficiencies along a section of SR 65. The EAR notes pavement distress, such as signs of asphalt surface aging and road base failures, including longitudinal, block, and alligator cracking. The existing storm sewer within the project area is in poor condition and is attributing to drainage failures along the project area.

This is page 2 of 23 Project name: SR 65, Pavement Replacement Project Date: February 19, 2026

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 &
2400688

Safety and utility issues have also been identified within the study area. The 90-degree curve at Mill St does not meet current INDOT standards for lane encroachment and design speed. The 90-degree curve at Mill Street and SR 65 is currently deficient as it pertains to lane encroachment and design speed. The existing curve has only a 60-foot radius, which is significantly smaller than the 107-foot recommended minimum radius for a 20-mph road with no superelevation. There is evidence of vehicles, especially large trucks, encroaching both across the centerline and outside of the roadway shoulder. This curve is signed with a speed of 20-mph; however, based on field visits, it appears many vehicles do not slow down to the warning speed before negotiating the curve. The intersection geometry at SR 168 and SR 65, in downtown Owensville, does not meet the needs of large vehicle traffic. Large vehicles have difficulty navigating the small turn radius, which may cause significant damage to the existing curb when turning northbound onto SR 65 or eastbound from SR 65. The lack of parking areas along SR 65, between Warrick Street and SR 168, has forced residents and the public to park on the grass buffer between the roadway and sidewalk within the existing right-of-way. The sidewalks are discontinuous, do not have transitions at the commercial or residential drives, and do not meet current American's with Disabilities Act (ADA) standards. The east-west crossing at the intersection of SR 65 and SR 168 is also unsignalized at the intersection of two highways in the town center.

The poor condition of the combined open and closed storm drainage system has caused failures along the edges of the roadway. The storm sewer is in such poor condition it has completely failed in places such that connections between structures could not be verified.

The purpose of the project is to improve the pavement and drainage deficiencies, realign Mill Street to meet current INDOT safety standards for the 20-mph speed limit posting, provide increased mobility for both automobile and pedestrian traffic at the SR 65 and SR 168 intersection, correct pedestrian facility deficiencies while bringing sidewalk crossings to ADA and INDOT safety standards, and provide safe public and residential parking.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Gibson

Municipality: Owensville

Limits of Proposed Work: SR 65, approximately 550 feet west of Mill Street to the intersection of SR 65 and SR 165.

Total Work Length: 0.47 Mile(s)

Total Work Area: 8.66 Acre(s)

Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: <input style="width: 100%;" type="text"/>	

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

The INDOT and the Federal Highway Administration (FHWA) intend to proceed with this pavement replacement project.

The project is located on SR 65, 0.26 mile south of SR 168 (South Limits Owensville) in Section 12, Township 3 South, Range 12 West, Montgomery Township, Gibson County, Indiana (Appendix B, page 1).

SR 65 is a two-lane rural and urban Major Collector comprised of two 11-foot lanes (one in each direction) with 1-foot paved shoulders. It is neither part of the National Highway System nor the National Truck Network. SR 65 at this location goes through downtown Owensville and a rural suburban area consisting of residential dwellings, commercial properties, and recreational facilities.

The existing sidewalks are five feet in width, are discontinuous, and are offset eight to 10 feet from the roadway.

The existing drainage system is comprised of a combined open ditch and closed system with concrete curbs at the urban intersections and open ditches in the rural section. Paving done by property owners, between Warrick St and SR 168, has increased the impervious area since the storm water system was built. Approximately 14 acres drain to the system within the project limits before reaching Black River. Specific details regarding the locations of open ditches, curb and gutters, and drainage for various sections can be found in the EAR (Appendix I, pages 1 to 18).

The preferred alternative will include a full depth pavement replacement on SR 65, from approximately 400 feet west of Mill Street to

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

the intersection of SR 165, then mill and pave 1.5 inches of pavement until the ending project limits at the intersection of SR 165 (West Brummitt Street). In addition to the full depth pavement replacement, the project will also include new curb and gutters, new sidewalks ADA compliant curb ramps (covered under DES 2400688), and an enclosed storm drainage system. A 10-foot-wide parking lane will be added to both sides of SR 65 from Warrick Street to SR 168 with a curb and gutter separating the parking lane from the sidewalk. Due to the insufficient existing curve radius on Mill Street, the project will increase the Mill Street curve to INDOT design and safety standards to adequately adjust for speeds of 20 MPH and roadway geometrics at Mill Street and SR 65. The project will also adjust the roadway curve geometry at SR 168 and SR 65 to a 30-foot turning radius with 10:1 tapers and a two-foot offset. A new, fully enclosed, storm water sewer system will be built on SR 65, including replacing the trunkline and adding more inlets, to adequately contain and transport storm water. A detention pond will be constructed near the project's southern terminus on the west side of SR 65, to collect the outflow of the storm water sewer system before the water is conveyed to Black Creek, via roadside ditch, which may improve the water storage capacity of the system. The project requires acquiring approximately 1.26 acres of permanent right-of-way (ROW), and approximately 0.23 acre of temporary ROW. Anticipated impacts include removing approximately 1.1 acres of trees. Every effort to avoid, minimize, and/or mitigate project impacts will be made. Preliminary project plans are included in Appendix B, pages 31 to 63.

The proposed maintenance of traffic (MOT) plan utilizes temporary road closures with detours and flagging operations used during asphalt milling work. The pedestrian MOT plan will include phased sidewalk closures (Appendix B, pages 36 to 46). The MOT for the project is discussed in further detail in the MOT During Construction section of this document and is included in Appendix B, pages 36 to 50.

This alternative addresses the identified deficiencies in the pavement, drainage, pedestrian facilities, parking, turn and intersection geometry. It provides long-term solutions which will ensure the efficient operation of the roadway, meeting both the purpose and need.

The construction limits on SR 65 extend from approximately 400 feet west of Mill Street, then north to the intersection of SR 165, which are the logical termini for the project since these are the rational end points of the transportation improvement and subsequent review of its environmental impacts. This project demonstrates independent utility because it will improve the roadway as an independent project and does not depend on any other planned projects.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

No Build alternative: The no build alternative does not address the pavement, drainage, pedestrian facilities, parking, or intersection concerns of the roadway. If no action is taken, the deterioration of the roadway will persist. This would not address the purpose and need. Therefore, this alternative was discarded.

Mill Street Curve alternative: This alternative proposes a 25 MPH design speed along the Mill Street curve. This speed reduction would provide a greater level of comfort around the curve for travelers; however, this alternative would require significantly more ROW to account for its superelevation transition. This alternative would increase cost and increase environmental impacts. For these reasons, this alternative was discarded.

SR 168 and SR 65 Intersection alternative: This alternative proposes a 10-foot parking lane/shoulder to the north and south of intersection on SR 65. This alternative decreases lane encroachment for turning vehicles. This alternative has increased impacts to adjoining properties, including increased environmental impacts, as well as cost. For these reasons, this alternative was discarded.

On-Street Parking Improvement alternatives: This alternative includes two scenarios of improving existing on-street parking. (1) Existing residential driveway approaches would be reconstructed to improve vehicular access to the subject properties. Additional ROW would be required, increasing the overall cost of the project and environmental impacts. For these reasons, this alternative was discarded. (2) A 10-foot parking lane on the west side of SR 65 on the block from Warrick Street to SR 168 was proposed, with a curb and gutter separating the parking lane from the sidewalk behind. This alternative would provide parking where needed; however, this alternative requires additional ROW increasing overall cost and environmental impacts. For these reasons, this alternative was discarded.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or

X
X

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

It would result in serious impacts to the motoring public and general welfare of the economy.
Other (Describe):

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway: SR 65
 Functional Classification: Rural and Urban Major Collector
 Current ADT: 2,242 (2025) Design Year ADT: 3,139 (2045)
 Design Hour Volume (DHV): 404 Truck Percentage (%): 10.6
 Designed Speed (mph): 25 to 35 Legal Speed (mph): 25 to 35

	Existing	Proposed
Number of Lanes:	2	2
Type of Lanes:	2 through lanes	
Pavement Width:	11 ft.	11 ft.
Shoulder Width:	1 ft.	1 ft.
Median Width:	N/A ft.	N/A ft.
Sidewalk Width:	N/A ft.	N/A ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): N/A Sufficiency Rating: N/A
 (Rating, Source of Information)

	Existing	Proposed
Bridge/Structure Type:		
Number of Spans:		
Weight Restrictions:	ton	ton
Height Restrictions:	ft.	ft.
Curb to Curb Width:	ft.	ft.
Outside to Outside Width:	ft.	ft.
Shoulder Width:	ft.	ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

No bridges or small structures are located within the project area.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

Is a temporary bridge proposed?
 Is a temporary roadway proposed?
 Will the project involve the use of a detour or require a ramp closure? (describe below)
 Provisions will be made for access by local traffic and so posted.
 Provisions will be made for through-traffic dependent businesses.
 Provisions will be made to accommodate any local special events or festivals.

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

Will the proposed MOT substantially change the environmental consequences of the action?

Is there substantial controversy associated with the proposed method for MOT?

Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)

Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).

Discuss closures and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Any local concerns about access and traffic flow should be detailed as well.

The MOT for the project will require a full closure of SR 65 with a detour, occurring when school is not in session. The official state detour route includes SR 68, US 41, to SR 64. The official detour is approximately 37 miles. Pedestrian MOT is planned for the project and will occur in three phases. Pedestrian MOT will include phase sidewalk closures along SR 65 with associated detours. Flagging operation will be utilized during milling operations, application of the HMA layer, and pavement marking application associated with the project. The detours will remain for the duration of construction for the project, approximately up to two construction seasons, or up to two years. MOT plans are included in Appendix B, pages 36 to 50.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 3,800,000 (FY TBD) Right-of-Way: \$ 342,000 (FY TBD) Construction: \$ 3,427,000 (FY TBD)

Anticipated Start Date of Construction: November 2026

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.07	0.11
Municipal	0.09	0.05
Agricultural	0.39	0.07
Forest	0.71	0.0
Wetlands	0.0	0.0
Other:		
TOTAL	1.26	0.23

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing ROW is approximately 25 feet west of centerline on SR 65 and approximately 30 feet east of centerline on SR 65 and consists of mowed grasses associated with adjoining residential and commercial properties, typical of being adjacent to an urban roadway. Areas of proposed ROW include residential acreage at the northwest and northeast quadrants of the intersection, agricultural acreage at the southwest and southeast quadrants of the intersection, and forested acreage at the southwest quadrant of the intersection.

The project requires approximately 1.26 acres of permanent ROW consisting of residential, agricultural, municipal, and forested land along the project area corridor. The project will also require 0.23 acre of temporary ROW consisting of residential, municipal, and agricultural use. Proposed ROW widths vary along US 36 to South Mill Street, ranging from the current edge of pavement to 25 feet from the edge of pavement. Proposed widths along South Mill Street to the southern project termini, ranging from 14 feet to 185 feet from edge of pavement. ROW locations are denoted in the ROW plans in Appendix B, pages 64 to 83.

If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 &
2400688

INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on January 30, 2024 (Appendix C, pages 1 to 2).

Agency	Date Sent	Response Date	Appendix
Federal Highway Administration (FHWA)	January 30, 2024	No response received	N/A
Indiana Geological and Water Survey (IGWS) (Automated Response)	January 30, 2024	January 30, 2024	Appendix C, pages 3 to 4
U.S. Department of Housing and Urban Development (HUD)	January 30, 2024	No response received	N/A
Natural Resources Conservation Service (NRCS)	January 30, 2024	February 10, 2024	Appendix C, page 5
Indiana Department of Environmental Management (IDEM) – Wetlands and Stormwater Programs	January 30, 2024	No response received	N/A
IDEM - Groundwater Section	January 30, 2024	February 1, 2024	Appendix C, pages 6 to 7
Indiana Department of Natural Resources, Division of Fish and Wildlife (IDNR-DFW)	January 30, 2024	February 29, 2024	Appendix C, pages 8 to 9
National Park Service (NPS)	January 30, 2024	No response received	N/A
INDOT Vincennes Environmental Section Manager (Supervisor)	January 30, 2024	January 30, 2024	Appendix C, page 10
INDOT Project Manager	January 30, 2024	No response received	N/A
U.S. Army Corps of Engineers (USACE)	January 30, 2024	No response received	N/A
Evansville Metropolitan Planning Organization (EMPO) – Executive Director	January 30, 2024	No response received	N/A
Gibson County Sheriff	January 30, 2024	No response received	N/A
Gibson county Surveyor's Office	January 30, 2024	No response received	N/A
Gibson County Highway Department – Superintendent	January 30, 2024	No response received	N/A
Gibson County Floodplain Administrator/Planning and Zoning	January 30, 2024	No response received	N/A
Gibson County Council - President	January 30, 2024	No response received	N/A
Gibson County EMA – Director	January 30, 2024	No response received	N/A
Owensville Police Department – Town Marshal	January 30, 2024	No response received	N/A
Owensville Montgomery Township Fire Protection District – Chief	January 30, 2024	No response received	N/A
Town of Owensville – Town Council	January 30, 2024	No response received	N/A
Town of Owensville – Water/Sewer Superintendent	January 30, 2024	No response received	N/A
Owensville REH Center	January 30, 2024	No response received	N/A

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

Town of Owensville – Street Supervisor	January 30, 2024	No response received	N/A
General Baptist Church	January 30, 2024	No response received	N/A
South Gibson School Corporation – Superintendent	January 30, 2024	No response received	N/A

All applicable recommendations are included in the Environmental Commitments section of this CE document.

SECTION B – ECOLOGICAL RESOURCES:

Streams, Rivers, Watercourses & Other Jurisdictional Features

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

Presence

X

Impacts

Yes	No
	X

Total stream(s) in project area: 356.67 Linear feet Total impacted stream(s): 0 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
Unnamed Tributary (UNT) to Black River	Intermittent	356.67	0	Located west portion of proposed detention area flowing north to south, likely Water of the US (Appendix F)

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area, and the Red Flag Investigation (RFI) report (Appendix E), there are 12 streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There is one stream, river, watercourse, or other jurisdictional feature within or adjacent to the project area. That number was confirmed by the site visit on April 20, 2024, by Kaskaskia Engineering Group, LLC (KEG).

No Federal, Wild and Scenic Rivers; State Natural, Scenic, and Recreational Rivers; Outstanding Rivers for Indiana; navigable waterways or National Rivers Inventory waterway are present within or adjacent to the project area.

Due to project design changes, an additional field visit was conducted on April 20, 2024, and a Waters of the U.S. Delineation Report was approved by INDOT EWPSO May 10, 2024 (Appendix F, page 1). It was determined that one jurisdictional stream, UNT to Black River, was located within the investigated area with the potential to be impacted by the project. The USACE makes all final determinations regarding jurisdiction.

UNT to Black River is likely classified as an intermittent stream because it has a defined bed and bank and well-defined ordinary high water mark (OHWM) even though there was no water present at the site during an incipient drought in the dry season. The OHWM was approximately five feet wide and approximately one foot deep. Upstream drainage land use is primarily rural town. Approximately 356.67 linear feet (LF) of the stream is within the investigated area. Based on a qualitative assessment, this resource is of average quality due to in-stream cover and good riparian and aquatic habitat potential. UNT to Black River flows south to the Black River, which flows to the Old Channel River, and finally to the Wabash River, which is considered a USACE-identified Section 10 River. Therefore, UNT to Black River would likely be considered a Waters of the United States.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

No direct or indirect impacts to UNT to Black River are anticipated since the limits of construction for this project end approximately 60 feet east of the toe of slope. Therefore, no impacts are expected.

IDNR-DFW responded, February 29, 2024, with recommendations regarding riparian habitat protection, pavement rehabilitation, and excavation and erosion control (Appendix C, pages 8 to 9). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Open Water Feature(s)	Presence	Impacts	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Retention/Detention Basin	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E), there are six open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on April 20, 2024, by KEG. Therefore, no impacts are expected.

A Waters of the U.S. Determination Report was approved by INDOT EWPSO May 10, 2024 (Appendix F, page 1). It was determined that no jurisdictional open water features were located within the investigated area and will be impacted by the project. The USACE makes all final determinations regarding jurisdiction.

Wetlands	Presence	Impacts	
		Yes	No
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total wetland area: _____ Acre(s) Total wetland area impacted: _____ Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetlands (Mark all that apply)	Documentation	ESD Approval Dates
	Wetland Determination	<input checked="" type="checkbox"/>
Wetland Delineation	<input type="checkbox"/>	<input type="checkbox"/>
USACE Isolated Waters Determination	<input type="checkbox"/>	<input type="checkbox"/>

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E), there are seven wetlands within the 0.5-mile search radius. There are no wetlands within or adjacent to the project area, which was confirmed by the site visit on October

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

12-13, 2022, by KEG. Therefore, no impacts are expected.

A *Waters of the U.S. Determination Report* was approved by INDOT EWPSO May 10, 2024 (Appendix F, page 1). It was determined that no jurisdictional wetlands were located within the investigated area and will be impacted by the project. The USACE makes all final determinations regarding jurisdiction.

Terrestrial Habitat

Presence

Impacts

Yes

No

Total terrestrial habitat in project area: 1.86 Acre(s) Total tree clearing: 0.9 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc.) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on April 20, 2024, by KEG, and the aerial map of the project area (Appendix B, page 2), there are forested tracts, grassy areas, shrubs, and parkway trees typical of a rural suburban setting, surrounding the project area. The dominant species include tall fescue (*Festuca arundinaces*), Canada goldenrod (*Poa pratensis*), black walnut (*Juglans nigra*), and giant ragweed (*Setaria faberi*). An estimated ten trees (0.9 acre), near the southwest portion of the project area, will be removed for construction of the enclosed drainage system and detention pond. The dominate species of tree to be removed include the Eastern black walnut (*Juglans nigra*). Impacts have been minimized by keeping work contained to the area necessary for the proposed construction. These impacts are necessary to achieve the proposed construction; therefore, avoiding the impacts is not practicable. Rehabilitation of disturbed areas shall be accomplished per the current INDOT Standard Specifications. Mitigation is not anticipated.

The IDNR-DFW responded on February 29, 2024, regarding minimization of tree and brush clearing, revegetation, and erosion control (Appendix C, pages 8 to 9). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Protected Species

Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed Yes No
 Section 7 informal consultation completed (IPaC cannot be completed) Yes No
 Section 7 formal consultation Biological Assessment (BA) required Yes No

Determination Received for Listed Bats from USFWS: NE NLAA LAA

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list) Yes No
 State species (not bird) found in project area (based upon consultation with IDNR) Yes No

Migratory Birds

Known usage or presence of birds (i.e. nests) Yes No
 State bird species based upon coordination with IDNR Yes No

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E), completed by KEG on January 31, 2024, the IDNR Gibson County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated February 29, 2024 (Appendix C, pages 8 to 9), the Natural Heritage Program's Database has been checked and to date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity. An INDOT 0.5-mile bat review occurred on January 16, 2024, and did not indicate the presence of endangered bat species in or within the project area.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

Project information was submitted through the U.S Fish and Wildlife Service's (USFWS) Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages 11 to 23). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*). Other species were generated in the IPaC species list along with the Indiana bat. Refer to the paragraphs below.

The project qualifies for the *Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat, Northern Long-eared Bat, and Tricolored Bat.*, dated December 13, 2024, between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on January 8, 2026, and based on the response provided, the project was found to "may affect, not likely to adversely affect (NLAA)" the Indiana bat (Appendix C, pages 24 to 35). INDOT reviewed and verified the effect finding on January 12, 2026, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) include general construction practices (General AMM1), temporary lighting (LAMM1), and tree removal and/or trimming (TRT AMM1, TRTAMM2, and TRTAMM3). AMMs and/or commitments are included as firm commitments in the Environmental Commitments section of this document.

The official species list generated from IPaC identified two additional species potentially occurring in the project area (Appendix C, page 17). The project is in the range of the monarch butterfly (*Danaus plexippus*), which is considered a "proposed threatened" species by USFWS, as well as the experimental population whooping crane (*Grus americana*). At this time, experimental population species do not require additional coordination with USFWS. INDOT, on behalf of FHWA, has determined the project will not jeopardize the continued existence of the monarch butterfly or its proposed critical habitat because this species relies on milkweed (*Asclepias sp.*) as a primary food source, especially in the larval stage, and a place to lay their eggs and it is not part of the dominant vegetation in the project area. Though roadsides can be good habitat for these butterflies as they often are comprised of milkweed and other nectar source plants; the project location does not contain vegetation vital to the survival of the monarch butterfly. There may be small areas of suitable habitat within the project area; however, temporary or permanent disturbances of this habitat will not jeopardize the continued existence of this species.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changes, the USFWS will be contacted for consultation.

Geological and Mineral Resources

- Project located within the Potential Karst Features Area of Indiana
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Study/Report reviewed by INDOT EWPSO (if applicable): _____

Discuss if project is located in Potential Karst Features Area of Indiana and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Describe if any impacts will occur to any karst features. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Karst MOU and coordinated and reviewed by INDOT EWPSO)

Based on a desktop review and the Indiana Karst Region Map, the project is located in the designated Indiana Karst Region as outlined in the most current INDOT *Protection of Karst Features during Project Development and Construction* guidance. According to the topo map of the project area (Appendix B, page 3) and the RFI report (Appendix E), there are no karst features identified within or adjacent to the project area. In the early coordination response on January 30, 2024, the IGWS did not indicate that karst features exist in the project area (Appendix C, pages 3 to 4). IGWS did indicate moderate liquefaction potential and the potential for active or abandoned petroleum exploration wells. The RFI report identified four petroleum wells located within 0.5 mile of the project area (Appendix E, page 11). Impacts to the wells are not anticipated as they are located outside of the project area. Response from IGWS has been communicated with the designer on December 6, 2024. No impacts are expected.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

SECTION C – OTHER RESOURCES

Drinking Water Resources

- Wellhead Protection Area(s)
- Source Water Protection Area(s)
- Water Well(s)
- Urbanized Area Boundary
- Public Water System(s)

Presence

X

Impacts

Yes	No
X	

- Is the project located in the St. Joseph Sole Source Aquifer (SSA):
- If Yes, is the FHWA/EPA SSA MOU Applicable?
- If Yes, is a Groundwater Assessment Required?

Yes	No
	X

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

This project is located in Gibson County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/Environmental Protection Agency (EPA)/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

In an early coordination response letter dated February 1, 2024, IDEM stated the project is not located within a wellhead protection area or source water assessment area (Appendix C, page 6 to 7). However, the Town of Owensville, Indiana Water and Sewer Website indicates the town is indeed implementing a Wellhead Protection Plan (WHP) (<https://owensville.org/water-sewer/>). The Owensville website indicates that its implementation of the WHP has begun in accordance with IDEM to protect the local drinking water. An early coordination letter was sent to the Town of Owensville Water & Sewer Superintendent on January 30, 2024. A response was not received within the 30-day timeframe. Therefore, no impacts are expected.

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on December 17, 2024, by KEG. One unconsolidated well is located adjacent to the east of the project area. This well is not anticipated to be impacted due to it being located outside the construction limits for the project. Should it be determined during the ROW phase that this well will be affected, a cost to cure will likely be included in the appraisal to restore the well.

Based on a desktop review of the INDOT MS4 website (<https://www.in.gov/indem/cleanwater/ms4s-boundaries-map-for-indiana/>) by KEG on December 17, 2024, this project is not located in an Urban Area Boundary. No impacts are expected.

Based on a desktop review, site visits on September 22, 2023, and April 20, 2024, by KEG, and the aerial map of the project area (Appendix B, page 2), this project is located where there is a public water system. The public water system will be affected due to project utility design, which includes a new storm trunk line to be constructed along the western curb line. Coordination with the Town of Owensville is ongoing. Work plan requests will be sent September 2025. Relocation plans are expected to be completed by January 2026. An early coordination letter was sent on January 30, 2024, to the Town of Owensville Water/Sewer Superintendent. A response from the superintendent was not received. Utility coordination has been initiated by the project designer and is ongoing throughout the design process.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

Floodplains

- Project located within a regulated floodplain
- Longitudinal encroachment
- Transverse encroachment
- Homes located in floodplain within 1000' up/downstream from project

Presence

Impacts

Yes No

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If applicable, indicate the Floodplain Level?

Level 1 Level 2 Level 3 Level 4 Level 5

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

Based on a desktop review of The IDNR Indiana Floodway Information Portal website (<https://gisdata.in.gov/portal/apps/webappviewer/index.html?id=7039bc8214154fd299da631f969064ea>) by KEG on December 17, 2024, and the RFI report, this project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page 12). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

Farmland

- Agricultural Lands
- Prime Farmland (per NRCS)

Presence

Impacts

Yes No

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006*) _____
 *If 160 or greater, see CE Manual for guidance.

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visits on September 22, 2023, and April 20, 2024, by KEG, and the aerial map of the project area (Appendix B, page 2), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPS do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on January 30, 2024, to NRCS. NRCS responded on February 10, 2024, indicating the project will not cause a conversion of prime farmland (Appendix C, page 5).

SECTION D – CULTURAL RESOURCES

Minor Projects PA **Category(ies) and Type(s)** Category B, Type 1: Condition A-ii & B-ii **INDOT Approval Date(s)** February 2, 2026 **N/A**

Full 106 Effect Finding
 No Historic Properties Affected No Adverse Effect Adverse Effect

Eligible and/or Listed Resources Present
 NRHP Building/Site/District(s) Archaeology NRHP Bridge(s)

Documentation Prepared (mark all that apply)	ESD Approval Date(s)	SHPO Approval Date(s)
APE, Eligibility and Effect Determination	<input type="checkbox"/>	<input type="checkbox"/>
800.11 Documentation	<input type="checkbox"/>	<input type="checkbox"/>
Historic Properties Report or Short Report	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Records Check and Assessment	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ic Survey Report	<input type="checkbox"/>	<input type="checkbox"/>
	August 29, 2025	

Indiana Department of Transportation

County Gibson Route SR 65 Des. No. 2001922 & 2400688

Other:

Memorandum of Agreement (MOA) **MOA Signature Dates** (List all signatories)

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

On February 2, 2026, the INDOT CRO determined that this project falls within the guidelines of Category B Type 1, Condition A-ii and B-ii, under the Minor Projects Programmatic Agreement (MPPA) (Appendix D, pages 1 to 12). Category B Type 1 includes replacement repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking. To satisfy Condition A-ii, an Archaeological Phase Ia Survey Report was required. The Phase Ia Survey was completed by Weintraut & Associates, Inc. (Weintraut) with a preliminary desktop review of the Indiana State Historical Architectural and Archaeological Research Database (SHAARD) and SHAARD GIS. The desktop review identified two previous investigations that overlap the project area with three sites previously recorded within or adjacent to the survey area: 12Gi1517, 12Gi1518, and 12Gi1530. Subject field investigations by Weintraut confirmed no evidence of previously recorded sites identified within survey area, and nine new archaeological sites were recorded, uncovering various artifact scatters most likely resulting from incidental discard and/or dumping over the course of several generations (Appendix D, pages 18 to 103). No additional investigations were recommended for the surveyed portion of the sites; however, an addendum Phase Ia investigation was conducted by WSP in November 2024 to account for additional survey areas related to additional proposed project right-of-way (Appendix D, pages 104 to 158). WSP's field investigation concluded with one new archaeological site recorded, 12Gi1713. The site was recorded to consist of historical artifact scatter related to potential demolition of one or more structures in the vicinity. No further archaeological assessments were recommended as a result of the surveys.

Additionally, since this project is adjacent to an Indiana Historic Sites and Structures Inventory (IHSSI) district, Owensville Historic District (IHSSI Nos. 051-493-06001 through 060107), this project falls under MPPA Category B-1, Condition B-ii, which includes work that is adjacent to or within a national Register-listed or National Register-eligible district or individual above-ground resource with unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, steps or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible individual above-ground resource or district. To satisfy Condition B-ii, a Section 2 Submittal was completed by Weintraut on April 22, 2025, and approved on February 2, 2026 (Appendix D, pages 13 to 17). The Section 2 submittal identifies nine contributing resources to the Owensville Historic District (IHSSI No. 051-493-06[001-107]):

- House at 202 West Brummitt Street (IHSSI No. 051-493-06009) – Queen Anne, c. 1895.
- First General Baptist Church (IHSSI No. 051-493-06026) – Gothic Revival, c. 1903/1961.
- Owensville Post Office (IHSSI No. 051-493-06013) – c. 1950.
- Owensville Public Library (IHSSI No. 051-49306010) – Brick columns and steps, as well as stone planting terrace walls
- House at 102 Mill Street (IHSSI No. 051-493-06018) – Bungalow/Craftsman, c. 1925.
- House at 104 Mill Street (IHSSI No. 051-493-06017) – Colonial Revival, c. 1890.
- House at 108 Mill Street (IHSSI No. 051-493-06039) – Queen Anne, c. 1895.
- House at 202 S. Main Street (IHSSI No. 051-493-06040) – Queen Anne, c. 1900
- House at 109 Warrick Street (IHSSI No. 051-493-06102) – Bungalow/Craftsman, c. 1920.

As a result of this survey, the following site concerns have been identified:

- First General Baptist Church (IHSSI No. 051-493-06026): Concrete steps and metal railing features are not anticipated to be impacted by project activities; however, it is best practice to avoid or replace in-kind as they add to the aesthetic of the resource and the historic district
- House at 102 Mill Street (IHSSI No. 051-493-06018): Brick steps and brick-paved front walk are not anticipated to be impacted by the project activities; however, it is best practice to avoid or replace in-kind as they add to the aesthetic of the resource and the historic district.
- House at 108 Mill Street (IHSSI No. 051-493-06039): A retaining wall is present adjacent to this resource. While the feature is not anticipated to be impacted by project activities; however, it is best practice to avoid or replace in-kind as they add to the aesthetic of the resource and the historic district.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

In addition to the aforementioned site concerns, the Section 2: Minor Projects PA Category B-1, Condition B-ii Submission form identifies the following unusual features that are adjacent to or within any of the properties or district adjacent to locations where curb, curb ramp, or sidewalk work will occur:

- House at 202 West Brummitt Street (IHSSI No. 051-493-06009) – Retaining wall and concrete entry steps (both look modern)
- General Baptist Church (IHSSI No. 051-493-06026) – Concrete steps and metal railing
- Owensville Public Library (IHSSI No. 051-49306010) – Brick columns and steps, as well as stone planting terrace walls
- House at 102 Mill Street (IHSSI No. 051-493-06018) – Brick entry steps and walk and stone planter boxes (boxes are modern stone)
- House at 104 Mill Street (IHSSI No. 051-493-06017) – Concrete steps to entry walk
- House at 108 Mill Street (IHSSI No. 051-493-06039) – Concrete retaining wall
- House at 109 Warrick Street (IHSSI No. 051-493-06102) – Concrete block retaining walls and walk steps
- House at 305 Mill Street – Concrete steps and tailings leading to porch
- House at 309 Mill Street – Concrete steps to enclosed porch (flower beds are modern)

As a result of the project, House at 305 Mill Street would have to rebuild the first three steps coming off the sidewalk. The sidewalk would tie into existing steps at the front of the larger trapezoid step and would likely need to remove and reset the wooden post retaining wall on either side of the stairs. Planters behind the wall would remain undisturbed. House at 309 Mill Street will not have impacts to the steps. The Sidewalk would tie in at the base of the bottom step and at the front of modern blocks. Modern blocks may need to be shifted during construction if they are just stacked. No planned disturbance of landscaping is anticipated for House at 309 Mill Street. No other impacts to the contributing buildings were identified. The findings described above will be included in the Environmental Commitments section of this document. No further consultation is required. This completed the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		Yes	No
Parks and Other Recreational Land			
Publicly owned park	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Publicly owned recreation area	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other (school, state/national forest, bikeway, etc.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Wildlife and Waterfowl Refuges			
National Wildlife Refuge	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
National Natural Landmark	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Wildlife Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Nature Preserve	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Historic Properties			
Site eligible and/or listed on the NRHP	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	<u>Evaluations Prepared</u>
Programmatic Section 4(f)	<input type="checkbox"/>
“De minimis” Impact	<input checked="" type="checkbox"/>
Individual Section 4(f)	<input type="checkbox"/>
Any exception included in 23 CFR 774.13	<input type="checkbox"/>

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife/waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page 2), and the RFI report (Appendix E), there are four potential 4(f) resources located within the 0.5-mile search radius. According to additional research and by the site visits on September 22, 2023, and April 20, 2024, by KEG, there is one Section 4(f) resource located adjacent to the project area. The Owensville Recreation Gym building is a nonprofit recreation facility that is open for public use adjacent to the western quadrant of the project area. No direct or indirect impacts to the recreation facility or supporting features are anticipated since the construction limits for the project are nearly 200 feet west; however, the publicly owned open space directly east of the recreation facility abuts the project area. Coordination with Owensville Clerk Treasurer confirmed that the Town of Owensville does not currently, nor plan to, utilize the subject area adjoining SR 65 as public use (Appendix D, page 1). Subsequent from this coordination, a response from INDOT's ESD agreed with the non-public use determination of the subject area and concurred the property is not a Section 4(f) resource. (Appendix D, page 2). The project will not use this resource by taking permanent ROW and will not indirectly use the resource in such a way that the protected activities, features, or attributes that qualify a resource for protection under Section 4(f) are substantially impaired. Therefore, no 4(f) use is expected at this resource.

Additionally, the MPPA assessment form completed by Weintraut of April 16, 2025, and approved by INDOT on February 2, 2026, lists four documented resources as Section 4(f) de minimis use of a historic resource (Appendix D, page 8):

- Carnegie Library (IHSSI No. 051-493-06010): approximately 0.012 acre of permanent ROW and 0.015 acre of temporary ROW are anticipated to be acquired from this resource for grading and drainage pipe installation construction activities.
- House at 104 Mill Street (IHSSI No. 051-493-06017): approximately 0.007 acre of temporary ROW is anticipated to be acquired from this resource for sidewalk construction activities.
- House at 102 Mill Street (IHSSI No. 051-493-06018): approximately 0.003 acre of temporary ROW is anticipated to be acquired from this resource for sidewalk construction activities.
- House at 109 Warrick Street (IHSSI No. 051-493-06102): approximately 0.007 acre of temporary ROW is anticipated to be acquired from this resource for sidewalk construction activities.

INDOT and FHWA considers the taking of the minimal amounts of temporary and permanent ROW from the properties listed above as a de minimis Section 4(f) use of historic resources. A de minimis impact involves the use of Section 4(f) property that is generally minor in nature. A de minimis impact is one that, after taking into account avoidance, minimization, mitigation, and enhancement measures, results in no adverse effect to the activities, features, or attributes qualifying a park, recreation area, or refuge for protection under Section 4(f).

Section 6(f) Involvement

Section 6(f) Property

Presence

Use

Yes

No

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits the conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of four properties in Gibson County (Appendix I, page 19). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

	Yes	No
Is the project in the most current STIP/TIP?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project located in an MPO Area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project in an air quality non-attainment or maintenance area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If Yes, then:		
Is the project in the most current MPO TIP?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the project exempt from conformity?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, then:		
Is the project in the Transportation Plan (TP)?	<input type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input type="checkbox"/>

Location in STIP: FY 2026-2030, Amended TBD

Name of MPO (if applicable): Evansville Metropolitan Planning Organization (EMPO)

Location in TIP (if applicable): EMPO TIP FY 2026-2030 (TBD)

Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

This project will be included in the Fiscal Year (FY) 2026-2030 Statewide Transportation Improvement Program (STIP). STIP/TIP approval verification will be included in the final document and is anticipated after this document's release for public involvement efforts.

This project is located in Gibson County, which is currently a maintenance area for PM-2.5 according to the Environmental Protection Agency (EPA) Green Book (https://www3.epa.gov/airquality/greenbook/anayo_in.html). This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c) or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION G - NOISE

	Yes	No
Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date Noise Analysis was approved/technically sufficient by INDOT ESD: _____		

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana INDOT Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

- Will the proposed action comply with the local/regional development patterns for the area?
- Will the proposed action result in substantial impacts to community cohesion?
- Will the proposed action result in substantial impacts to local tax base or property values?
- Will construction activities impact community events (festivals, fairs, etc.)?
- Does the community have an approved transition plan?
If No, are steps being made to advance the community's transition plan?
- Does the project comply with the transition plan? (explain in the discussion below)

Yes	No
X	
	X
	X
	X
X	
X	

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

On January 7, 2026, KEG reviewed www.indianafestivals.org for any special events or festivals in Gibson County throughout the year. The following five special events or festivals were noted, assuming an annual occurrence:

- Haubstadt Sommerfest: June 18 – June 20, 2026
- Gibson County Fair: July 4 – July 11, 2026
- Owensville Watermelon Festival: July 1 – July 31, 2026
- Oakland City Sweet Corn Festival: July 30 – August 1, 2026
- Lyles Station Corn Maze: October 4 – October 26, 2026

If these events are held during the proposed construction activities, the commute times to events may be impacted causing a short-term impact. SR 65 will undergo a full closure during the project's construction and an associated detour will be posted accordingly. Announcements regarding construction activities will be published on the INDOT social media pages and coordination with the community will occur to minimize disruption to the extent practical.

SR 65 will close for approximately up to two construction seasons. Delays shall occur during construction but will cease with project completion. Temporary community and economic impacts may occur due to increased travel time and expense; therefore, no long-term negative impacts to the community or its economy are expected.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B, page 2), and the RFI report (Appendix E) there are 14 public facilities within the 0.5-mile search radius. There are three public facilities within or adjacent to the project area, which was confirmed by the site visits on September 22, 2023, and April 20, 2024, by KEG. The General Baptist Church is located adjacent to the northeast quadrant of the project area. An early coordination letter was sent on January 30, 2024, to the General Baptist Church. No response was received within the 30-day time frame. The Owensville Carnegie Library is located adjoining to the northeast quadrant of the project area. An early coordination letter was sent on January 30, 2024, to the council of Owensville. No response was received within the 30-day timeframe. The Owensville Recreation Gym is located adjacent to the south-southwest quadrant of the project area. An early coordination letter was sent to the recreation gym January 30, 2024. No response was received within the 30-day timeframe. No direct impacts are anticipated, as full closure of SR 65 is not planned when school is in session. Temporary indirect impacts are anticipated during construction. The official state detour route is approximately 37 miles; however, local traffic can utilize local roads to avoid major inconveniences. Access to all properties will be maintained during construction.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Indiana Department of Transportation

County Gibson Route SR 65 Des. No. 2001922 & 2400688

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Will the project result in adversely high and disproportionate impacts to EJ populations?

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high and adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

This analysis was performed for this project prior to the issuance of recent federal Executive Orders (EO) from January 2025, including EO 14154, EO 14148, and EO 14173. As such, this analysis is included for transparency but is no longer applicable to the impacts analysis for federal projects and this impact was not considered in the federal decision.

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an EJ Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 1.81 acres of permanent right-of-way and 0.24 acre of temporary right-of-way. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exist and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Gibson County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Census Tract 501 in Gibson County. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2024 ACS 5-Year Estimates was obtained from the U.S. Census website (<https://data.census.gov/cedsci/>) on February 28, 2024, by KEG. The data collected for minority and low-income populations within the AC are summarized in the below table.

Table: Minority and Low-Income Data (2021: US Census Bureau, ACS 5-Year Estimates)

	COC – Gibson County	AC-1 – Census Tract 501, Gibson County
Percent Minority	9	3
125% of COC	11	AC > 125% COC
EJ Population of Concern		No
Percent Low-Income*	10	7
125% of COC	12	AC > 125% COC
EJ Population of Concern		No

AC-1, Census Tract 501, Gibson County has a percent minority of 3% which is below 50% and is below the 125% COC threshold. Therefore, AC-1 does not contain minority populations of EJ concern.

AC-1, Census Tract 501, Gibson County has a percent low-income of 7% which is below 50% and is below the 125% COC threshold. Therefore, AC-1 does not contain low-income populations of EJ concern.

The census data sheets, map, and calculations can be found in Appendix I, pages 20 to 23. The project will result in positive impacts to all populations within the project area in the form of improved traffic flow and connectivity. No further environmental justice analysis is warranted.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is a BIS or CSRS required?

Number of relocations: Residences: N/A Businesses: N/A Farms: N/A Other: N/A

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)

X

Phase I Environmental Site Assessment (Phase I ESA)

--

Phase II Environmental Site Assessment (Phase II ESA)

--

Design/Specifications for Remediation required?

--

Date RFI concurrence by INDOT SAM (if applicable): April 15, 2024

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of GIS and available public records, the RFI was completed on January 31, 2024, by KEG and INDOT Site Access and Management (SAM) provided their concurrence on April 15, 2024 (Appendix E). One State Cleanup Site is located within 0.5 mile of the project area. Two Underground Storage Tank (UST) sites are located within 0.5 mile of the project area. Three Leaking Underground Storage (LUST) sites are located within 0.5 mile of the project area. Two National Pollutant Discharge Elimination System (NPDES) facilities are located within 0.5 mile of the project area. One Institutional Controls (IC) site is located within 0.5 mile of the project area. One NPDES Pipe Location is located within 0.5 mile of the project area. Only the hazmat sites that could affect the project area are discussed below.

One (1) state cleanup site is located within the 0.5-mile search radius. The site, Montgomery Manufacturing, 109 W Montgomery Street/202 South Main Street, AI ID #35359, is located adjacent to the east of the project area, south of Montgomery Street. The facility constructed and repaired clocks and timers and used various solvents, which included tetrachloroethene (PCE) and trichloroethene (TCE). Remediation of the soil and groundwater at the site included removal of 4,860 tons of impacted soil (soil target=1 mg/kg PCE) from the southwest corner of Main St and West Warrick St and in-situ reductive dechlorination. Soil and groundwater contamination remain on- and off-site from Montgomery St south to past West Warrick St. Chemicals of concern include PCE, TCE, and breakdown products resulting from the reductive dechlorination process. Groundwater in the area is shallow, less than five (5) feet below ground surface and flows west and southwest. An Environmental Restrictive Covenant (ERC) was recorded on the property on October 3, 2023. The ERC specifically prohibits the use of the property for residential purposes or the use or extraction of groundwater on the property. On January 4, 2024, the Town of Owensville adopted a "Resolution Approving Reliance on Town of Owensville Ordinance Chapter 12.2 For Purpose of Obtaining IDEM Site Closure", which states that no groundwater wells are within the plume boundary and any use/installation of groundwater wells within the plume boundary is prohibited. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) and the IDEM Project Manager, Nicolas Monroe (NMonroe@idem.in.gov) before RFC. A Phase II Environmental Site Assessment (ESA) is recommended prior to RFC. Prior to any investigation activities, a scope of work plan (SOW) will be prepared and submitted to INDOT SAM for review and approval.

If groundwater monitoring wells are encountered in the project area, they should be maintained in-place. If they cannot be maintained, then the contractor must contact the INDOT Project Manager who will notify INDOT EWPSO who will then notify the permit holder that the well must be removed prior to construction. The permit holder is responsible for coordination with IDEM and INDOT EWPSO for replacement or relocation of the well. If a property owner cannot be found in connection with the monitoring well, then well abandonment will be included in the project contract. All well abandonment activities must be completed by an Indiana Licensed Well Driller in accordance with 312 IAC 13-10. Regardless of whether the well is abandoned by the contractor or the property owner, a record of well abandonment, including the well driller's license number, must be provided to the INDOT Project Manager once the well has been abandoned.

Two UST sites are located within the project area and are associated with Consolidated Grain & Barge, previously known as Gibson County Co Op Owensville Grain, 420 South Mill Street, AI ID # 31397. One (1) 500-gallon gasoline UST was removed in October of 1988. No record of confirmatory sampling was found. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 & 2400688

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Other	<input type="checkbox"/>

IN Department of Environmental Management (401/Rule 5)

Nationwide Permit (NWP)	<input type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Isolated Wetlands	<input type="checkbox"/>
Rule 5	X
Other	<input type="checkbox"/>

IN Department of Natural Resources

Construction in a Floodway	<input type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the discussion below)

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

Since more than one acre of soil disturbance is planned for the project, a Construction Stormwater General Permit (CSGP), formerly known as Rule 5, will be required.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations. It is the responsibility of the project sponsor to identify and obtain all required permits.

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 &
2400688

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

1. If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT Vincennes District)
2. It is the responsibility of the project sponsor to notify school corporations, Town of Owensville and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT ESD)
4. General GAMM1: Ensure all operators, employees, and contractors working in areas of Indiana bat, NLEB, or TCB suitable habitat are aware of all Transportation Agency environmental commitments, including all applicable AMMs. (USFWS)
5. Lighting LAMM1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
6. Tree Removal TRTAMM1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to the extent practicable to avoid tree removal/trimming in excess of what is required to implement the project safely. (USFWS)
7. Tree Removal TRTAMM2: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
8. Tree Removal TRTAMM 3: Ensure tree removal/trimming is limited to the inactive season, occurs within 100 feet of the road/rail surface, and is outside of documented habitat for the Indiana bat, NLEB, and TCB. (USFWS & IDNR-DFW)
9. State Cleanup Sites (1 of 2): One (1) state cleanup site is located within the 0.5 mile search radius. The site, Montgomery Manufacturing, 109 W Montgomery Street/202 South Main Street, AI ID #35359, is located adjacent to the east of the project area, south of Montgomery Street. The facility constructed and repaired clocks and timers and used various solvents, which included tetrachloroethene (PCE) and trichloroethene (TCE). Remediation of the soil and groundwater at the site has included removal of 4,860 tons of impacted soil (soil target=1 mg/kg PCE) from the southwest corner of Main and West Warrick and in-situ reductive dechlorination. Soil and groundwater contamination remain on- and off-site from Montgomery Street south to past West Warrick Street. COCs include PCE, TCE, and breakdown products resulting from the reductive dechlorination process. Groundwater has been as shallow as less than five (5) feet below ground surface and flow direction is to the west and southwest. An Environmental Restrictive Covenant (ERC) was recorded on the property on October 3, 2023.
10. State Cleanup Sites (2 of 2): The ERC specifically prohibits the use of the property for residential purposes or the use or extraction of groundwater on the property. On January 4, 2024, the Town of Owensville adopted a "Resolution Approving Reliance on Town of Owensville Ordinance Chapter 12.2 For Purpose of Obtaining IDEM Site Closure", which states that no groundwater wells are within the plume boundary and any use/installation of groundwater wells within the plume boundary is prohibited. Coordination will be conducted with the IDEM Institutional Controls section (institutionalcontrols@idem.IN.gov) and the IDEM Project Manager, Nicolas Monroe (NMonroe@idem.in.gov) before RFC. A Phase II Environmental Site Assessment (ESA) is recommended to occur before RFC. Prior to any investigation activities, a scope of work plan (SOW) will be prepared and submitted to INDOT SAM for review and approval. (INDOT SAM)
11. If groundwater monitoring wells are encountered in the project area, they should be maintained in-place. If they cannot be maintained, then the contractor must contact the INDOT Project Manager who will notify the INDOT Permits Group. The INDOT Permits Group will notify the permit holder that the well must be removed prior to construction. The permit holder is responsible for coordination with IDEM and the INDOT Permits Group for replacement or relocation of the well. If a property owner cannot be found in connection with the monitoring well, then well abandonment will be included in the project contract. All well abandonment activities must be completed by an Indiana Licensed Well Driller in accordance with 312 IAC 13-10. Regardless of whether the well is abandoned by the contractor or the property owner, a record of well abandonment, including the well driller's license number, must be provided to the INDOT Project Manager once the well has been abandoned. (INDOT SAM).
12. Underground Storage Tank (UST) Sites: Two (2) Underground Storage Tank (UST) sites are located within the 0.5 mile radius. Both UST sites are located adjacent to the project area and are associated with Consolidated Grain & Barge, previously known as Gibson County Co Op Owensville Grain, 420 South Mill Street, AI ID # 31397. One (1) 500 gallon gasoline UST was removed in October of 1988. No record of confirmatory sampling was found. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)
13. House at 202 West Brummitt Street (IHSSI No. 051-493-06009): no impacts will occur to the sidewalk, concrete entry steps, curb ramp, or wall of concrete blocks at this property and will be marked as "Do Not Disturb" on the project plans. This commitment will be entered into INDOT's project commitment database. (INDOT CRO)

Indiana Department of Transportation

County Gibson

Route SR 65

Des. No. 2001922 &
2400688

14. General Baptist Church (IHSSI No 051-493-06026): no impacts will occur to the existing concrete steps and metal railing at this property and will be marked as "Do Not Disturb" on the project plans. This commitment will be entered into INDOT's project commitment database. (INDOT CRO)
15. Owensville Public Library (IHSSI No. 051-493-06010): no impacts will occur to the brick columns, steps, stone planter terrace wall, or light poles at this property and will be marked as "Do Not Disturb" on the project plans. This commitment will be entered into INDOT's project commitment database. (INDOT CRO)
16. House at 102 Mill Street (IHSSI No. 051-493-06018): no impacts will occur to the brick entry steps, stone planter boxes, stone wall, or railings at this property and will be marked as "Do Not Disturb" on the project plans. This commitment will be entered into INDOT's project commitment database. (INDOT CRO)
17. House at 104 Mill Street (IHSSI No. 051-493-06017): no impacts will occur to the existing concrete steps at this property and will be marked as "Do Not Disturb" on the project plans. This commitment will be entered into INDOT's project commitment database. (INDOT CRO)
18. House at 108 Mill Street (IHSSI No. 051-493-06039): no impacts will occur to the existing steps or concrete retaining wall at this property and will be marked as "Do Not Disturb" on the project plans. This commitment will be entered into INDOT's project commitment database. (INDOT CRO)
19. House at 109 Warrick Street (IHSSI No. 051-493-06102): no impacts will occur to the existing walk steps or concrete block retaining wall at this property and will be marked as "Do Not Disturb" on the project plans. This commitment will be entered into INDOT's project commitment database. (INDOT CRO)
20. House at 309 Mill Street: no impacts will occur to the existing steps or landscaping at this property. Modern blocks may need to be shifted during construction if they are just stacked. This commitment will be entered into INDOT's project commitment database. (INDOT CRO)
21. Notable Carnegie Library (IHSSI No. 051-493-06010): no impacts will occur to the existing brick columns and must be avoided during project work. These columns at this property will be marked as "Do Not Disturb" on the project plans. (INDOT CRO)

Further Consideration:

22. Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ration based on area of impact. Impacts under 0.10 acre in a rural area typically do not require mitigation or additional plantings beyond seeding and stabilizing disturbed area, though there are exceptions for high quality habitat sites. Impacts under 0.10 acre in an urban area should be mitigated by replacing trees that are 10" diameter-at-breast height (dbh) or greater by planting five trees, 1" to 2" in dbh, for each tree which is removed that is 10" dbh or greater (IDNR-DFW)

Categorical Exclusion Level 2
US 65
DES 2001922 & 2400688, Roadway Improvement Project
Gibson County, Indiana

APPENDICES

<u>Title</u>	<u>Page</u>
A: INDOT Supporting Documentation	
INDOT Threshold Table	A-1
B: Graphics	
Project Site Map.....	B-1
Project Aerial Map.....	B-2
Project Topographic Map	B-3
Project Site Photographs	B-4
Preliminary Project Plans	B-31
Preliminary Project Right of Way Plans.....	B-64
C: Early Coordination	
Early Coordination Sample Letter.....	C-1
Indiana Geological and Water Survey Report	C-3
USDA NRCS Response.....	C-5
IDEM Wellhead Proximity Response.....	C-6
Indiana Department of Natural Resources, Division of Fish and Wildlife	C-8
INDOT Vincennes Environmental Section Manager Response	C-10
USFWS Official IPaC Species List	C-11
USFWS NLAA Concurrence Letter	C-24
D: Section 106 of the NHPA	
Minor Projects PA Project Submittal and Assessment Form	D-1
Section 2: Minor Projects PA Category B-1, Condition B-ii Submission Form	D-13
Phase Ia Archaeological Field Reconnaissance Report Excerpt	D-18
Phase Ia Addendum Archaeological Survey Report Excerpt.....	D-104
Town of Owensville, Clerk Treasurer Section 4(f) Response.....	D-159
INDOT ESD Section 4(f) No Use Concurrence	D-160
E: Red Flag and Hazardous Materials	
Red Flag Investigation	E-1
F: Water Resources	
Approved Waters Report.....	F-1
IDNR Floodplain Map.....	F-12
G: Public Involvement	
Notice of Survey Sample Letter.....	G-1
H: Air Quality	
INDOT STIP FY 2026-2030	TBC
EMPO TIP FY 2026-2030	TBC

I: Additional Information

Engineering Assessment Report (excerpt) I-1
Gibson County LWCF SitesI-19
EJ Analysis – US Census Data Sheets, Maps, CalculationsI-20

APPENDIX D

Section 106 of the NHPA

APPENDIX A

INDOT Supporting Documents

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	"No Effect", "Not likely to Adversely Affect" (With select AMMs ⁷)	"Not likely to Adversely Affect" (With any AMMs or commitments)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or "No Effect"	"Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁹
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level					
<ul style="list-style-type: none"> • District Env. (DE) • Env. Serv. Div. (ESD) • FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a "Likely to Adversely Affect". Other findings can be processed as a lower-level CE.

⁹ Potential for causing a disproportionately high and adverse impact.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

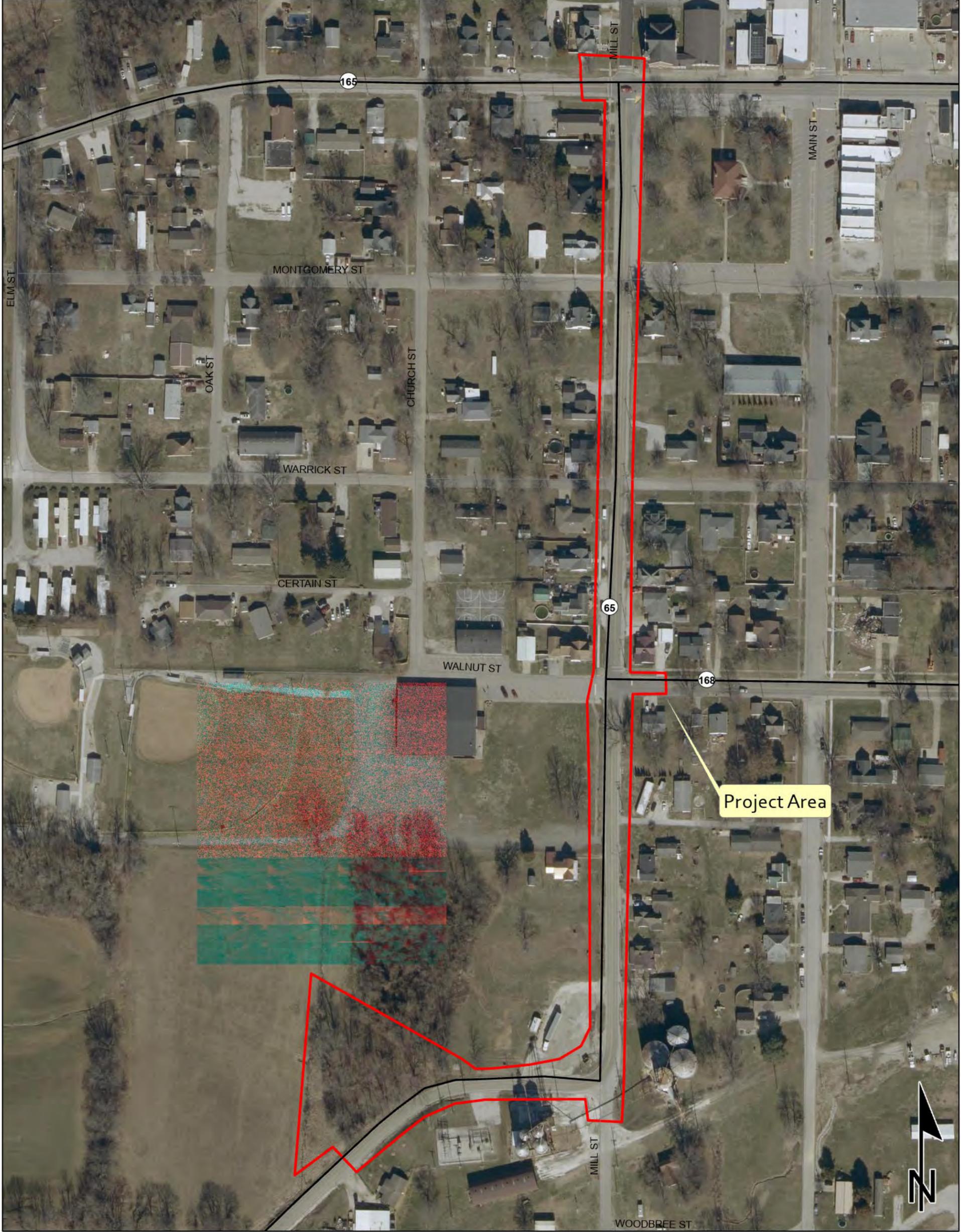
APPENDIX B

Graphics

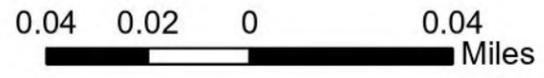
Aerial Map

SR 65, 0.26 Mile South of SR 168 (S. Limits Owensville) to SR 165

Des. No. 2001922, HMA Overlay with Minor Structural, Gibson County, Indiana



Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

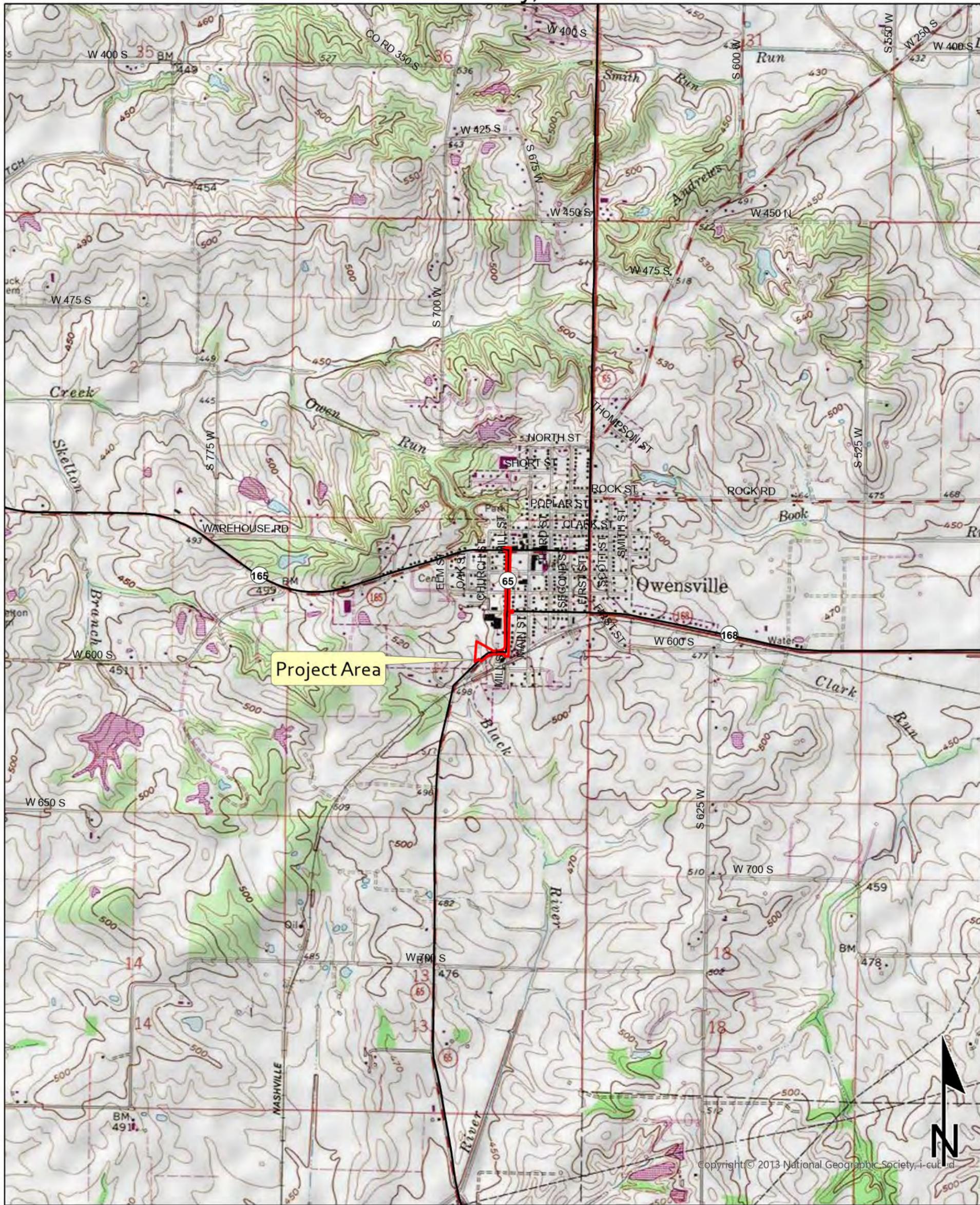


This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Topographic Map

SR 65, 0.26 Mile South of SR 168 (S. Limits Owensville) to SR 165

Des. No. 2001922, HMA Overlay with Minor Structural, Gibson County, Indiana

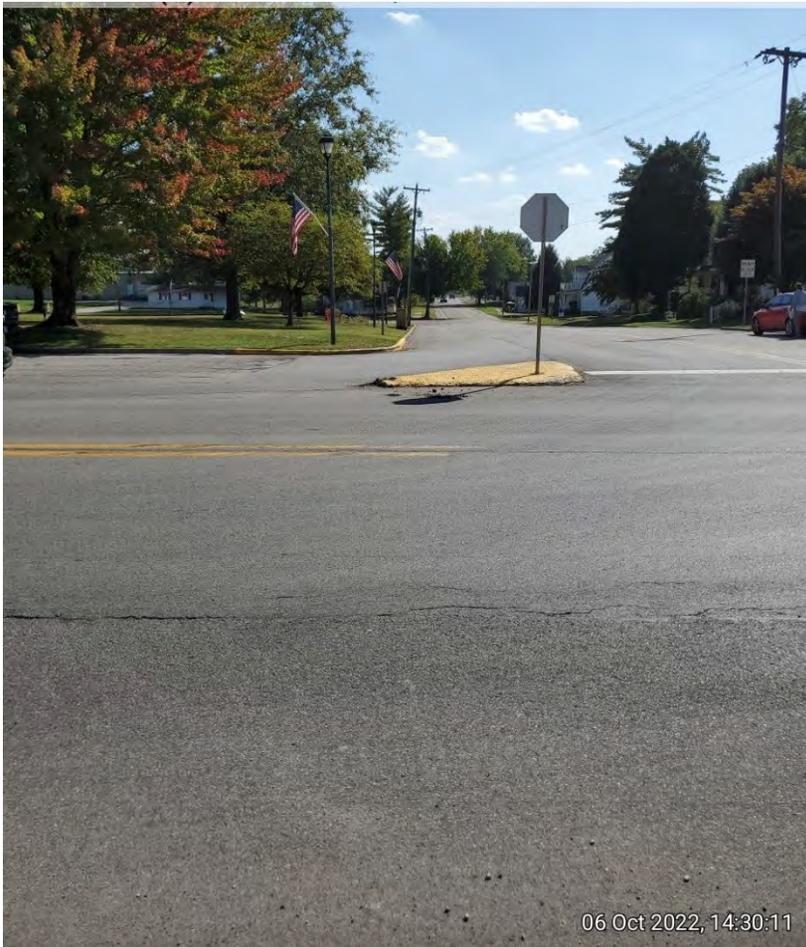


Sources: 0.4 0.2 0 0.4 Miles
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**OWENSVILLE
 QUADRANGLE
 INDIANA
 7.5 MINUTE SERIES
 (TOPOGRAPHIC)**

De No. 2001922

Photo Date: October 6, 2022



1. Looking south SR 65. from the intersection of SR 165 (Brummitt St) and SR 65. There is no sidewalk but there are curbs and gutters on the east side of SR 65. Photo Map 1.



2. Looking north along SR 65 from the intersection of SR 165 and SR 65. There is a sidewalk, curb, and gutter along the east side of SR 65. There is no ADA ramp. There is city water for the fire hydrant on this corner. Photo Map 1.

De No. 2001922

Photo Date: October 6, 2022



3. Looking west down SR 165 from the intersection of SR 165 and SR 65. There are sidewalks with ADA ramps on both crossings, curbs, and gutter Photo Map 1.



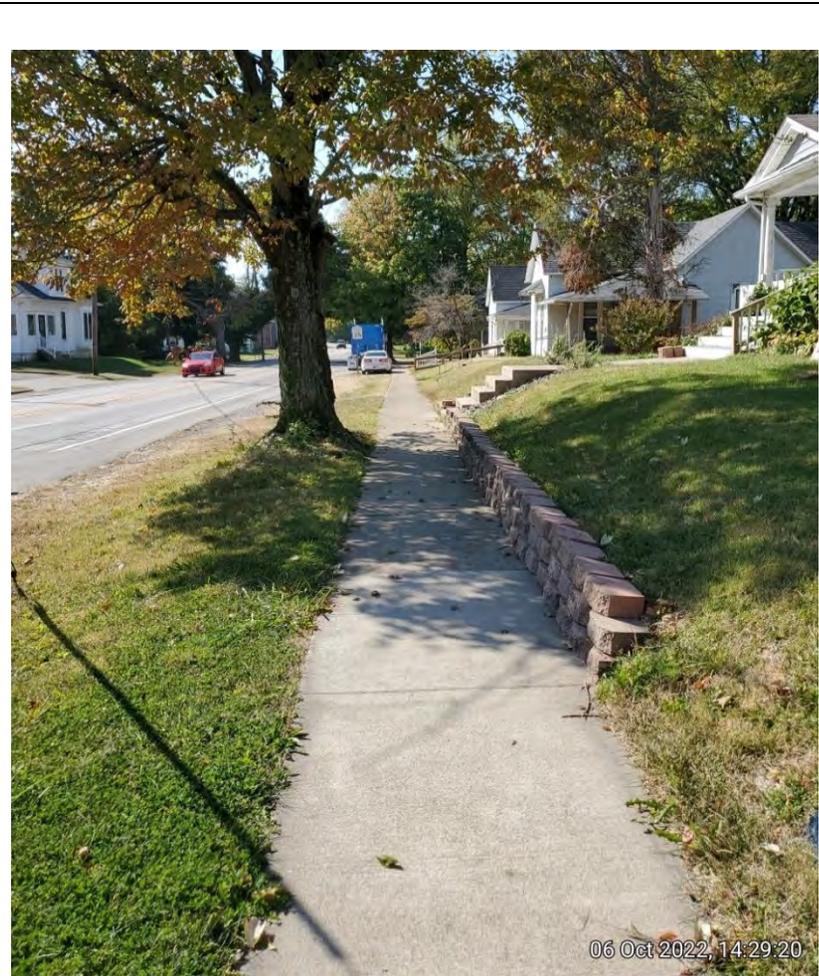
4. Looking east down SR 165 from the intersection of SR 165 and SR 65. There are sidewalks with ADA ramps on both crossings, curbs, and gutter Photo Map 1.

De No. 2001922

Photo Date: October 6, 2022



5. Looking south down the west side of SR 65. There are ADA ramps, curbs, and gutter Photo Map 1.



6. Looking west along the north side of SR 165. from the intersection of SR 65 and SR 165. There is a sidewalk but no ADA ramp, curbs, or gutter Photo Map 1.

De No. 2001922

Photo Date: October 6, 2022



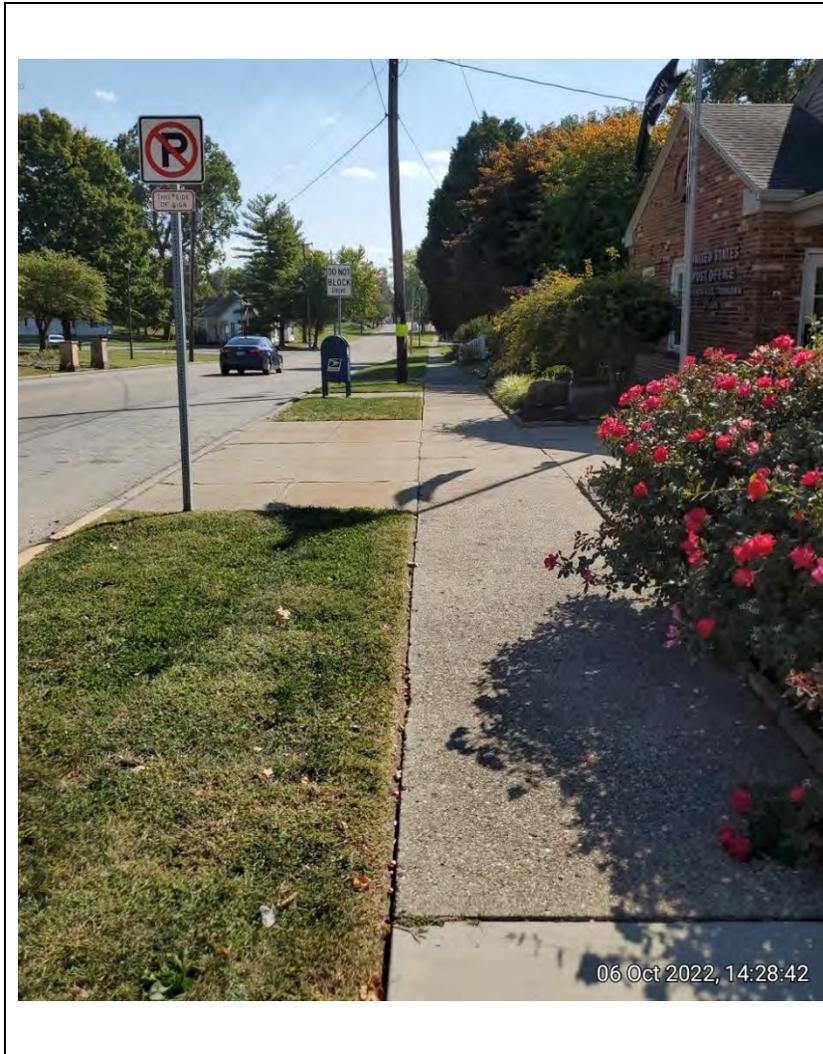
7. Looking north up the east side of SR 65 from the intersection of SR 65 and SR 165. There is an ADA ramp, short curb, and gutter. Photo Map 1.



8. Looking west along SR 165 from the intersection with SR 165 and SR 65., farther west from image 3. Photo Map 1.

De No. 2001922

Photo Date: October 6, 2022



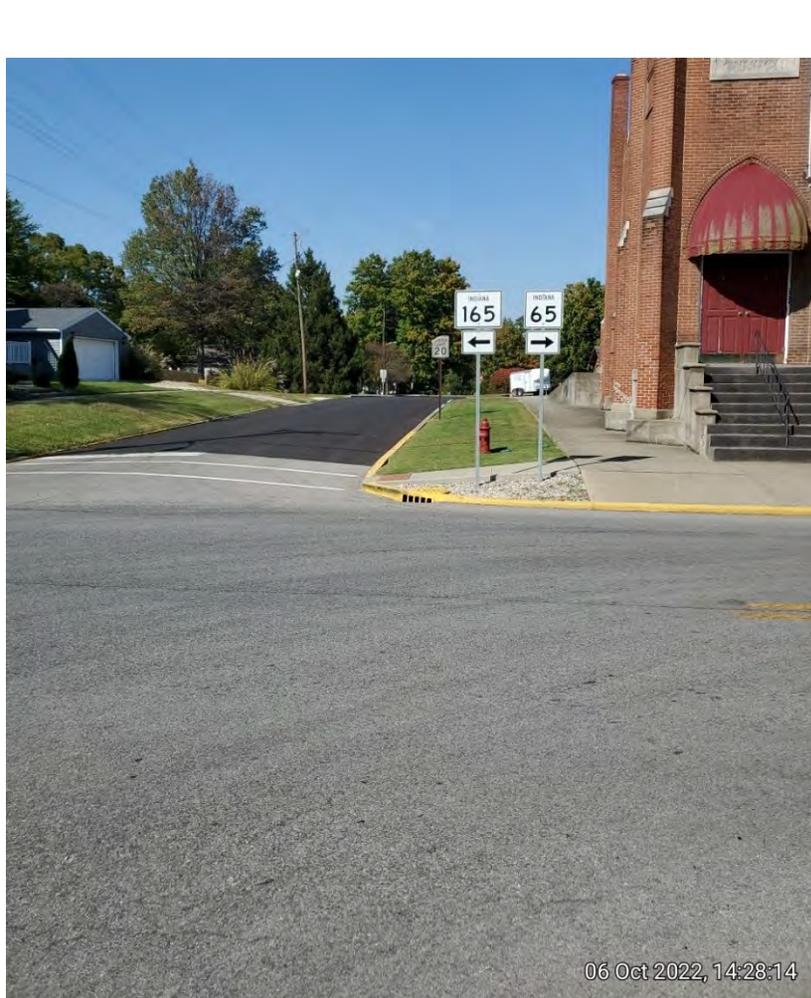
9. Looking south on the west side of SR 65 from the intersection of SR 65 and SR 165. There is an ADA ramp at the intersection (see image 5) and curbs but no gutter Photo Map 1.



10. Looking east down the center of SR 165 from the intersection of SR 165 and SR 65. This is a parking area for the courthouse and businesses along SR 165. Photo Map 1.

De No. 2001922

Photo Date: October 6, 2022



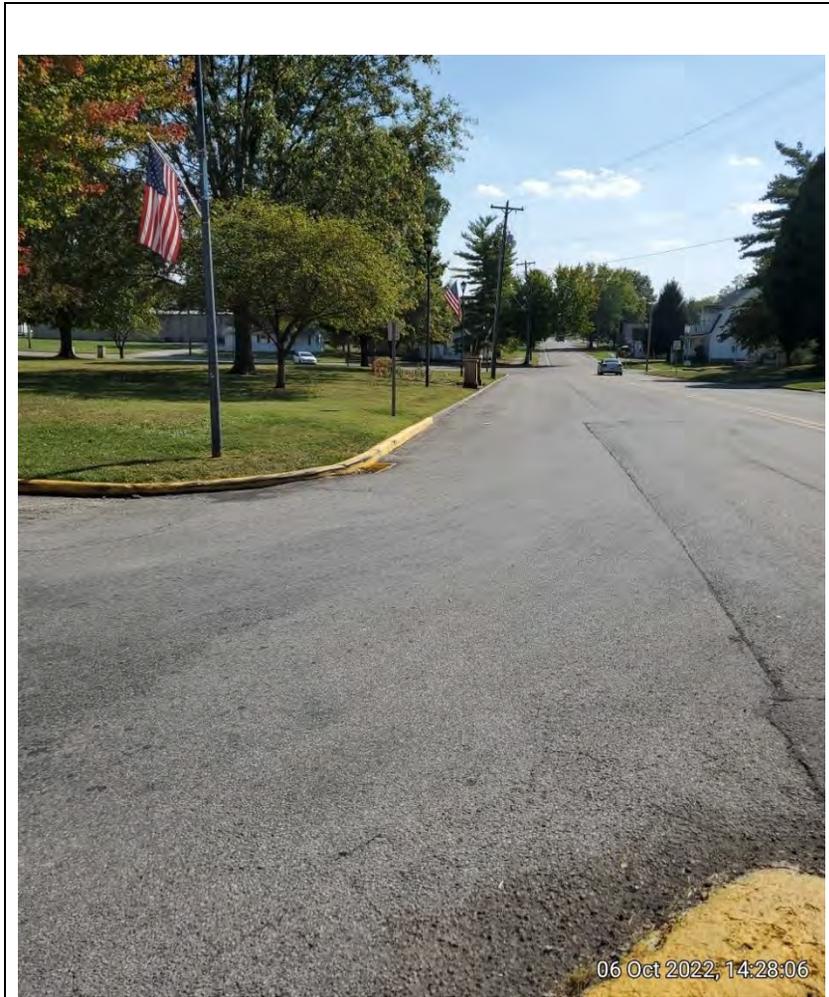
11. Looking north from the center of the intersection of SR 165 and SR 65 Photo Map 1.



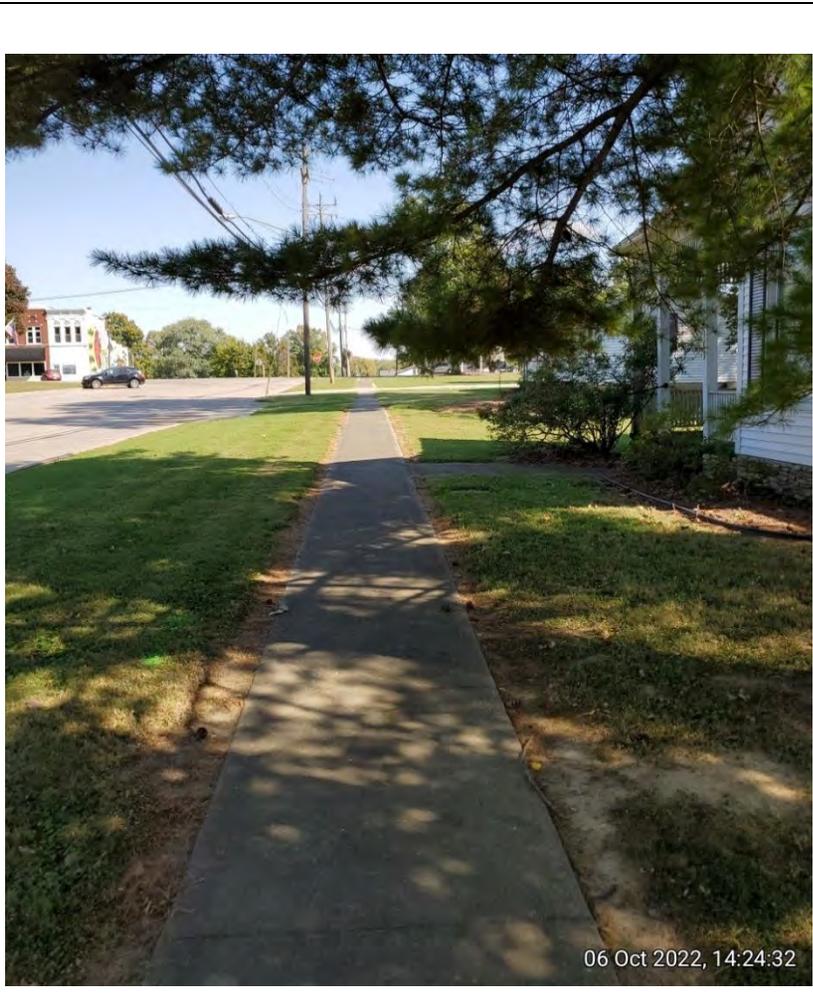
12. Looking west along the south side of SR 165 from the intersection of SR 165 and SR 65. There is an ADA ramp, sidewalk, and curb but no gutter. Photo Map 1.

De No. 2001922

Photo Date: October 6, 2022



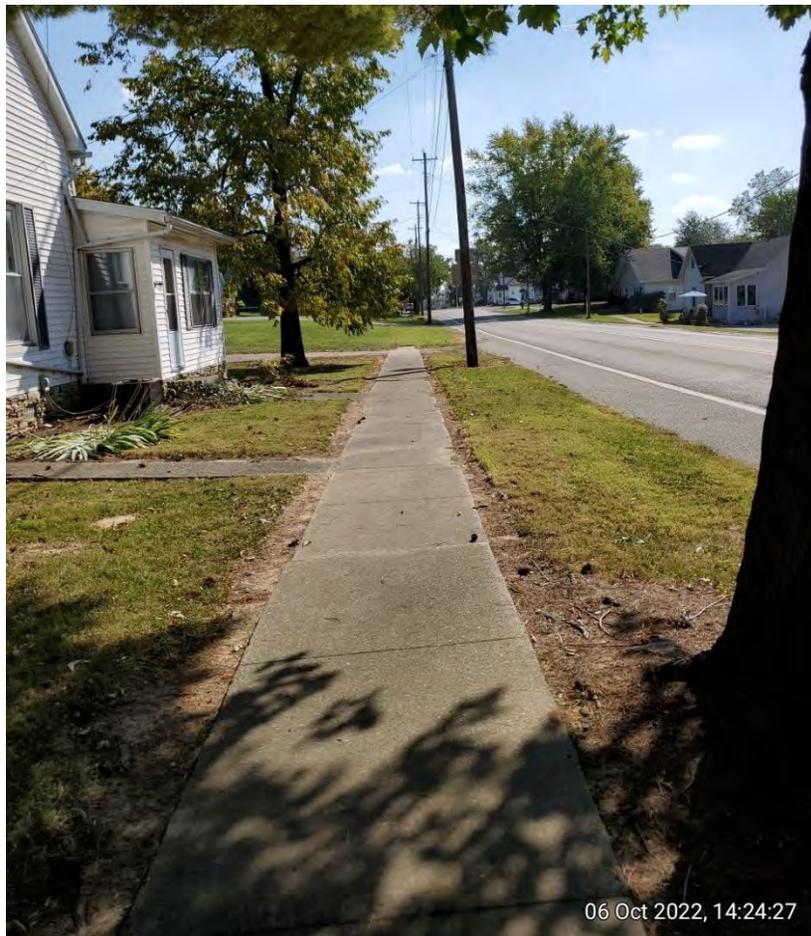
13. Looking south down the east side of SR 65 from the intersection of SR 165 and SR 65. There are curbs and gutters but no sidewalks or ADA ramp Photo Map 1.



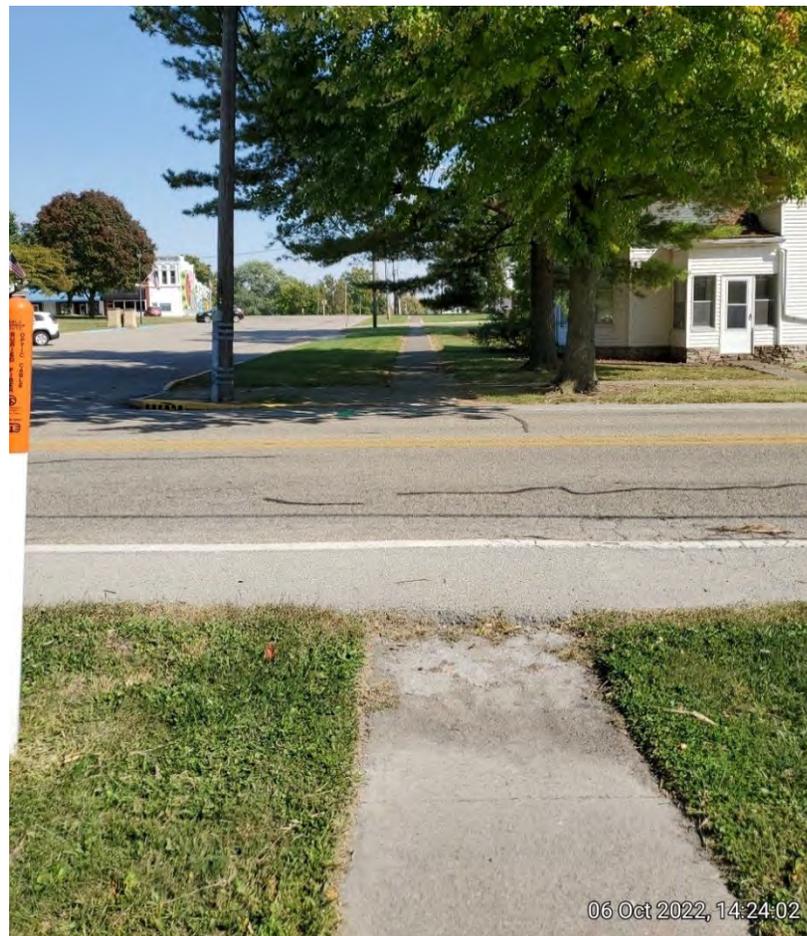
14. Looking east down the south side of Montgomery St from the east side of the intersection of S SR 65 and W. Montgomery St. There is a sidewalk, but no ADA ramp, curb, or gutter Photo Map 2.

De No. 2001922

Photo Date: October 6, 2022



15. Looking south down the east side of SR 65 from the east side of the intersection of SR 65 and W. Montgomery St. There is no ADA ramp, curb, or gutter. Photo Map 2.



16. Looking east down the south side of Montgomery St. from the west side of the intersection of SR 65 and W. Montgomery St. There is no ADA ramp, but there is a curb and gutter on the south side of the intersection. Photo Map 2.

De No. 2001922

Photo Date: October 6, 2022



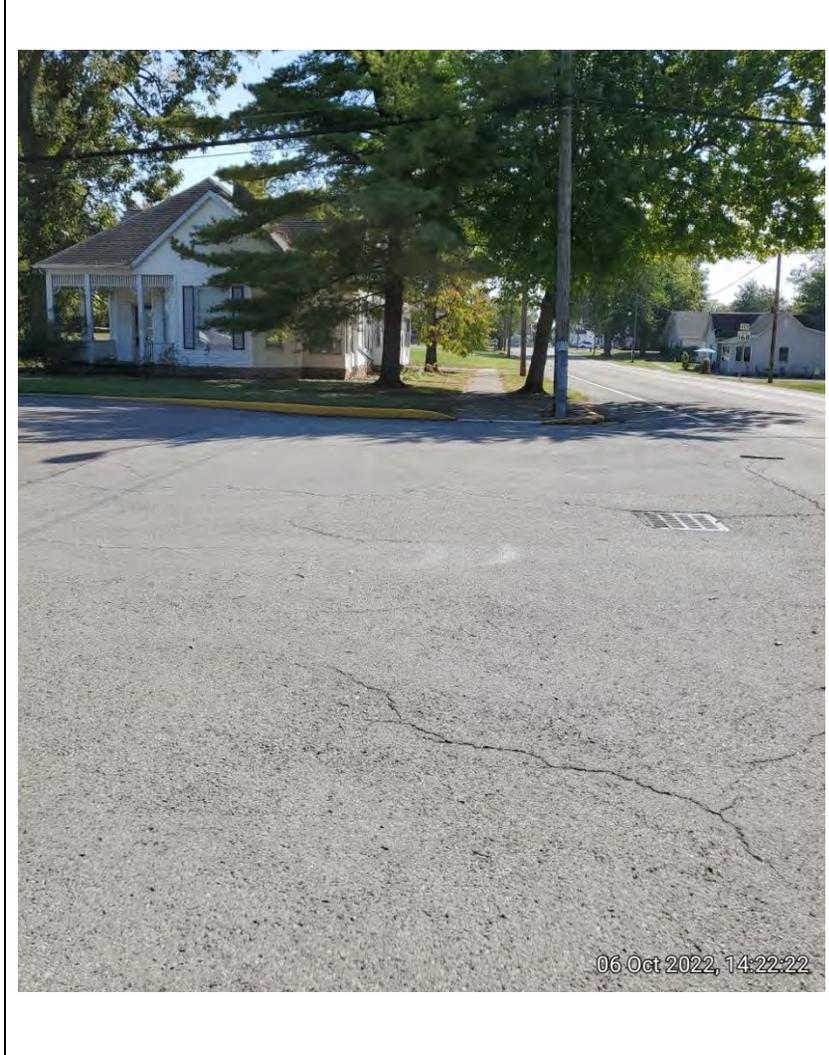
17. Looking north along SR 65 from the west side of the intersection of SR 65 and W. Montgomery St. There is a sidewalk but there is no ADA ramp, curb, or gutter. Photo Map 2.



18. Looking north SR 65 from the east side of the intersection of SR 65 and W. Montgomery St. There is no sidewalk or ADA ramp, but there is a curb and gutter. Photo Map 2.

De No. 2001922

Photo Date: October 6, 2022



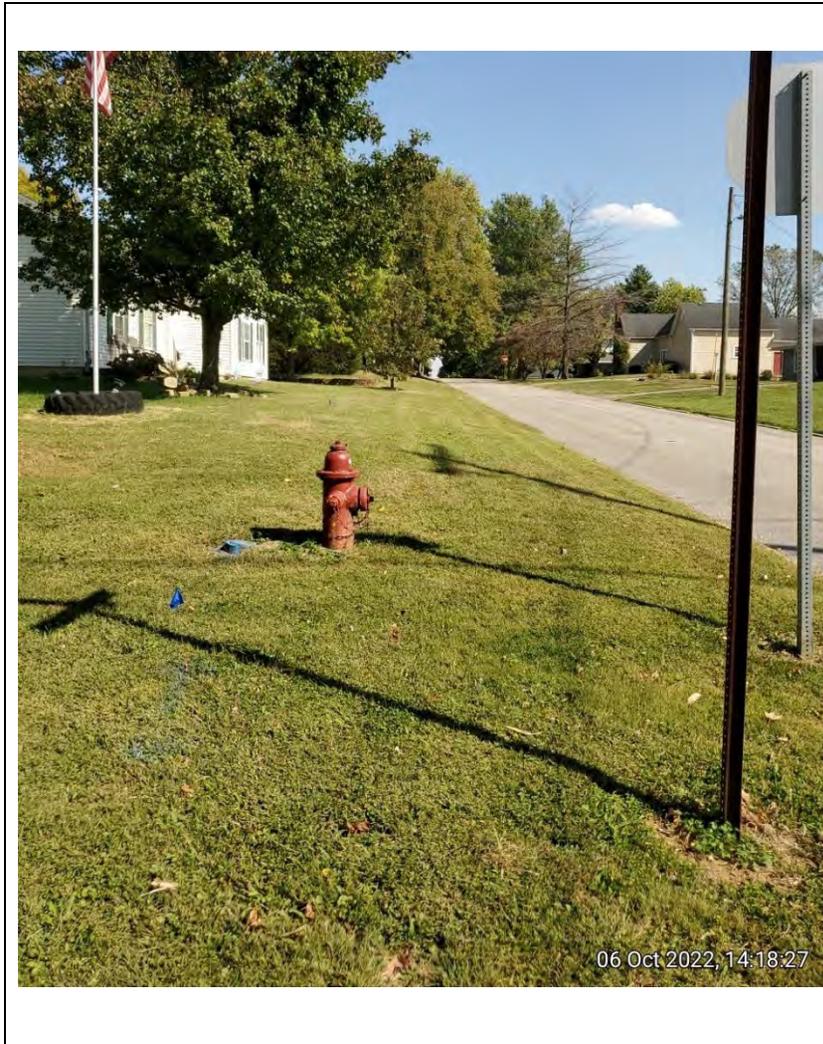
19. Looking south along the east side of SR 65 from the intersection of SR 65 and W. Montgomery St. There is no ADA ramp or gutter but there is a short curb visible. Photo Map 2.



20. Looking north along west side SR 65 from the intersection of SR 65 and W. Warrick St. There is access to municipal water at this location. No sidewalks, curbs, or gutters. Photo Map 3.

De No. 2001922

Photo Date: October 6, 2022



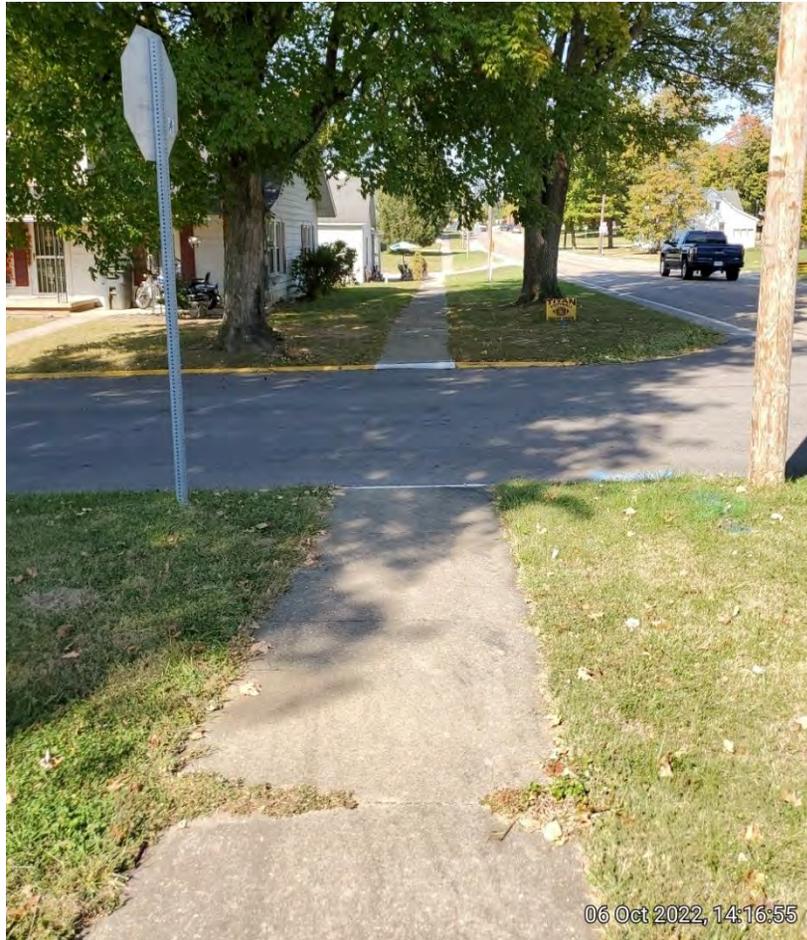
21. Looking east along the north side of W. Warrick St from the intersection of SR 65 and W. Warrick St. There is a hydrant with municipal water access at this intersection. There are no sidewalks, curbs or gutters at this location. Photo Map 3



22. Looking east along W. Warrick St from the intersection of SR 65 and W. Warrick St. There is a short curb, but no sidewalks and no gutters. Photo Map 3

De No. 2001922

Photo Date: October 6, 2022



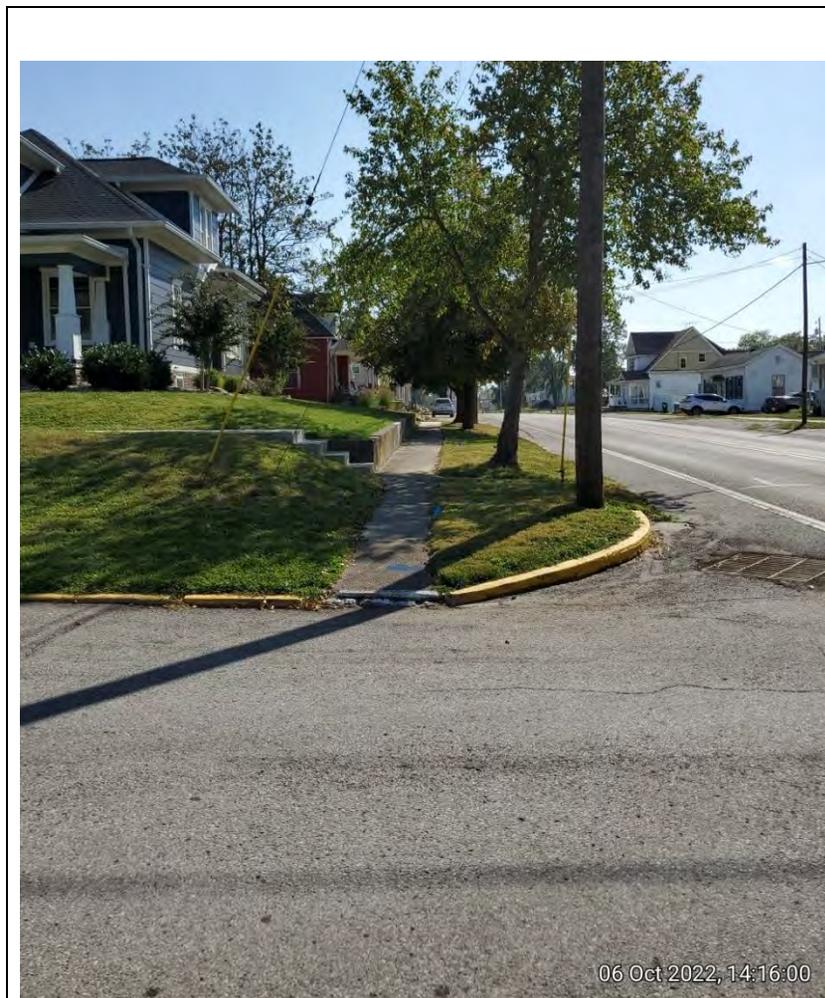
23. Looking east looking north along the west side of SR 65 from the intersection of SR 65 and W. Warrick. There are sidewalks but no ADA ramps, curbs but no gutters. Photo Map 3



24. Looking west along the south side of W. Warrick St from the intersection of SR 65 and W. Warrick. There are sidewalks but no ADA ramps, curbs, or gutters. There is a fire hydrant. Photo Map 3

De No. 2001922

Photo Date: October 6, 2022



25. Looking south along the east side of SR 65 from the intersection of SR 65 and W. Warrick St. There is a sidewalk but no ADA ramps, there is a curb but no gutters. There is an opening to a municipal storm drain in the roadway. Photo Map 3



26. Looking west along the north side of Walnut St from the intersection of SR 65 and Walnut St. There is a sidewalk but no ADA ramps, a curb but no gutters. Photo Map 4.

De No. 2001922

Photo Date: October 6, 2022

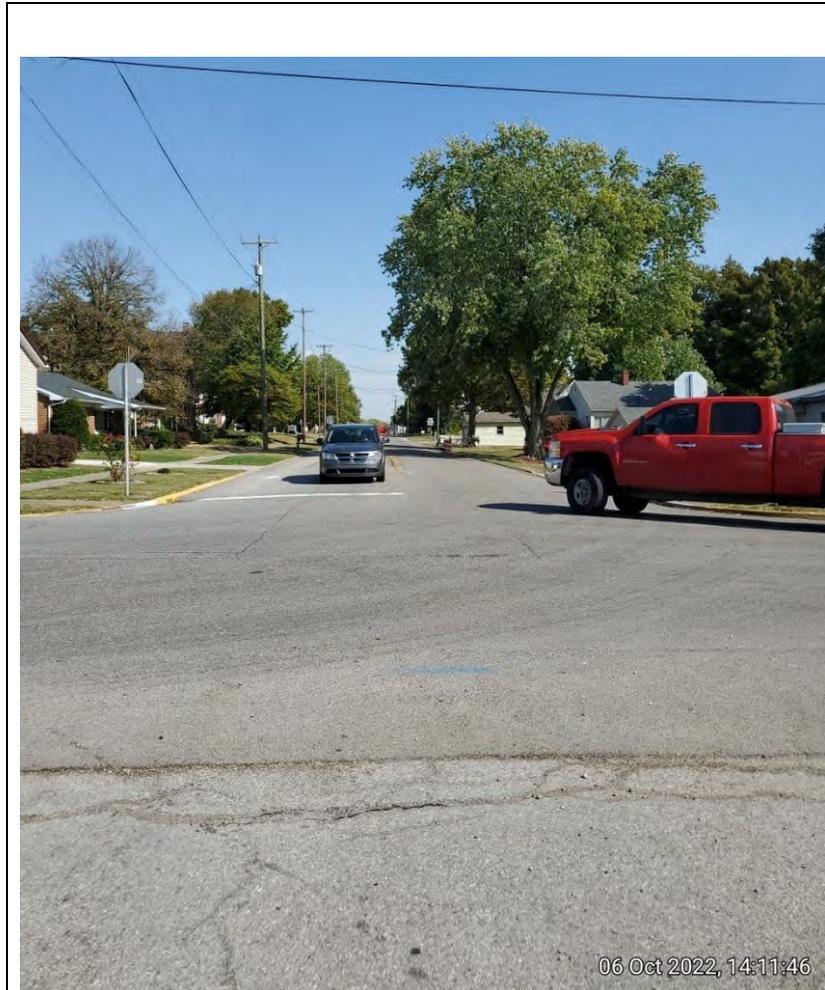


27. Looking north along SR 65 from the intersection of SR 65 and Walnut St. there are sidewalks on both sides. There are no curbs or gutters on either side. Photo Map 4.

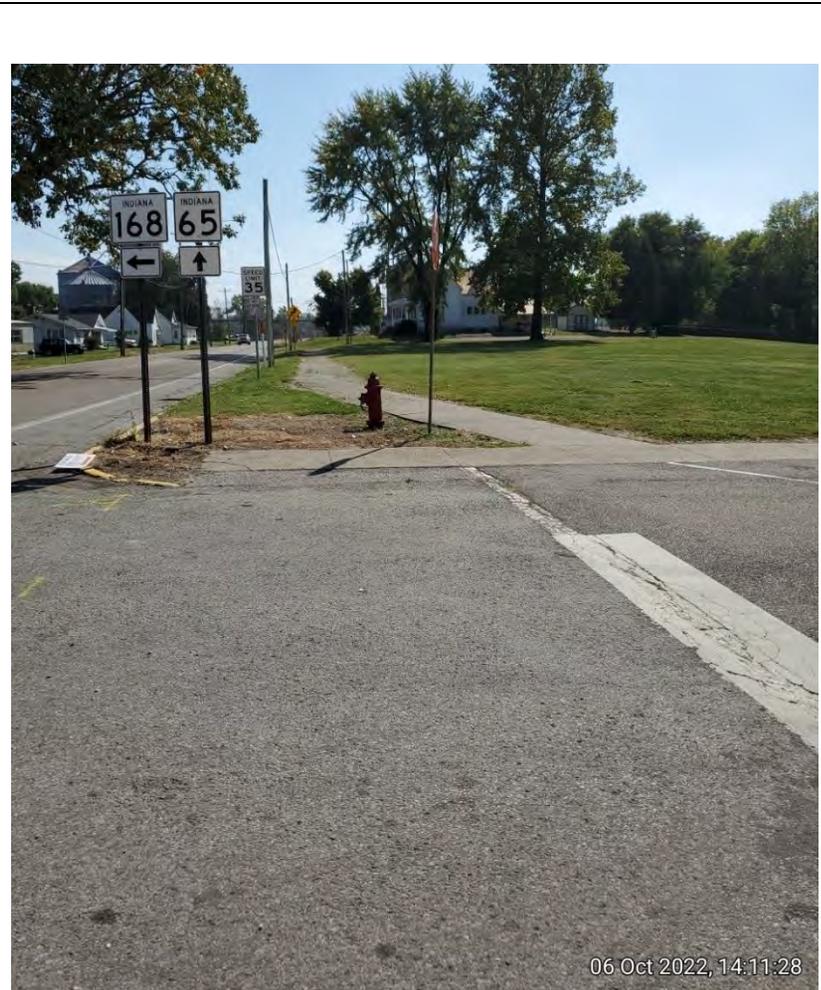
28. Looking east along the south side of Walnut St from the intersection of SR 65 and Walnut St. There are sidewalks but no ADA ramps. There are curbs but no gutters. Photo Map 4.

De No. 2001922

Photo Date: October 6, 2022



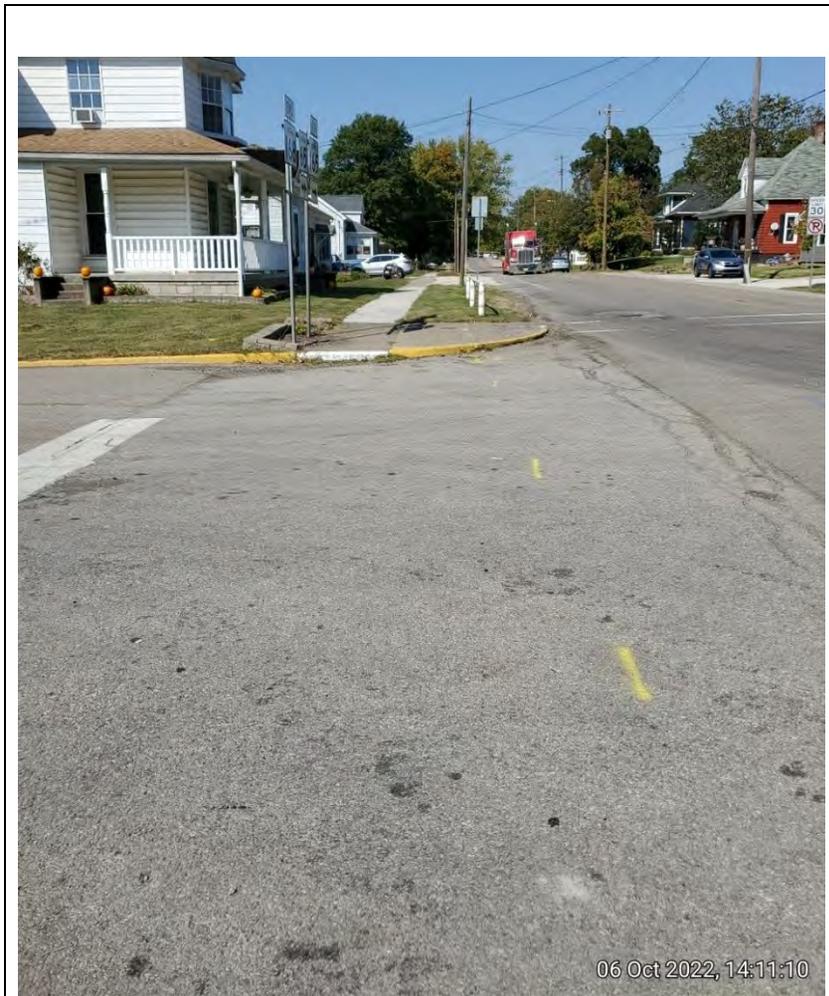
29. Looking east along Walnut St from the intersection of SR 65 and Walnut St. There are sidewalks (no ADA ramps), curbs and gutters on the north side. There are no sidewalks on the south side but there are curbs. Photo Map 4.



30. Looking south along the west side of SR 65 from the intersection of SR 65 and Walnut St. There are sidewalks but no ADA ramps. There is a very short curb. There are no gutters. There is a fire hydrant on this corner. Photo Map 4.

De No. 2001922

Photo Date: October 6, 2022



31. Looking north along the west side of SR 65 from the intersection of SR 65 and Walnut St. There are sidewalks but no ADA ramps. There are curbs but no gutters. Photo Map 4.



32. Looking south along the west side of SR 65 from the intersection of SR 65 and Walnut St. There are sidewalks but no ADA ramps. There is a very short curb. There are no gutters. There is a fire hydrant on this corner. Photo Map 4.

De No. 2001922

Photo Date: October 6, 2022



33. Looking south along the west side of SR 65. There is a sidewalk and low curb but no gutters. Photo Map 4.



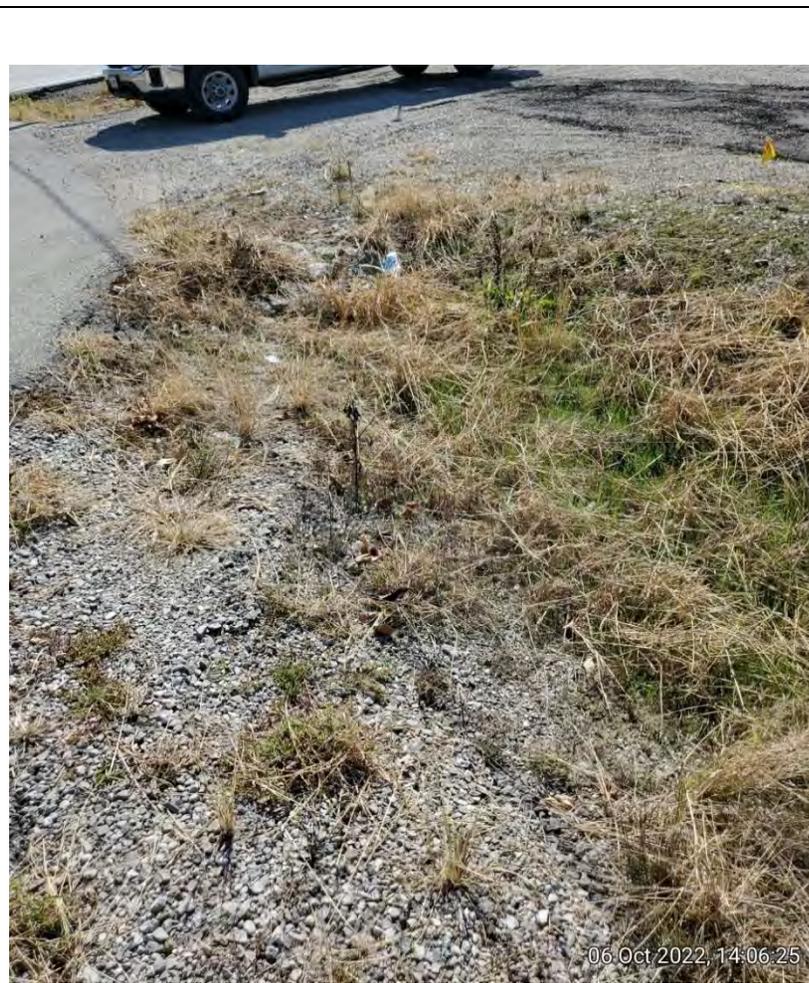
34. Looking south along the east side of SR 65. There is no sidewalk, curb, or gutters. There is a riprapped open drainage feature. Photo Map 4.

De No. 2001922

Photo Date: October 6, 2022



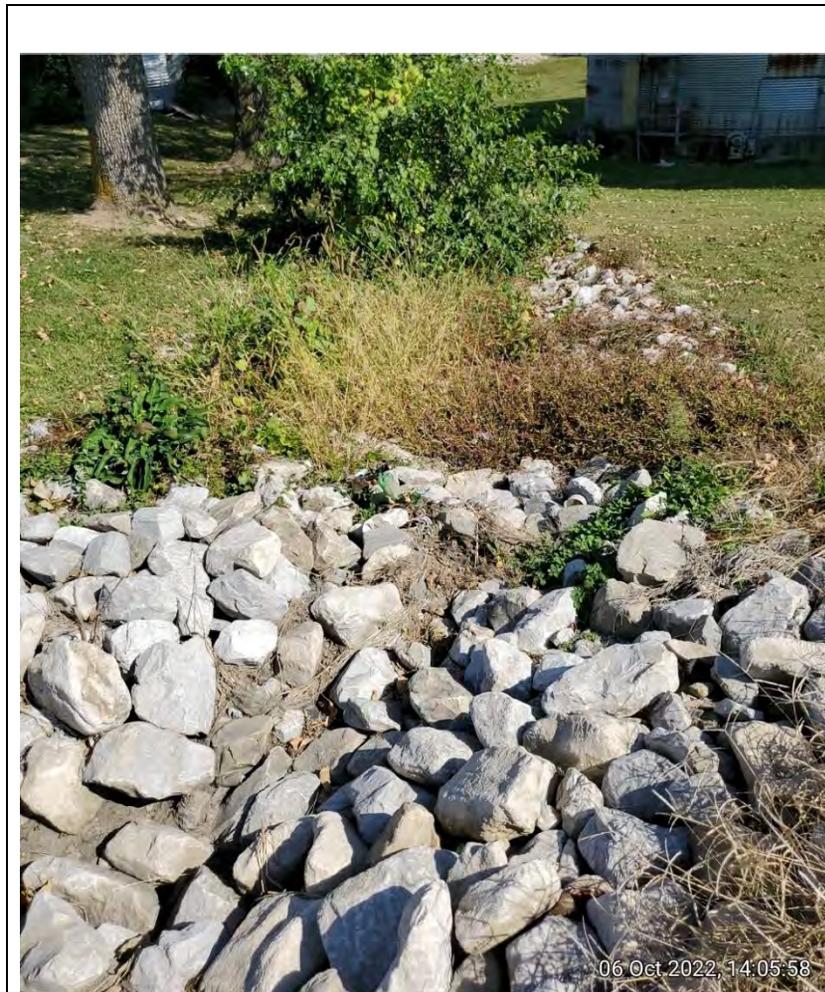
35. Looking north at open grassy veg between commercial drive and SR 65 at SR 65 curve south of Owensville. Photo Map 5.



36. Looking southwest at open grassy veg between commercial drive and SR 65 at SR 65 curve south of Owensville. Photo Map 5.

De No. 2001922

Photo Date: October 6, 2022



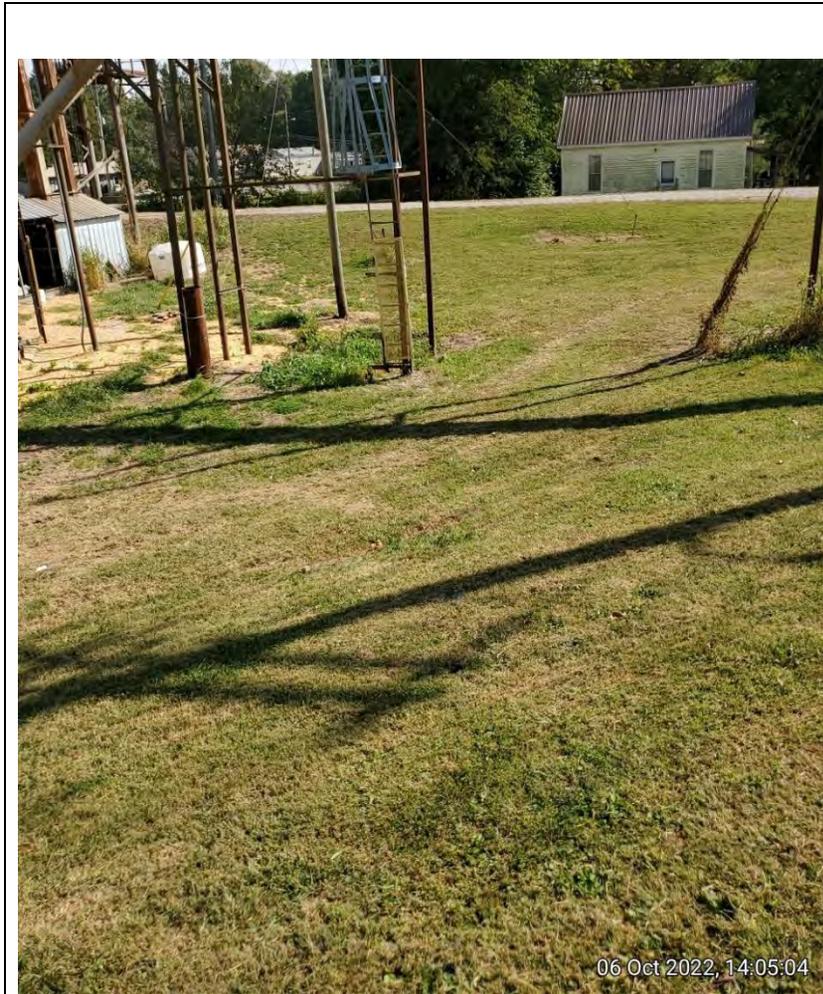
37. Looking east at open drainage feature on the east side of SR 65, just north of SR 65 and Mill St split south of Owensville. At the beginning of the north end of the curve. Photo Map 5.



38. Looking north on the east side of SR 65, just north of SR 65 and Mill St split south of Owensville. Along the beginning of the north end of the curve. Photo Map 5.

De No. 2001922

Photo Date: October 6, 2022



06 Oct 2022, 14:05:04

39. Looking southeast at the grassy area on the outside of the curve at the SR 65 and Mill St split south of Owensville. Photo Map 5.

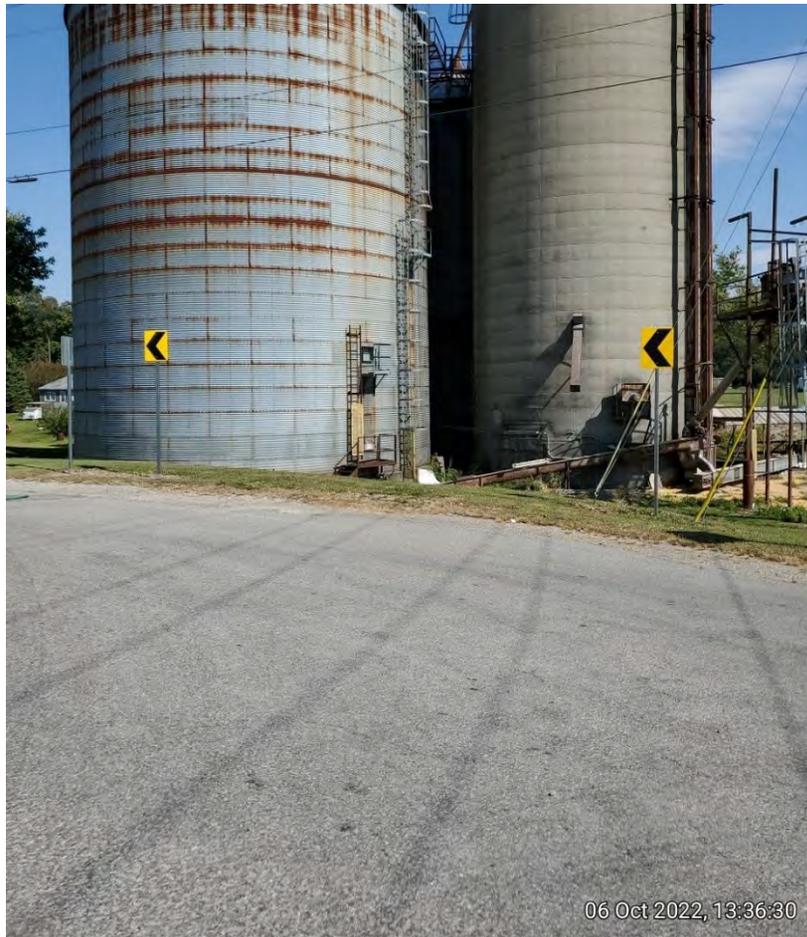


06 Oct 2022, 14:04:15

40. Looking north from the inside of the curve at the SR 65 and Mill St split south of Owensville. Photo Map 5.

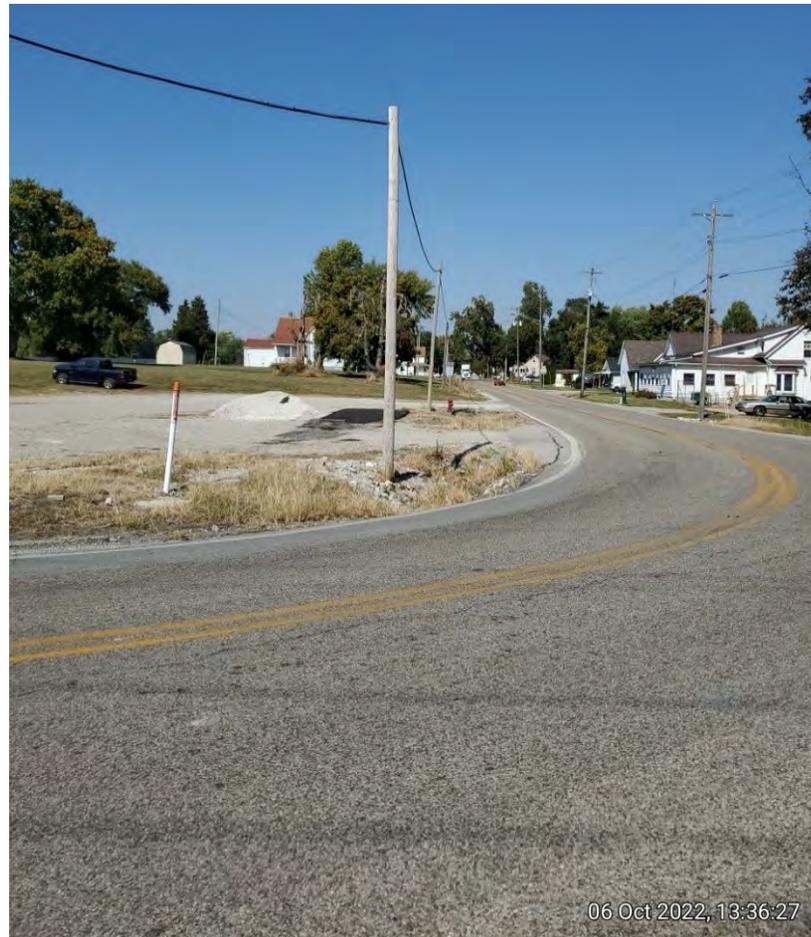
De No. 2001922

Photo Date: October 6, 2022



06 Oct 2022, 13:36:30

41. Looking northeast at the grain silos on the outside of the curve at the SR 65 and Mill St split south of Owensville. Photo Map 5.

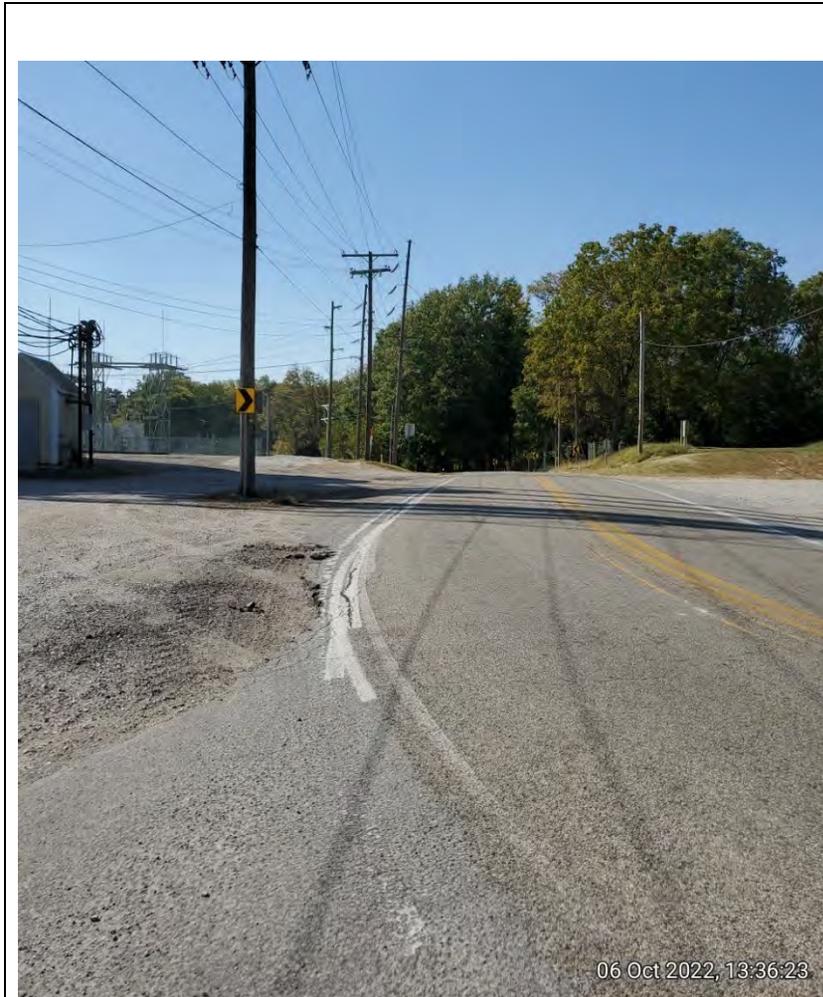


06 Oct 2022, 13:36:27

42. Looking north at the inside of the curve at the SR 65 and Mill St split south of Owensville. Photo Map 5.

De No. 2001922

Photo Date: October 6, 2022



43. Looking west along the south side of the SR 65 curve from the intersection of the SR 65 and Mill St split. This is a slight industrial corner with a grain storage and a grain truck filling/emptying station. Photo Map 5.



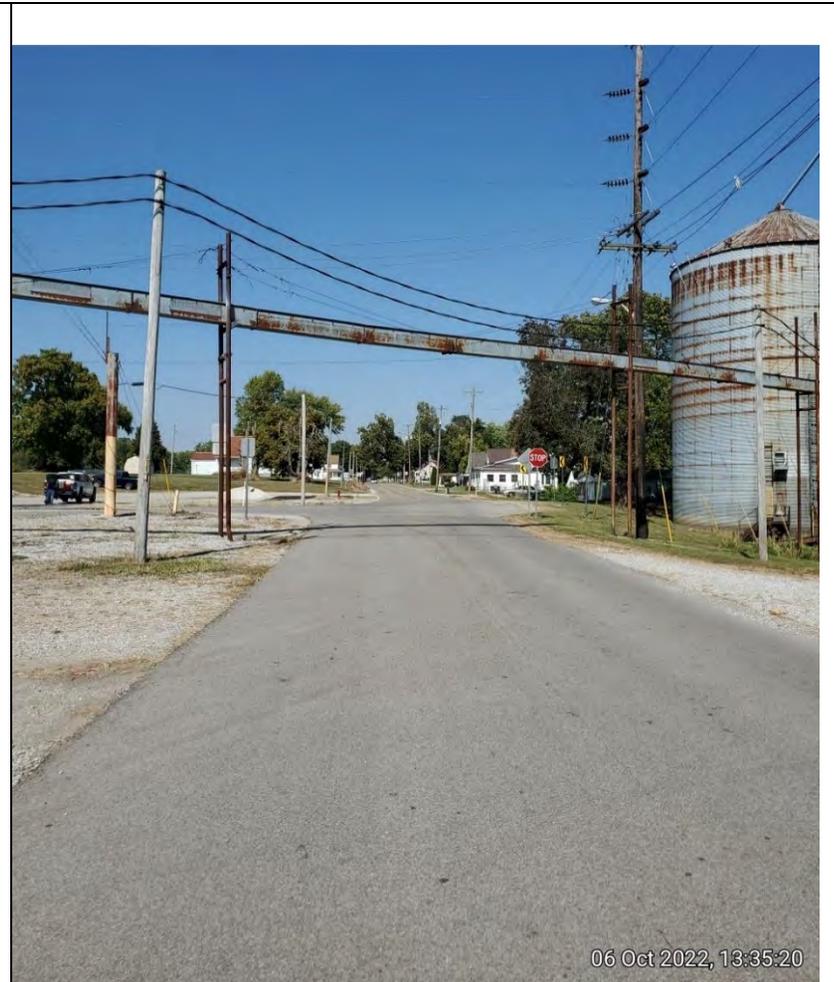
44. Looking east just south of the intersection of the SR 65 and Mill St split at the end of the investigated area. Photo Map 5.

De No. 2001922

Photo Date: October 6, 2022



45. Looking west just south of the intersection of the SR 65 and Mill St split at the end of the investigated area. Photo Map 5.



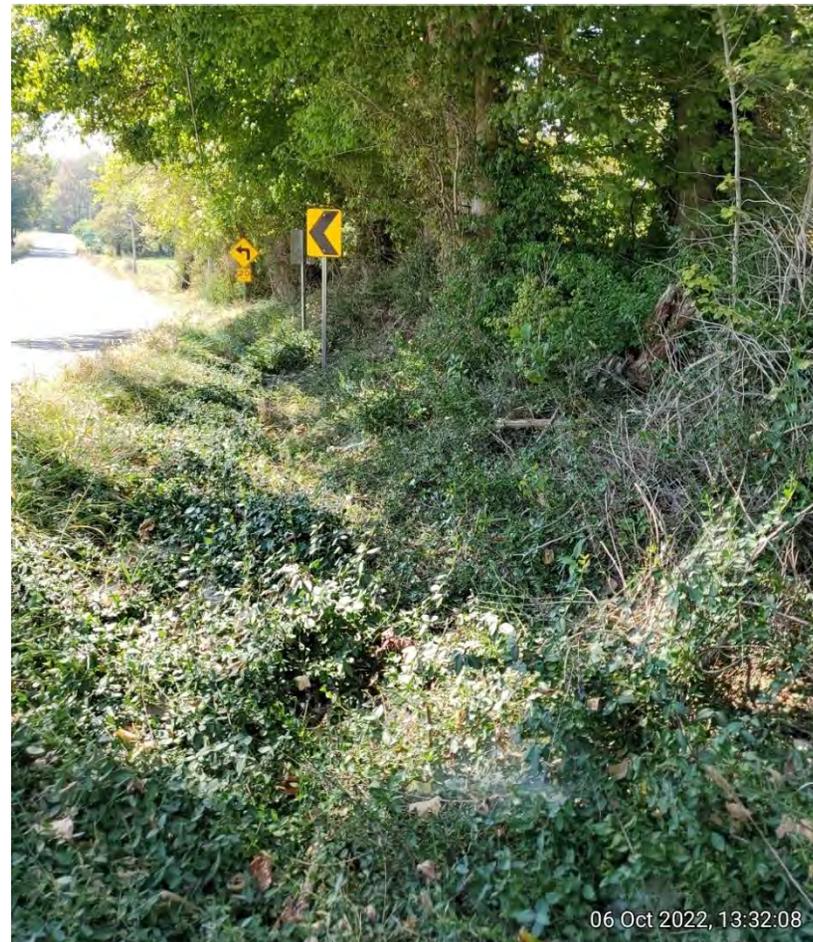
46. Looking north just south of the intersection of the SR 65 and Mill St split at the end of the investigated area. Photo Map 5.

De No. 2001922

Photo Date: October 6, 2022



47. Looking north at a gas utility junction on the northwest side of SR 65 south of Owensville. Photo Map 5.



48. Looking southwest down an open roadside drainage feature on the west side of SR 65 south of Owensville. There was no OHWM, these are not wetland plants. Photo Map 5.

De No. 2001922

Photo Date: October 6, 2022



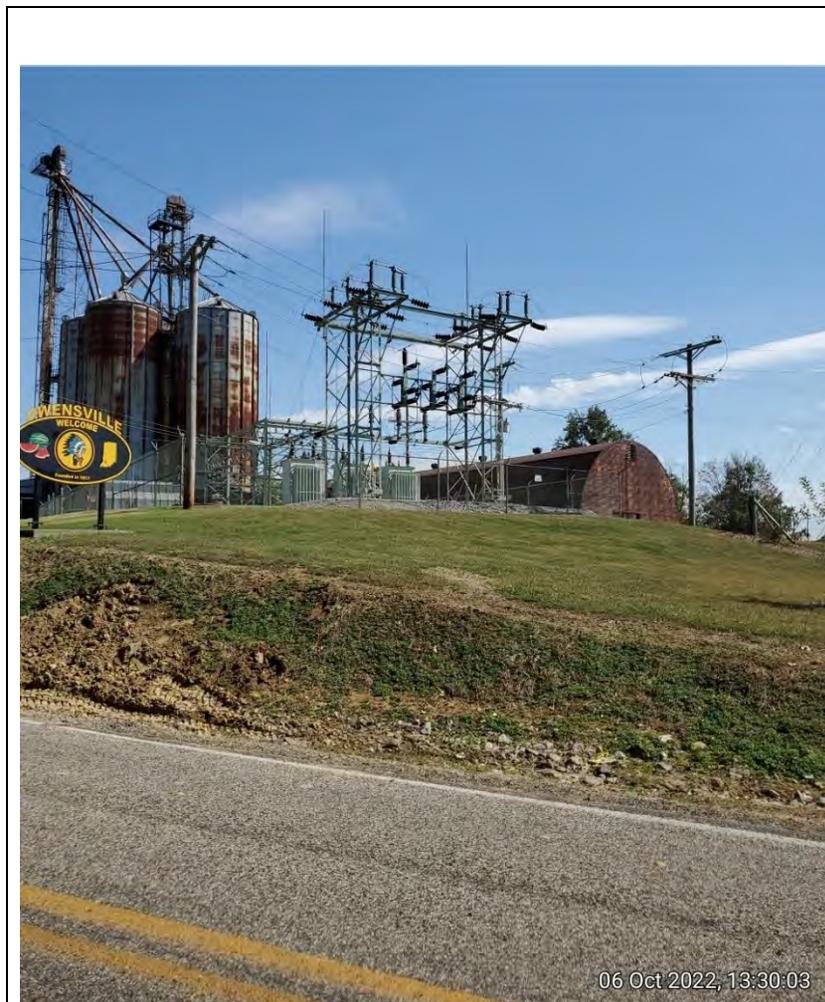
49. Looking southwest at an open roadside drainage feature on the east side of SR 65 south of Owensville. Photo Map 5.



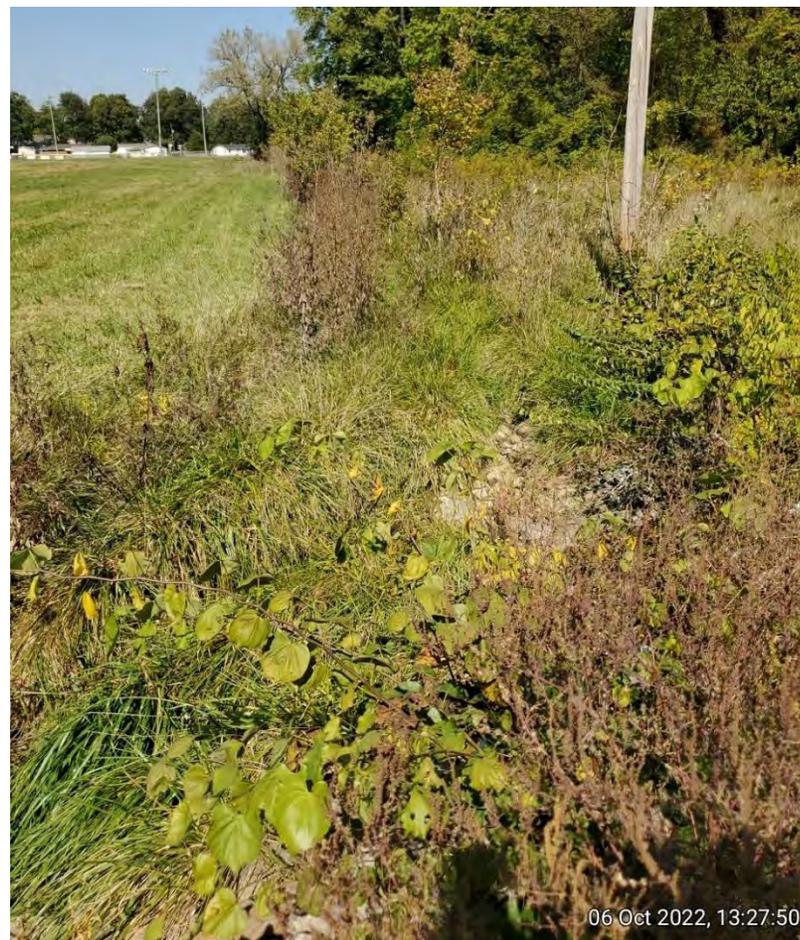
50. Looking northeast at an open roadside drainage feature on the east side of SR 65 south of Owensville. Photo Map 5.

De No. 2001922

Photo Date: October 6, 2022



51. Looking southeast at the electrical substation south of the town of Owensville on the southeast side of SR 65. Photo Map 5.



52. Looking north an open roadside drainage feature on the east side of SR 65 south of Owensville. There was no OHWM, these are not wetland plants. Photo Map 5.

De No. 2001922

Photo Date: October 6, 2022



53. Looking southeast at the residential area on the east side of SR 65 at the end of the investigated area. Photo Map 5.



54. Looking northwest at the open field on the west side of SR 65 at the end of the investigated area. Photo Map 5.

PROJECT	DESIGNATION
2001922	2001922
CONTRACT	
R-43253	

KIN DESIGNATION NUMBERS	
2001922 (LEAD)	SR 65 Reconstruction
1900271	SR 165 Minor Structural Overlay
2001920	SR 168 HMA Overlay
2400688	SR 65 / SR 165 / SR 168 Sidewalk Construction

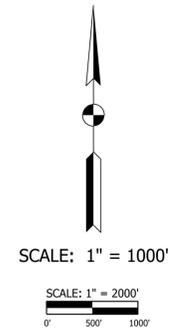
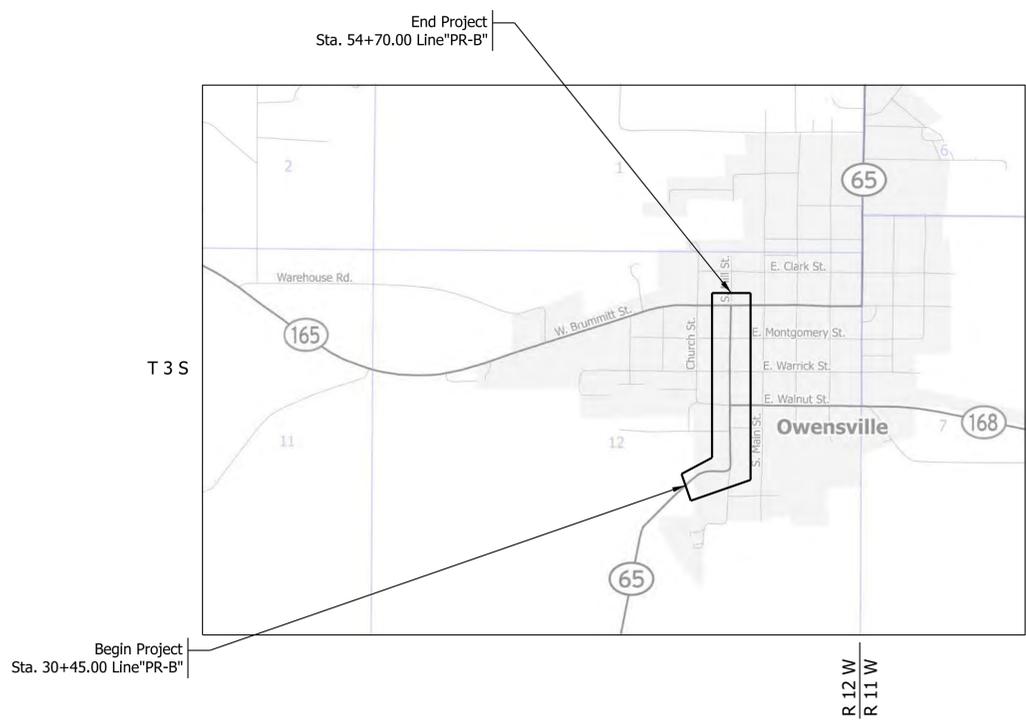
INDIANA DEPARTMENT OF TRANSPORTATION



ROAD PLANS

ROUTE: SR 65 FROM: RP 19+85 TO: RP 20+27
PROJECT NO. 2001922 P.E.
R/W
CONST.

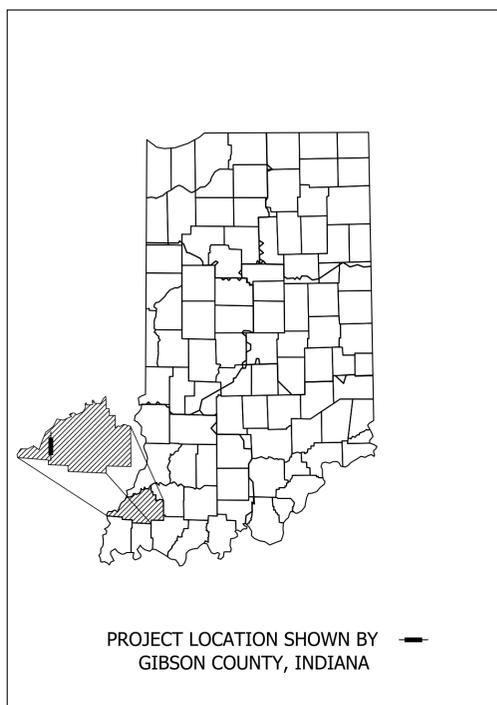
Pavement Reconstruction and Sidewalk Replacement
From: 0.26 mi. S. SR 168 (S. Limits Owensville) To: SR 165
Section 12, T-3-S, R-12-W, Montgomery Township, Gibson County, Indiana



TRAFFIC DATA		
A.A.D.T. (2025)		2620 V.P.D.
A.A.D.T. (2045)		3668 V.P.D.
D.H.V. (2045)		454 V.P.H.
DIRECTIONAL DISTRIBUTION		51/49 %
TRUCKS		10 % A.A.D.T. 4.5 % D.H.V.

DESIGN DATA - RESURFACE	
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	PARTIAL 3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MAJOR COLLECTOR
URBAN/RURAL	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	NONE

DESIGN DATA - RECONSTRUCTION	
DESIGN SPEED	20, 25, 30, 35 M.P.H.
PROJECT DESIGN CRITERIA	4R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MAJOR COLLECTOR
URBAN/RURAL	URBAN
TERRAIN	LEVEL
ACCESS CONTROL	NONE



LATITUDE: 36° 16' 10" N LONGITUDE: 87° 41' 34" W

GROSS LENGTH:	0.47 MI.
NET LENGTH:	0.47 MI.
MAX. GRADE:	N/A %

HUC: 051201130503
HUC: 051201130501

STAGE 2 PLANS: 02/25/2025

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2024
TO BE USED WITH THESE PLANS



WSP USA Inc.
115 W. Washington Street
Suite 1270S
Indianapolis, IN 46204
TEL: 317-972-1706

PLANS PREPARED BY: _____ PHONE NUMBER _____
CERTIFIED BY: _____ DATE _____
APPROVED FOR LETTING: _____ INDIANA DEPARTMENT OF TRANSPORTATION DATE _____

DESIGNATION	
2001922	
SHEETS	
1	of 114
PROJECT	
R-43253	
2001922	

Plot: 2/24/2025 12:36 PM

- 7 SB Sidewalk
5' to 6.5' - Sta. 48+22 to Sta. 48+30
6.5' - Sta. 48+30 to Sta. 48+70
6.5' to 5' - Sta. 48+70 to Sta. 49+01

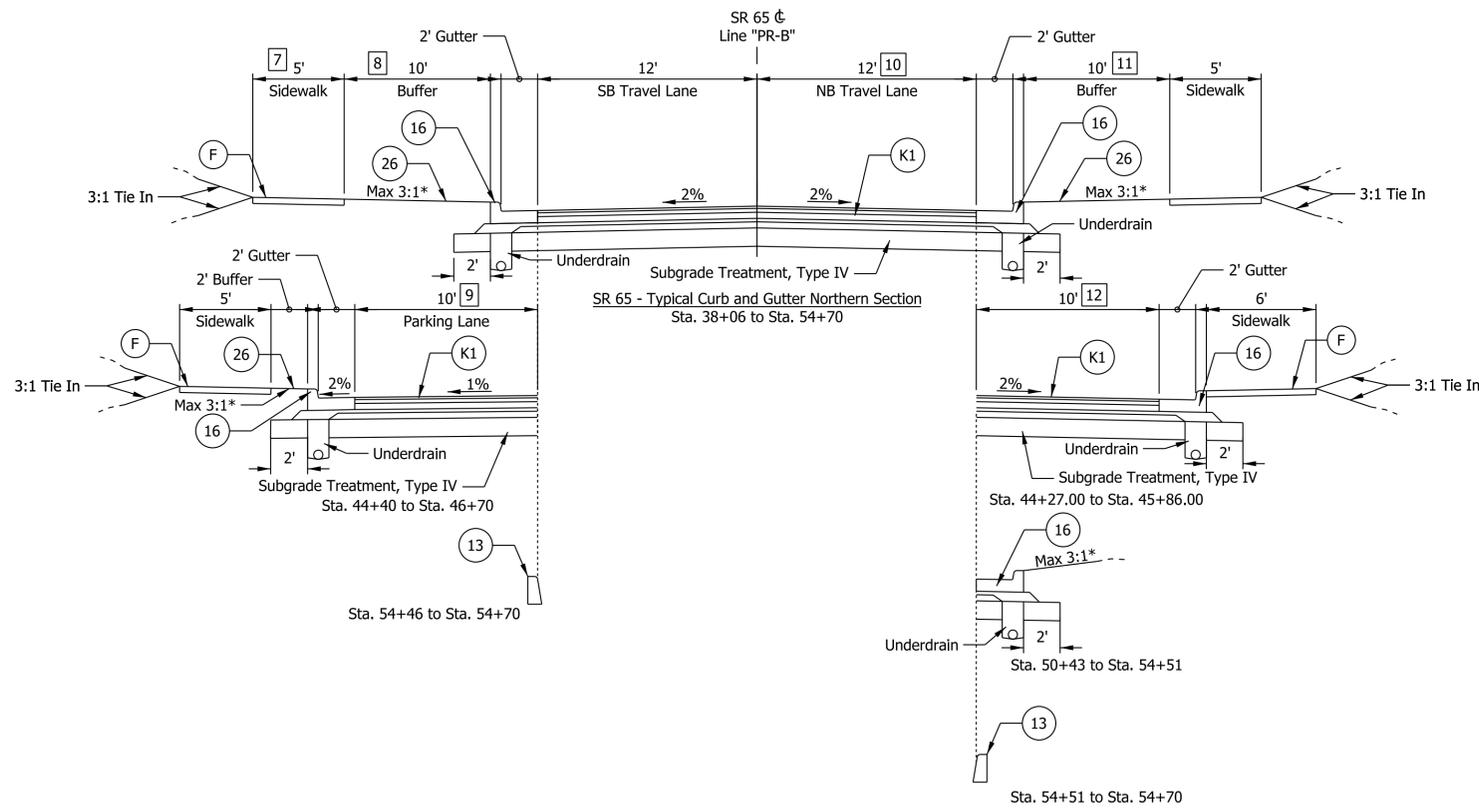
- 8 SB Buffer
10' to 9.5' - Sta. 41+40 to Sta. 41+50
9.5' - Sta. 41+50 to Sta. 42+80
9.5' to 10' - Sta. 42+80 to Sta. 42+90
10' to 2' - Sta. 44+40 to Sta. 44+60
2' - Sta. 44+60 to Sta. 46+50
2' to 10' - Sta. 46+50 to Sta. 46+70
11' - Sta. 47+46 to Sta. 48+70
8' - Sta. 50+84 to Sta. 53+04
8' to 10' - Sta. 53+04 to Sta. 53+15

- 9 SB Parking Lane
0' to 9' - Sta. 44+40 to Sta. 44+60
9' - Sta. 44+60 to Sta. 46+50
9' to 0' - Sta. 46+50 to Sta. 46+70

- 10 NB Travel Lane
12' to 14' - Sta. 42+38 to Sta. 42+94
18' to 14' - Sta. 43+87 to Sta. 44+27
Right Turn Lane:
12' to 21' - Sta. 51+42 to Sta. 51+62
21' - Sta. 51+62 to Sta. 54+12

- 11 NB Buffer
10' to 4' - Sta. 42+22 to Sta. 42+93
4.5' to 7.5' - Sta. 43+87 to Sta. 44+27
0' - Sta. 44+75 to Sta. 45+48
0' to 10' - Sta. 45+48 to Sta. 45+86

- 12 NB Parking Lane
14' to 22' - Sta. 44+27 to Sta. 44+77
22' - Sta. 44+75 to Sta. 45+48
22' to 12' - Sta. 45+48 to Sta. 45+86



*Varies. Max. Value Provided. See Cross Sections for additional details.

LEGEND	
(F) Sidewalk, Concrete	(K1) Full Depth Reconstruction with HMA: 165 lb/sys QC/QA-HMA, 3, 58H, Surface, 9.5 mm on 275 lb/sys QC/QA-HMA, 3, 58H, Intermediate, 19.0 mm on 440 lb/sys QC/QA-HMA, 3, 58S, Base, 19.0 mm on Drainage Layer Consisting of 250 lb/sys QC/QA-HMA, 4, 58E, Intermediate, OG, 19.0 mm on Separation Layer Consisting of 4.0 in. Compacted Aggregate, No. 53 on Subgrade Treatment, Type IC on Geogrid Type IB
(13) Curb, Concrete	(K2) HMA for Shoulder Widening: 165 lb/sys QC/QA-HMA, 3, 58H, Surface, 9.5 mm on 275 lb/sys QC/QA-HMA, 3, 58H, Intermediate, 19.0 mm on 440 lb/sys QC/QA-HMA, 3, 58S, Base, 19.0 mm on
(16) Curb and Gutter, Concrete	(O) Compacted Aggregate, No. 53
	(R) Mill and Overlay for Incidental Construction 1.5" Asphalt Milling 165 lb/sys QC/QA-HMA, 3, 58H, Surface, 9.5 mm on existing pavement
	(17) Curb and Gutter, Concrete, 4"

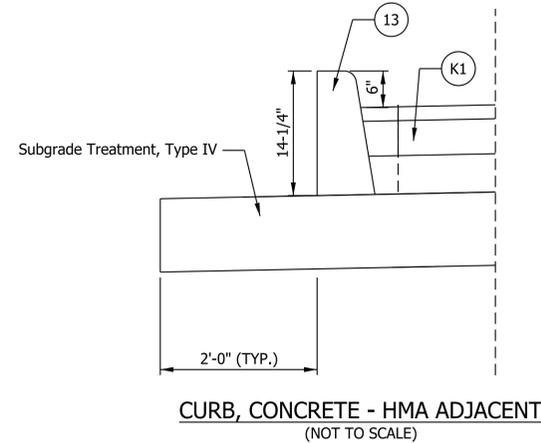
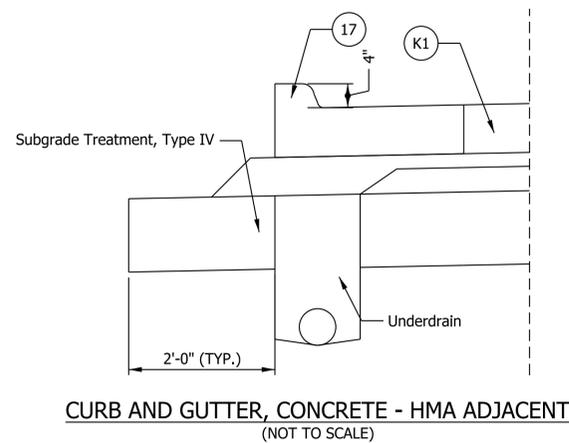
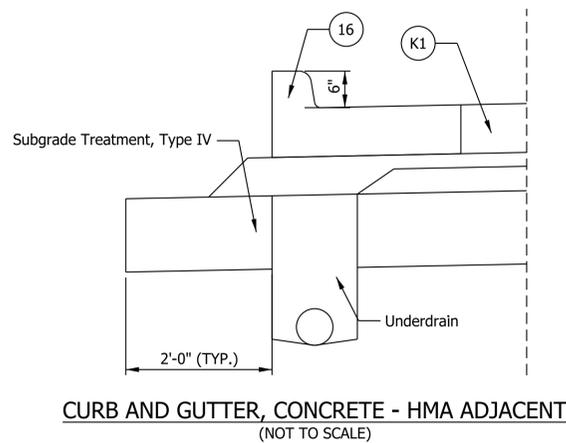
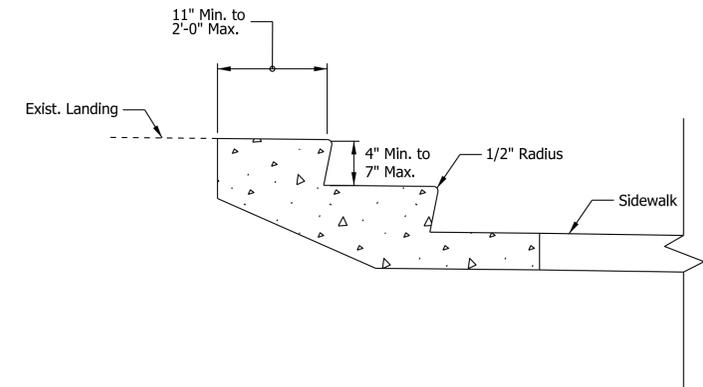
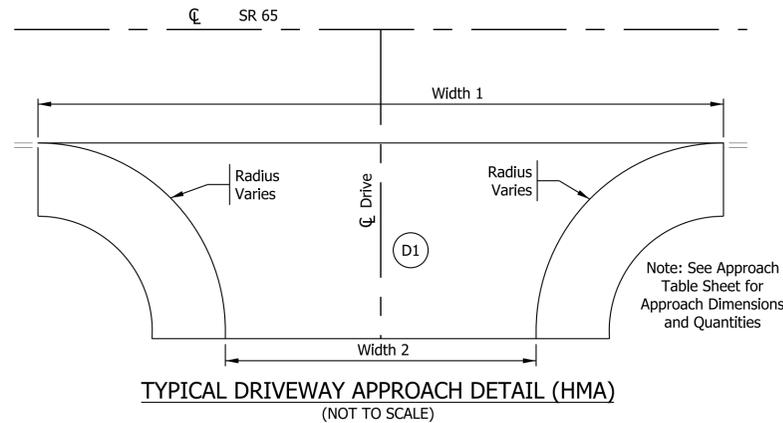
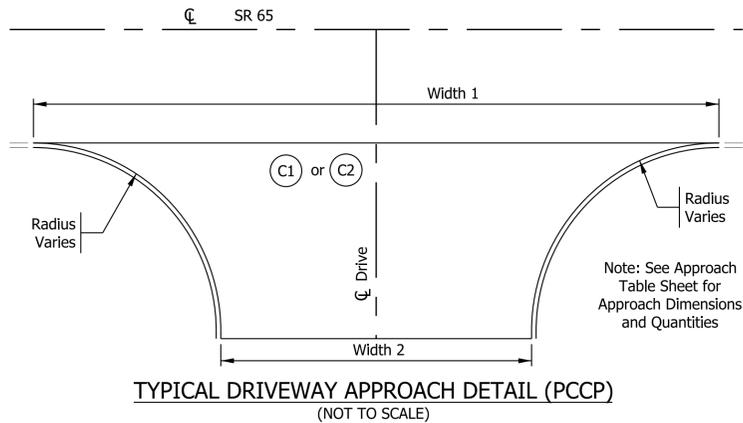
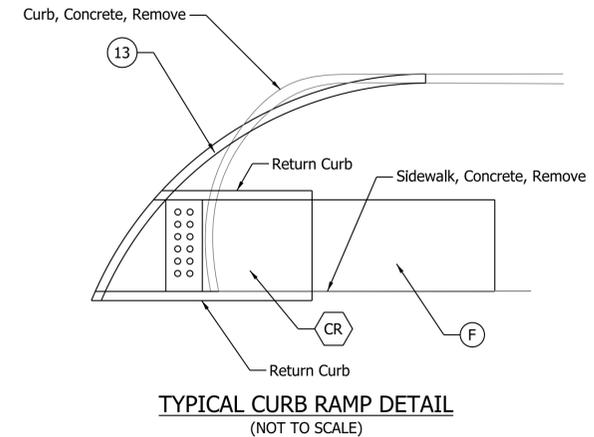
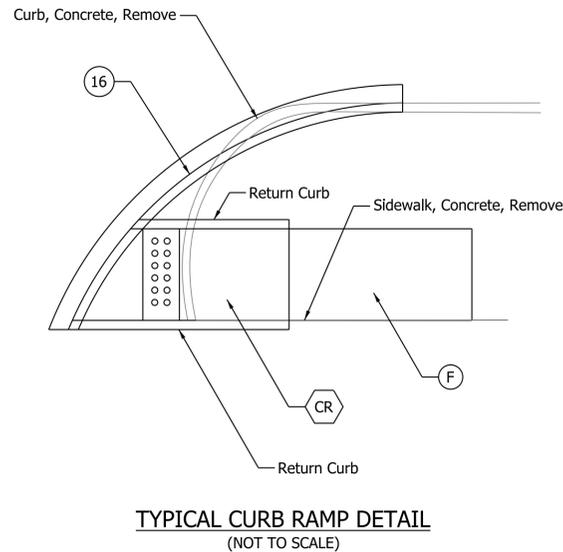
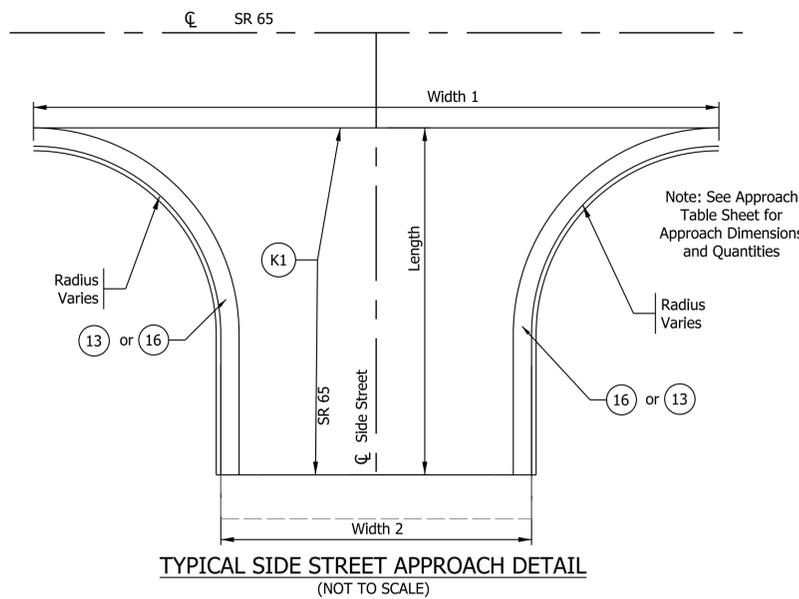
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS	

HORIZONTAL SCALE	BRIDGE FILE
1" = 5'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	4 of 114
CONTRACT	PROJECT
R-43253	2001922

Plot: 2/26/2025 2:44 PM IP_PWP:d0443600\ShT RD TS_02.dgn



Plc: 2/26/2025 2:44 PM
 IP_PWP:d0443600\SH RD Misc.dgn

LEGEND			
(F) Sidewalk, Concrete	(K1) Full Depth Reconstruction with HMA: 165 lb/sys QC/QA-HMA, 3, 58H, Surface, 9.5 mm on 275 lb/sys QC/QA-HMA, 3, 58H, Intermediate, 19.0 mm on 440 lb/sys QC/QA-HMA, 3, 58S, Base, 19.0 mm on Drainage Layer Consisting of 250 lb/sys QC/QA-HMA, 4, 58E, Intermediate, OG, 19.0 mm on Separation Layer Consisting of 4.0 in. Compacted Aggregate, No. 53 on Subgrade Treatment, Type IC on Geogrid Type IB	(C1) PCCP for Approaches, 6 IN. on Dense Graded Subbase, 6" on Subgrade Treatment, Type II	(D1) HMA for Approaches, Type B composed of: 165 lb/syd HMA Surface Type B, on 275 lb/syd HMA, Intermediate, Type B, on 660 lb/syd HMA Base, Type B, on Subgrade Treatment, Type II on Geogrid, Type IB
(13) Curb, Concrete		(C2) PCCP for Approaches, 9 IN. on Dense Graded Subbase, 6" on Geogrid, Type IB	(CR) Curb Ramp, Concrete
(16) Curb and Gutter, Concrete			
(17) Curb and Gutter, Concrete, 4"			

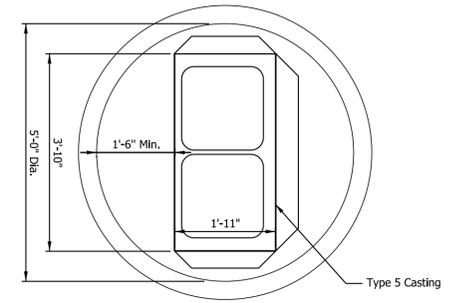
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: HEW _____	DRAWN: HEW _____	
CHECKED: BEA _____	CHECKED: BEA _____	

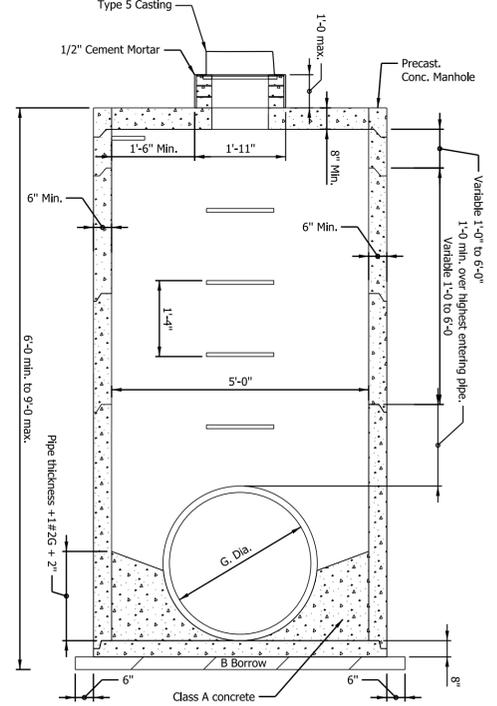
INDIANA DEPARTMENT OF TRANSPORTATION	
MISCELLANEOUS DETAILS	

HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	5 of 114
CONTRACT	PROJECT
R-43253	2001922

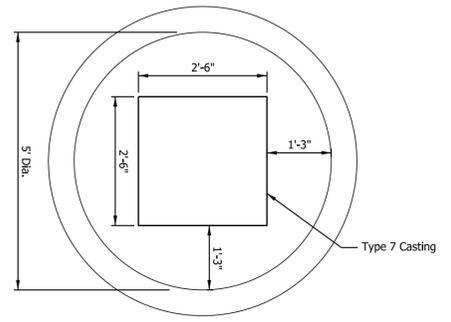
MANHOLE PIPE SIZES				
Type	G (in.)	F (ft. in.)	Maximum Pipe Size Rt. Angle to Mainline (in.)	Maximum Pipe Size for Mainline (in.)
H	24 to 36	-----	30	36
J	24 to 36	5'-0	30	36
K	36 to 48	6'-0	36	48
L	48 to 54	6'-0	48	54
M	54 to 72	6'-6	66	72
N	72 to 84	9'-0	72	84



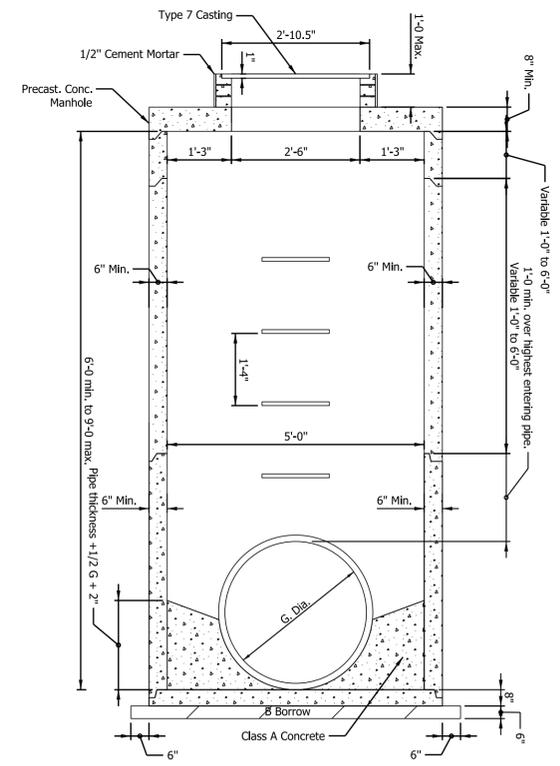
PLAN VIEW
MOD. COVER CAP FOR PRECAST MANHOLE, TYPE J-5
(NOT TO SCALE)



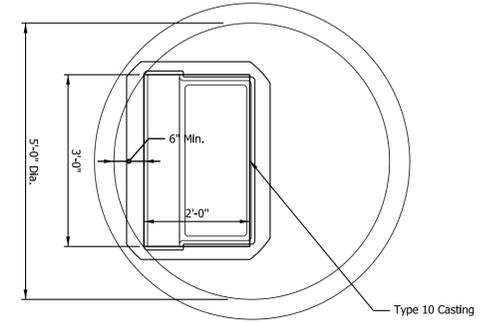
MOD. MANHOLE TYPE J-5



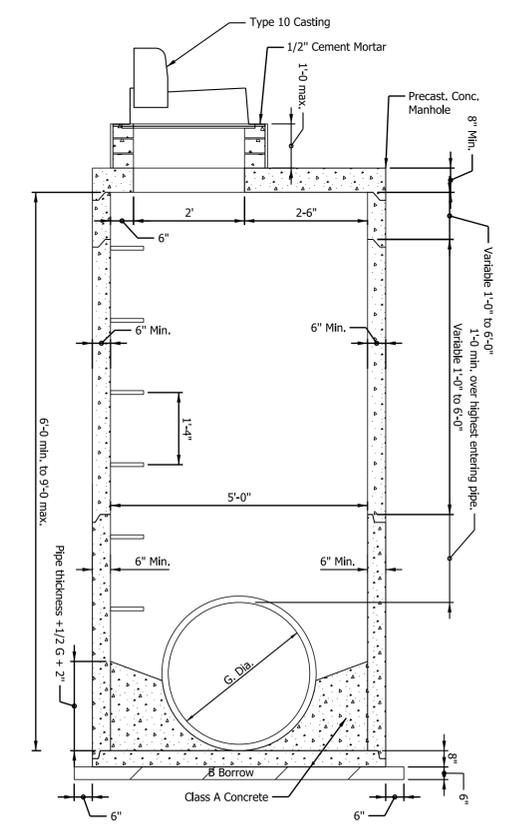
PLAN VIEW
MOD. COVER CAP FOR PRECAST MANHOLE, TYPE J-7
(NOT TO SCALE)



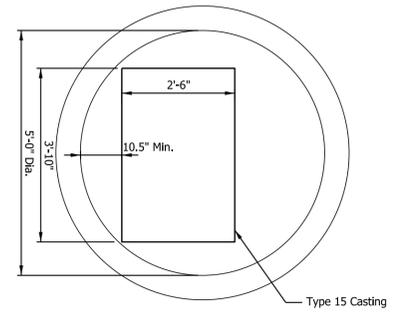
MOD. MANHOLE TYPE J-7
Elevation View
(NOT TO SCALE)



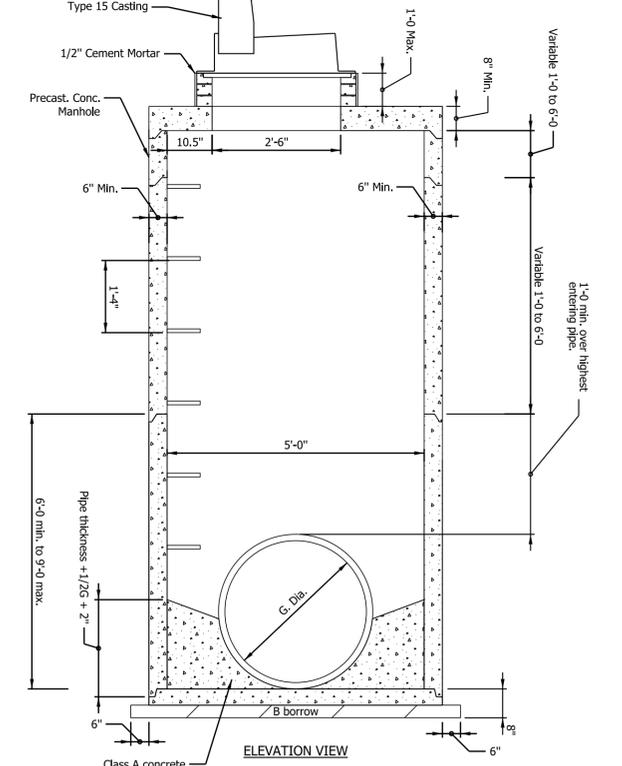
PLAN VIEW
MOD. COVER CAP FOR PRECAST MANHOLE, TYPE J-10
(NOT TO SCALE)



MOD. MANHOLE TYPE J-10
Elevation View
(NOT TO SCALE)



PLAN VIEW
MOD. COVER CAP FOR PRECAST MANHOLE, TYPE J-15
(NOT TO SCALE)



MOD. MANHOLE TYPE J-15
ELEVATION VIEW
(NOT TO SCALE)

Plot: 2/24/2025 1:44 PM

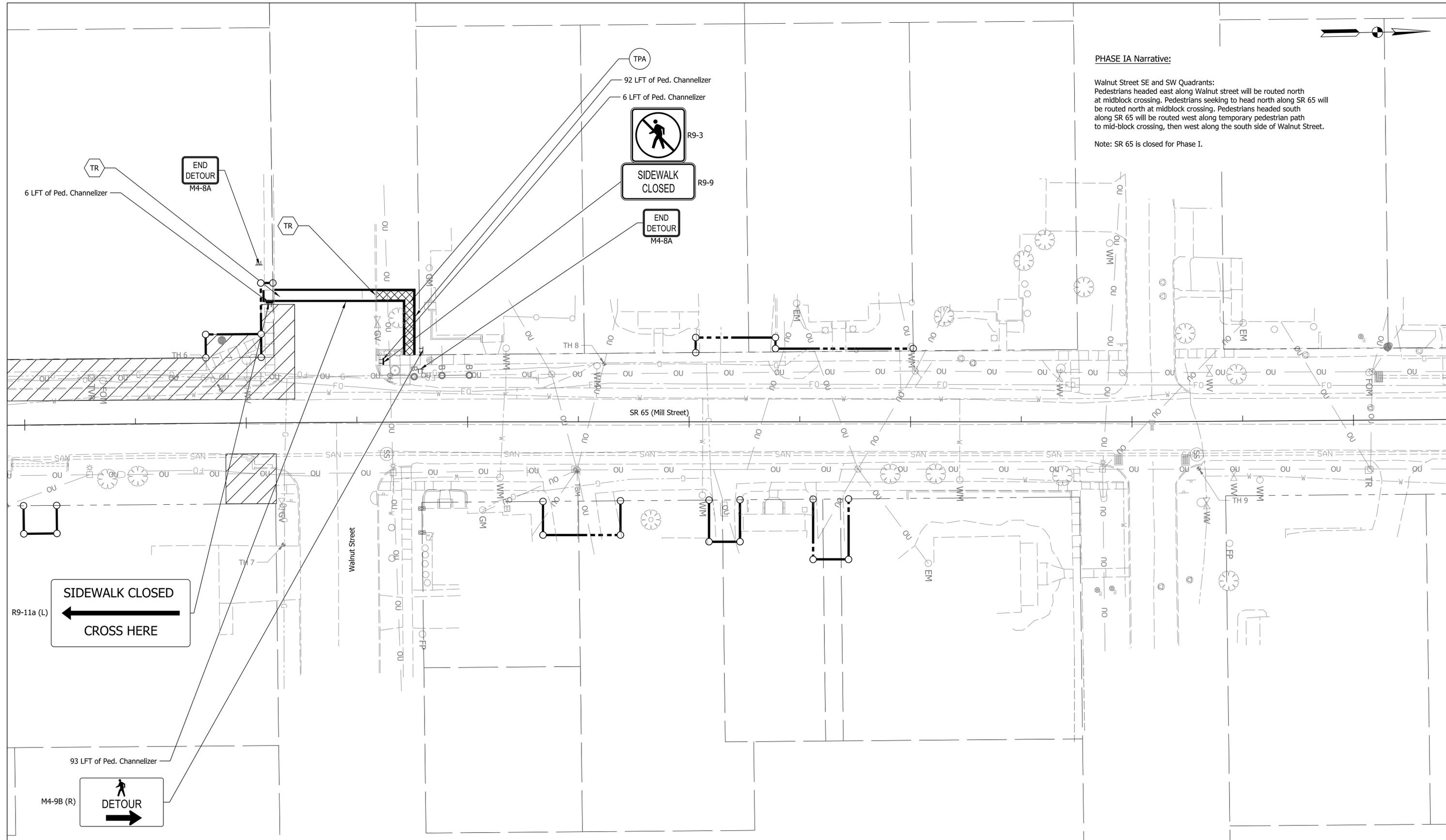
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JAF	DRAWN: JAF	
CHECKED: BEA	CHECKED: BEA	

INDIANA
DEPARTMENT OF TRANSPORTATION

DRAINAGE DETAILS 02

HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	6 of 114
CONTRACT	PROJECT
R-43253	2001922



PHASE IA Narrative:

Walnut Street SE and SW Quadrants:
 Pedestrians headed east along Walnut street will be routed north at midblock crossing. Pedestrians seeking to head north along SR 65 will be routed north at midblock crossing. Pedestrians headed south along SR 65 will be routed west along temporary pedestrian path to mid-block crossing, then west along the south side of Walnut Street.

Note: SR 65 is closed for Phase I.

Note: Phasing matches previous maintenance of traffic sheets.

LEGEND	
	Construction Signs
	Work Zone
	Pedestrian Channelizer
	Temporary Curb Ramp
	Temporary Accessible Pedestrian Path
	Temporary Traverse Pavement Marking, Removable, 6 IN., White
	Channelizing Device

NOT FOR CONSTRUCTION

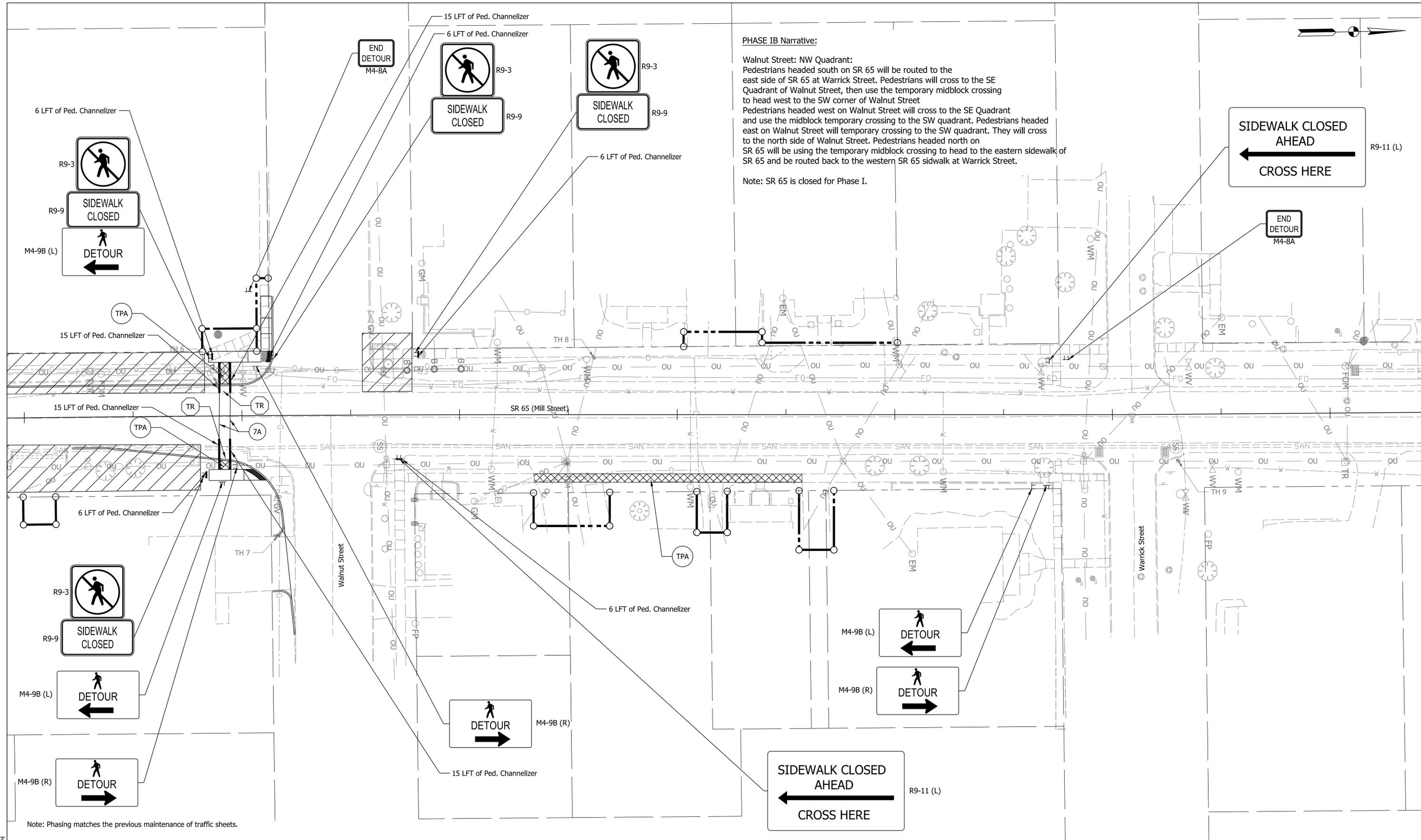
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE IA- WALNUT SE/SW QUAD

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	14 of 114
CONTRACT	PROJECT
R-43253	2001922

Plot: 2/26/2025 2:47 PM IP_PWP:d0443562\Sht Ped MOT_01.dgn



PHASE IB Narrative:

Walnut Street: NW Quadrant:
 Pedestrians headed south on SR 65 will be routed to the east side of SR 65 at Warrick Street. Pedestrians will cross to the SE Quadrant of Walnut Street, then use the temporary midblock crossing to head west to the SW corner of Walnut Street. Pedestrians headed west on Walnut Street will cross to the SE Quadrant and use the midblock temporary crossing to the SW quadrant. Pedestrians headed east on Walnut Street will use temporary crossing to the SW quadrant. They will cross to the north side of Walnut Street. Pedestrians headed north on SR 65 will be using the temporary midblock crossing to head to the eastern sidewalk of SR 65 and be routed back to the western SR 65 sidewalk at Warrick Street.

Note: SR 65 is closed for Phase I.

Note: Phasing matches the previous maintenance of traffic sheets.

2/26/2025 2:48 PM

LEGEND	
	Construction Signs
	Work Zone
	Pedestrian Channelizer
	Temporary Curb Ramp
	Channelizing Device
	Temporary Accessible Pedestrian Path
	Temporary Traverse Pavement Marking, Removable, 6 IN., White

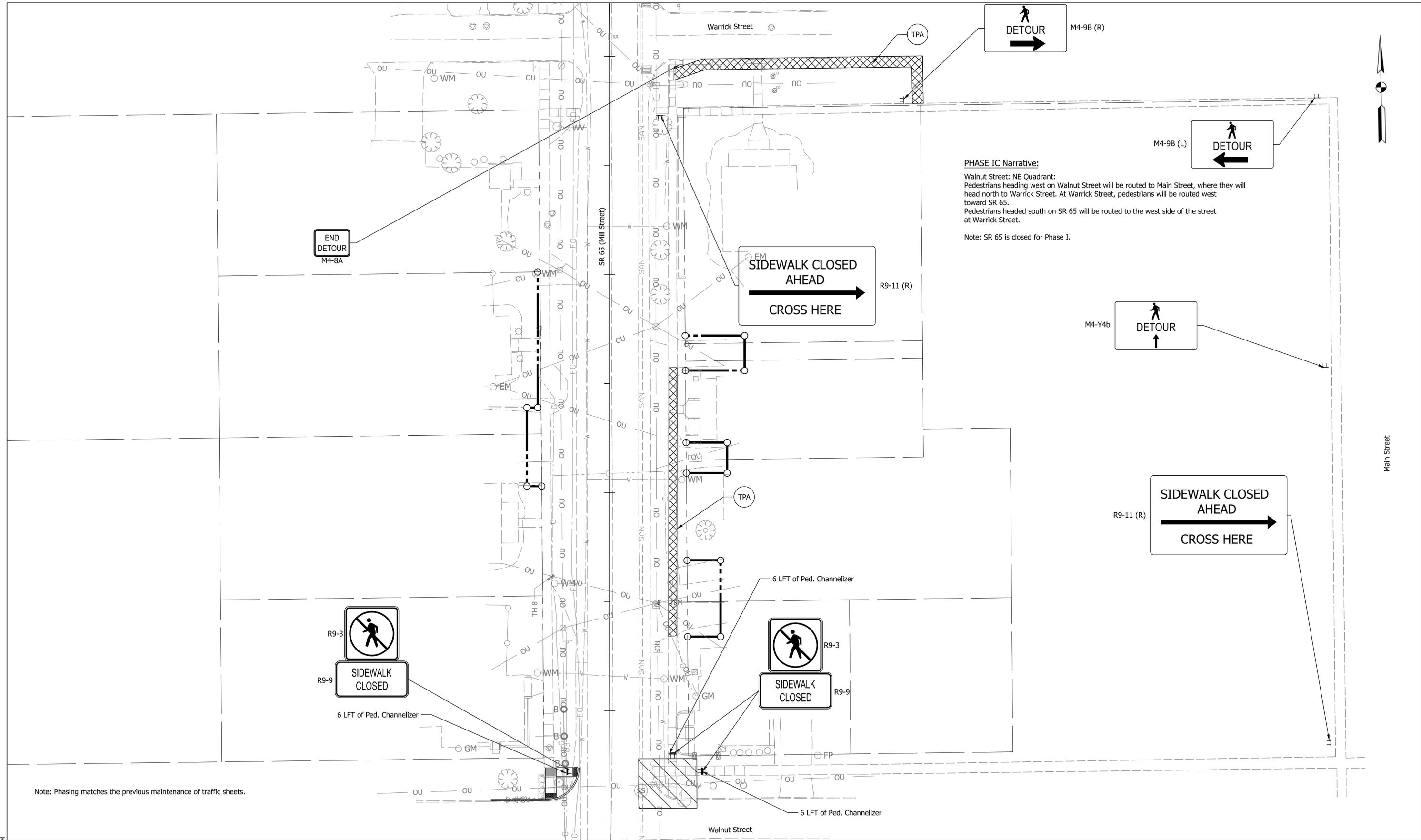
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE IB- WALNUT NW QUAD

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	15 of 114
CONTRACT	PROJECT
R-43253	2001922



PHASE IC Narrative:
 Walnut Street: NE Quadrant:
 Pedestrians heading west on Walnut Street will be routed to Main Street, where they will head north to Warrick Street. At Warrick Street, pedestrians will be routed west toward SR 65.
 Pedestrians headed south on SR 65 will be routed to the west side of the street at Warrick Street.
 Note: SR 65 is closed for Phase I.

Note: Phasing matches the previous maintenance of traffic sheets.

LEGEND		
	Construction Signs	
	Work Zone	
	Pedestrian Channelizer	
	Temporary Curb Ramp	
	Temporary Accessible Pedestrian Path	
	Temporary Traverse Pavement Marking, Removable, 6 IN., White	

NOT FOR CONSTRUCTION

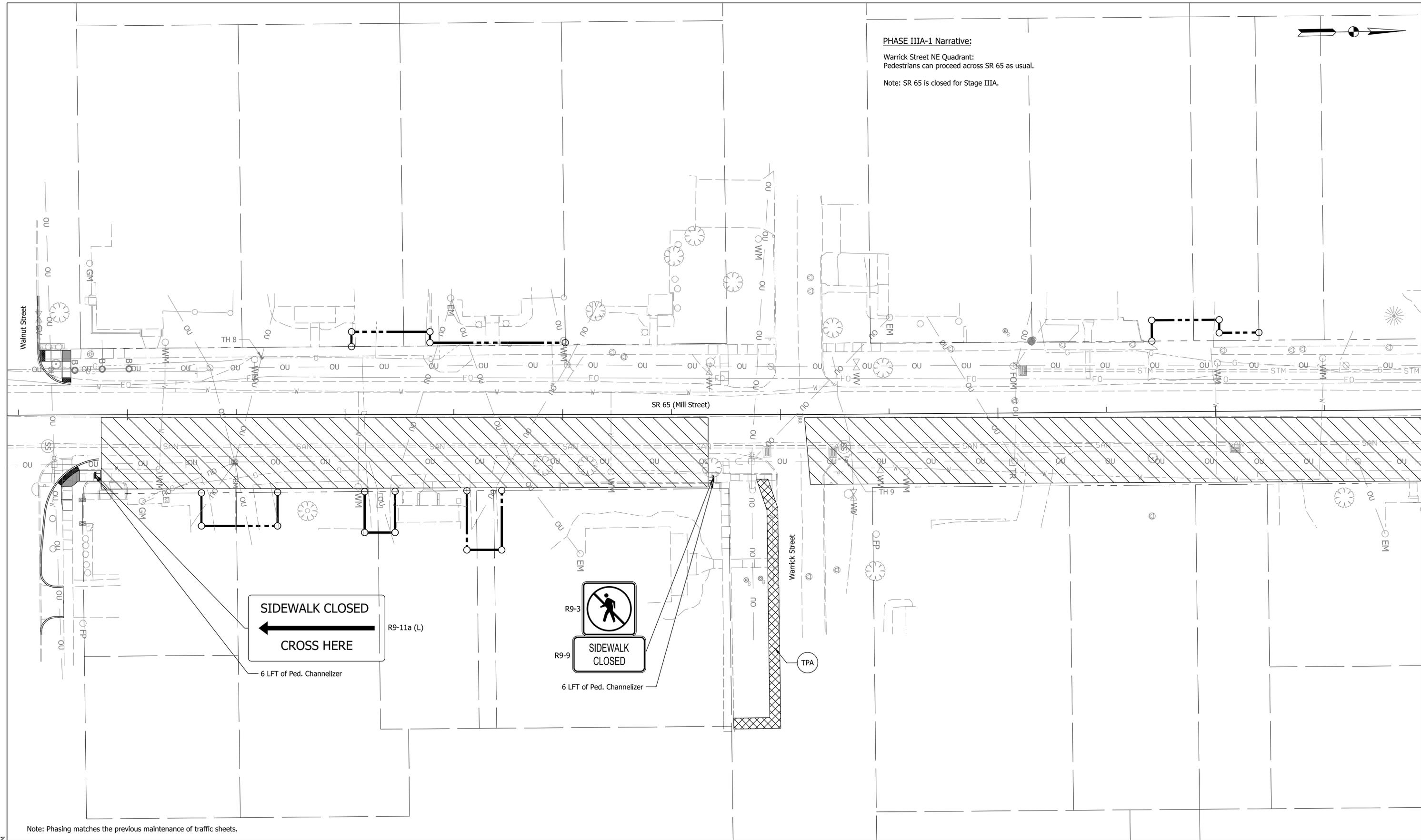
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**PEDESTRIAN MAINTENANCE OF TRAFFIC
PHASE IC- WALNUT NE QUAD**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 2001922
SURVEY BOOK	SHEETS 16 of 114
CONTRACT R-43253	PROJECT 2001922

Plot: 2/26/2025 2:48 PM IP_PWP:d0443562\ShT Ped MOT_03.dgn



PHASE IIIA-1 Narrative:
 Warrick Street NE Quadrant:
 Pedestrians can proceed across SR 65 as usual.
 Note: SR 65 is closed for Stage IIIA.

Note: Phasing matches the previous maintenance of traffic sheets.

LEGEND	
	Construction Signs
	Work Zone
	Pedestrian Channelizer
	Temporary Curb Ramp
	Channelizing Device
	Temporary Accessible Pedestrian Path
	Temporary Traverse Pavement Marking, Removable, 6 IN., White

NOT FOR CONSTRUCTION

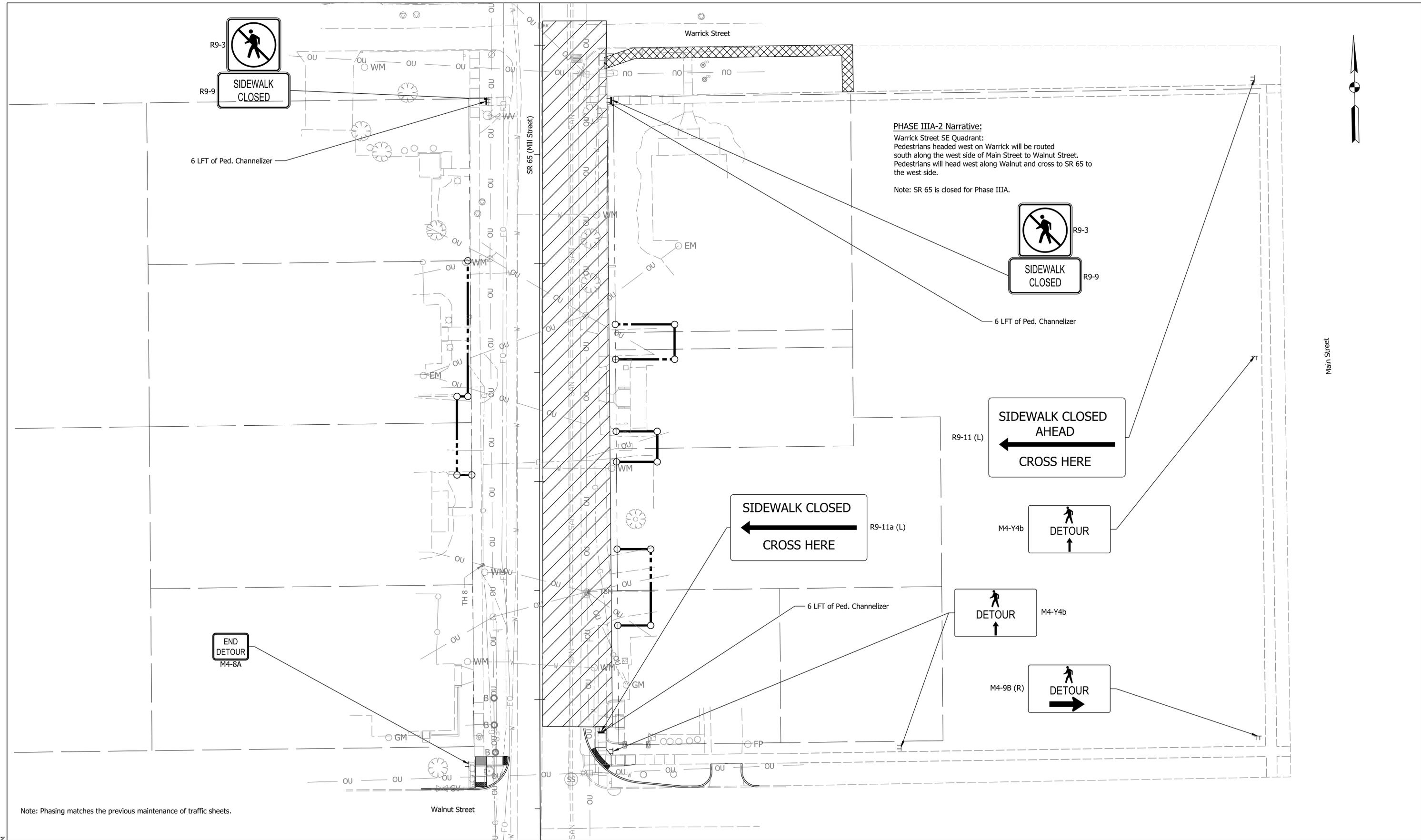
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**PEDESTRIAN MAINTENANCE OF TRAFFIC
PHASE IIIA-1- WARRICK NE QUAD**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 2001922
SURVEY BOOK	SHEETS 17 of 114
CONTRACT R-43253	PROJECT 2001922

Plct: 2/26/2025 2:49 PM
IP_PWP:d0443562\ShT Ped MOT_04.dgn



Note: Phasing matches the previous maintenance of traffic sheets.

2/26/2025 2:50 PM
 IP_PWP:d0443562\Sht Ped MOT_05.dgn

LEGEND	
	Construction Signs
	Work Zone
	Pedestrian Channelizer
	Temporary Curb Ramp
	Temporary Accessible Pedestrian Path
	Temporary Traverse Pavement Marking, Removable, 6 IN., White
	Channelizing Device

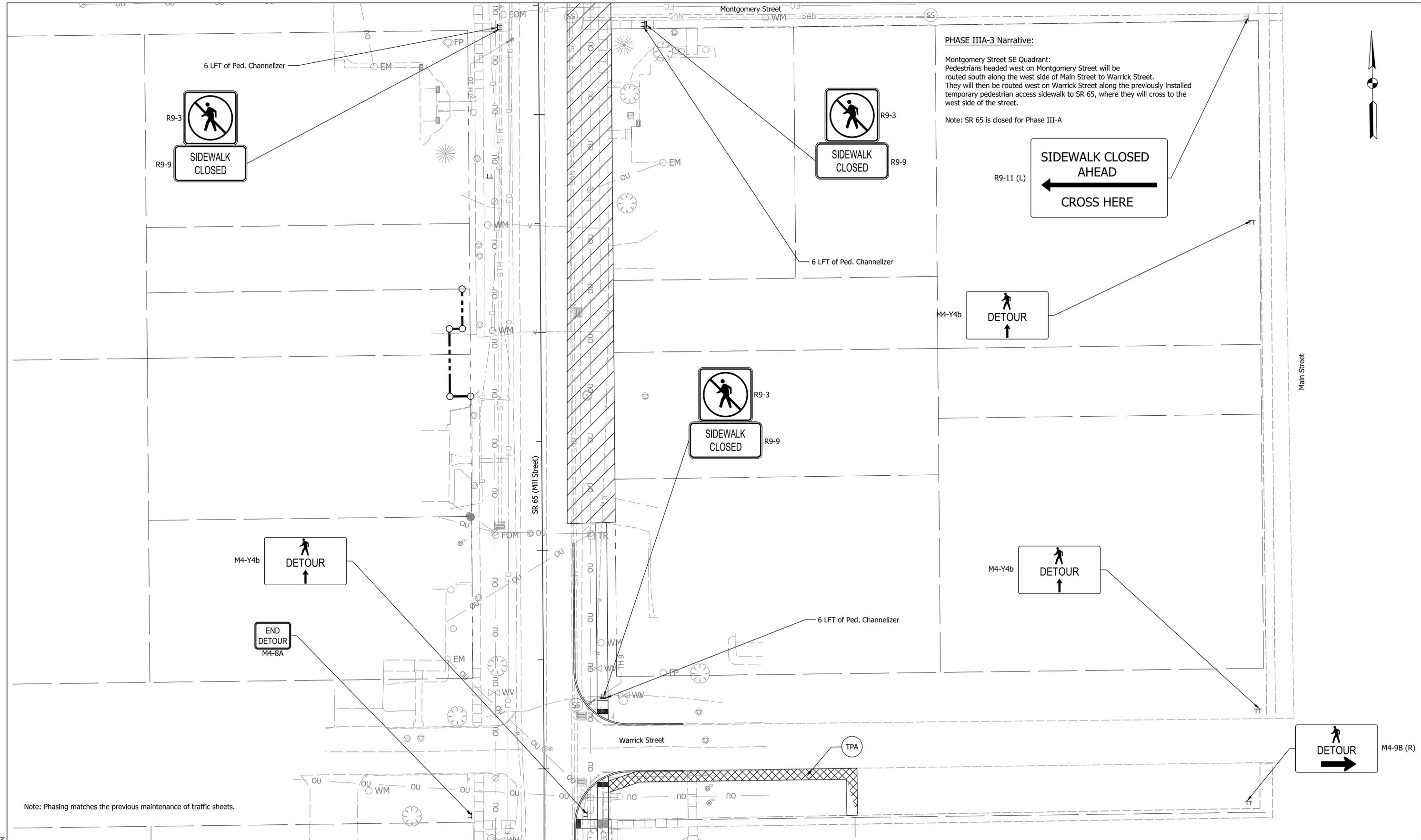
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: MHK _____	DRAWN: MHK _____	
CHECKED: BEA _____	CHECKED: BEA _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

**PEDESTRIAN MAINTENANCE OF TRAFFIC
PHASE IIIA-2- WARRICK SE QUAD**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 2001922
SURVEY BOOK	SHEETS 18 of 114
CONTRACT R-43253	PROJECT 2001922



PHASE IIIA-3 Narrative:
 Montgomery Street SE Quadrant:
 Pedestrians headed west on Montgomery Street will be routed south along the west side of Main Street to Warrick Street. They will then be routed west on Warrick Street along the previously installed temporary pedestrian access sidewalk to SR 65, where they will cross to the west side of the street.

Note: SR 65 is closed for Phase III-A

Note: Phasing matches the previous maintenance of traffic sheets.

2/26/2025 2:51 PM

LEGEND	
	Construction Signs
	Work Zone
	Pedestrian Channelizer
	Temporary Curb Ramp
	Channelizing Device
	Temporary Accessible Pedestrian Path
	Temporary Traverse Pavement Marking, Removable, 6 IN., White

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE IIIA-3- MONTGOMERY SE QUAD

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	19 of 114
CONTRACT	PROJECT
R-43253	2001922

PHASE IIIB-1 Narrative:

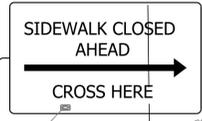
Warrick Street NW & SW Quadrant
Montgomery Street NW Quadrant
Brummitt Street SW Quadrant:
Pedestrians headed south on SR 65 at Montgomery Street will use temporary midblock crossing to reach the SW corner of Montgomery Street to cross to the east sidewalk of SR 65.
Pedestrians heading north on SR 65 will be directed to the east sidewalk at Walnut Street.

Note: SR 65 is closed for Phase IIIB.

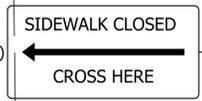
6 LFT of Ped. Channelizer



R9-3
R9-9



R9-11 (R)

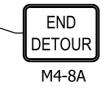


R9-11a (L)

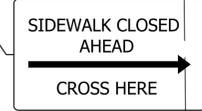


R9-3
R9-9

6 LFT of Ped. Channelizer



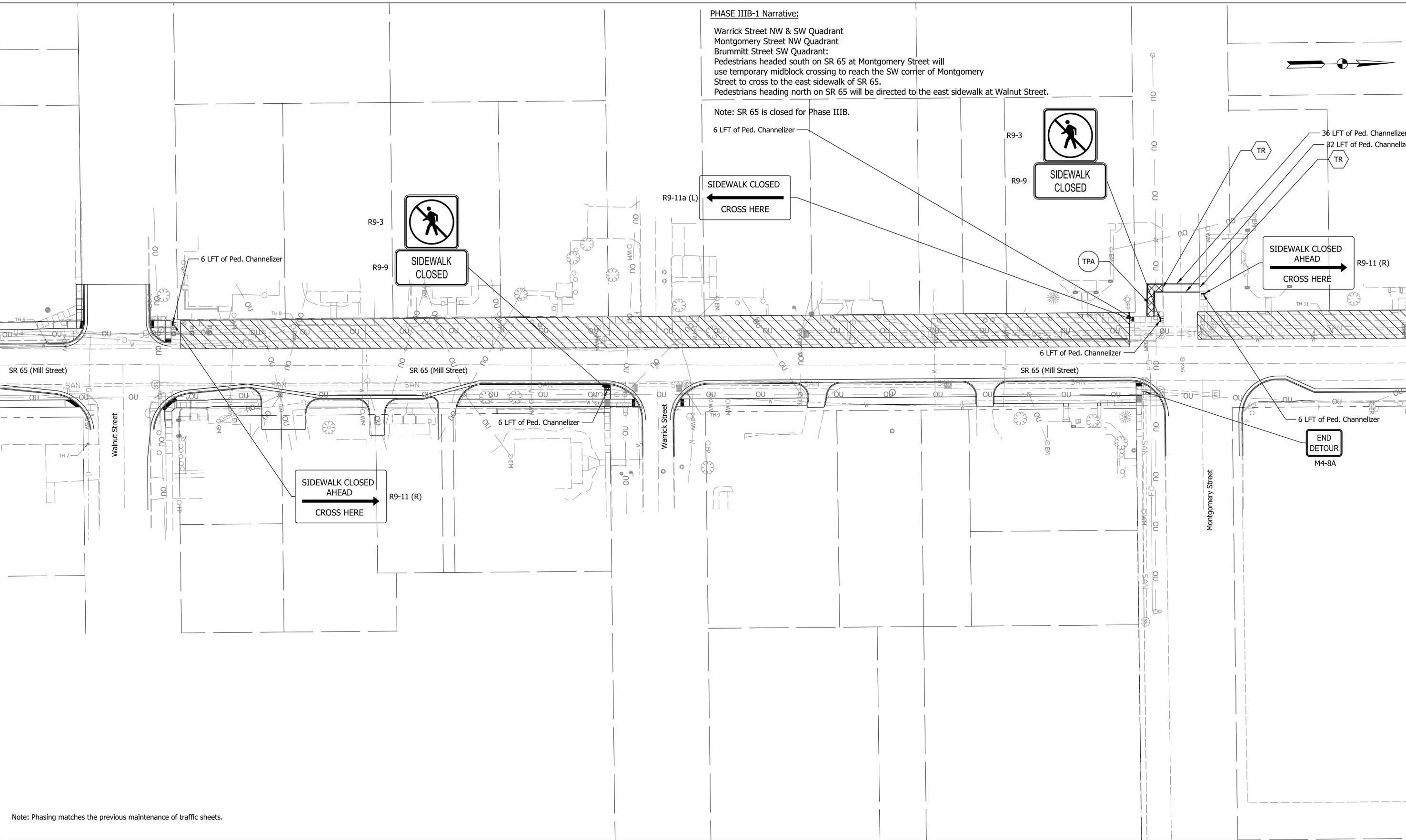
6 LFT of Ped. Channelizer



R9-11 (R)

6 LFT of Ped. Channelizer

6 LFT of Ped. Channelizer



Note: Phasing matches the previous maintenance of traffic sheets.

LEGEND	
	Construction Signs
	Work Zone
	Pedestrian Channelizer
	Temporary Curb Ramp
	Temporary Accessible Pedestrian Path
	Temporary Traverse Pavement Marking, Removable, 6 IN., White
	Channelizing Device

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

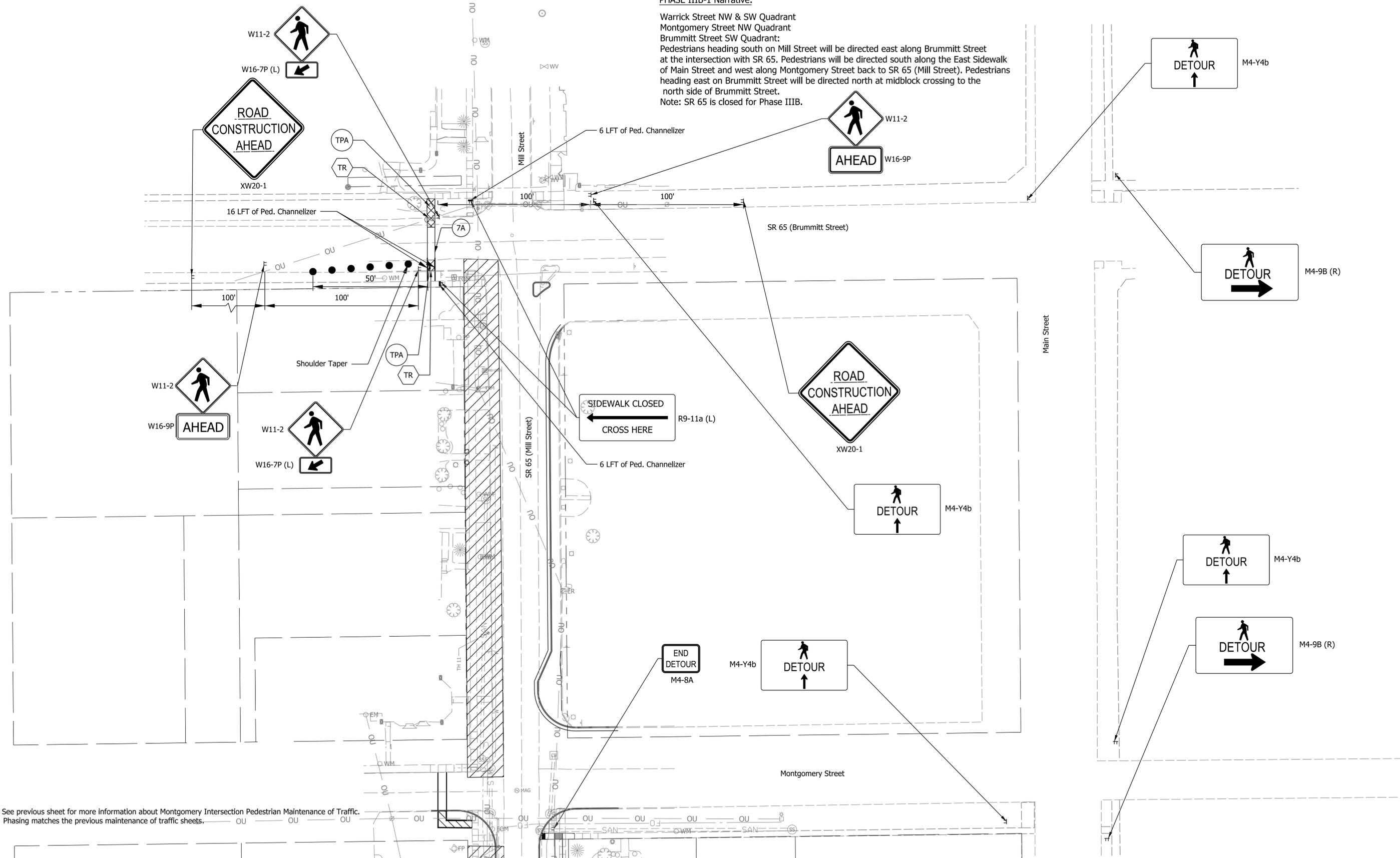
PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE IIIB-1- WEST SIDEWALK

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	20 of 114
CONTRACT	PROJECT
R-43253	2001922

Plot: 2/26/2025 2:51 PM

PHASE IIIB-1 Narrative:

Warrick Street NW & SW Quadrant
Montgomery Street NW Quadrant
Brummitt Street SW Quadrant:
Pedestrians heading south on Mill Street will be directed east along Brummitt Street at the intersection with SR 65. Pedestrians will be directed south along the East Sidewalk of Main Street and west along Montgomery Street back to SR 65 (Mill Street). Pedestrians heading east on Brummitt Street will be directed north at midblock crossing to the north side of Brummitt Street.
Note: SR 65 is closed for Phase IIIB.



Note: See previous sheet for more information about Montgomery Intersection Pedestrian Maintenance of Traffic. Phasing matches the previous maintenance of traffic sheets.

Plot: 2/26/2025 2:52 PM

LEGEND		
	Construction Signs	
	Work Zone	
	Pedestrian Channelizer	
	Temporary Curb Ramp	
	Temporary Accessible Pedestrian Path	
	Temporary Traverse Pavement Marking, Removable, 6 IN., White	
	Channelizing Device	

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

INDIANA
DEPARTMENT OF TRANSPORTATION

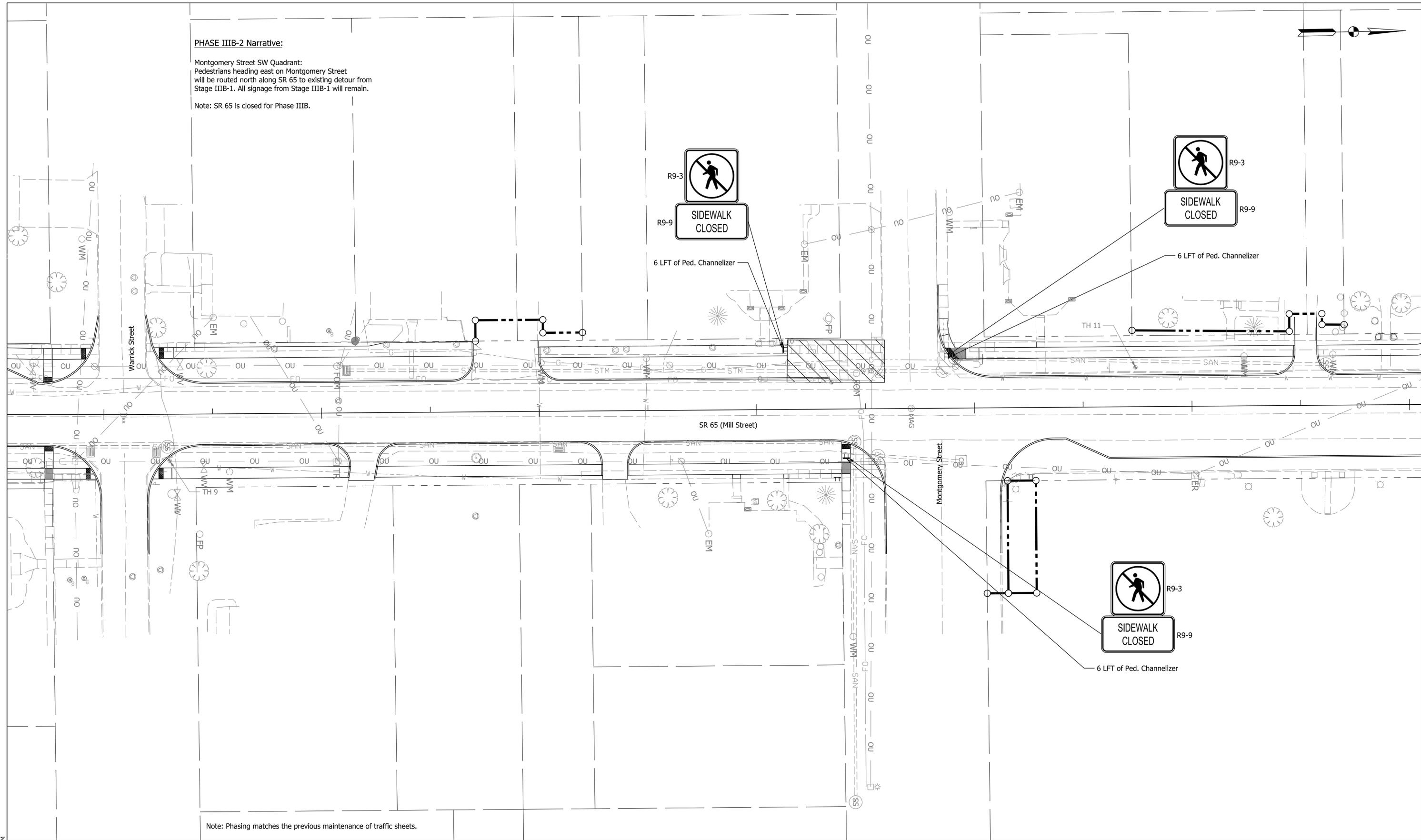
PEDESTRIAN MAINTENANCE OF TRAFFIC
PHASE IIIB-1- WEST SIDEWALK

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	21 of 114
CONTRACT	PROJECT
R-43253	2001922

PHASE IIIB-2 Narrative:

Montgomery Street SW Quadrant:
Pedestrians heading east on Montgomery Street
will be routed north along SR 65 to existing detour from
Stage IIIB-1. All signage from Stage IIIB-1 will remain.

Note: SR 65 is closed for Phase IIIB.



Note: Phasing matches the previous maintenance of traffic sheets.

Plot: 2/26/2025 2:52 PM
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LEGEND

	Construction Signs		Temporary Curb Ramp		Channelizing Device
	Work Zone		Temporary Accessible Pedestrian Path		
	Pedestrian Channelizer		Temporary Traverse Pavement Marking, Removable, 6 IN., White		

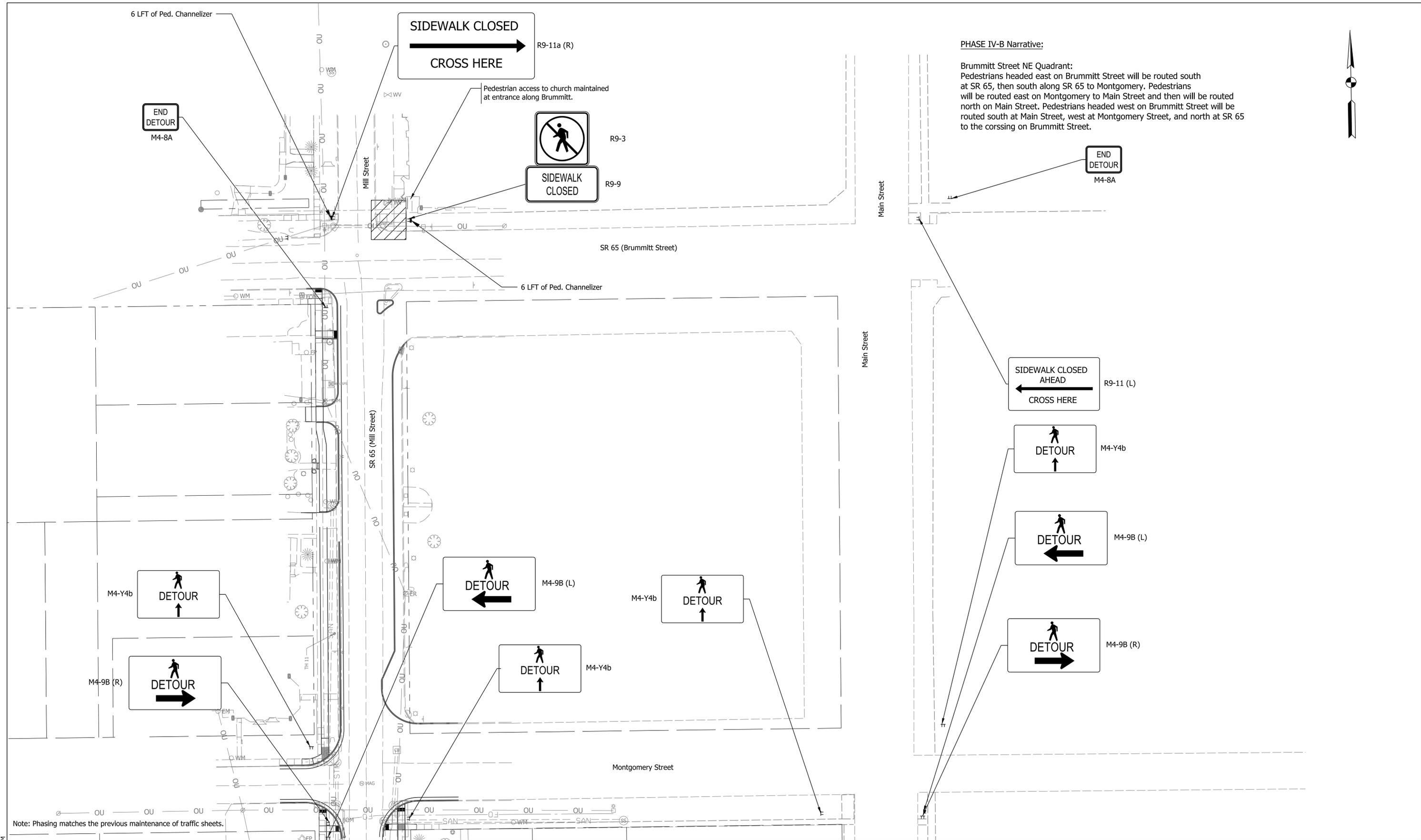
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**PEDESTRIAN MAINTENANCE OF TRAFFIC
PHASE IIIB-2- MONTGOMERY SW QUAD**

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	22 of 114
CONTRACT	PROJECT
R-43253	2001922



Note: Phasing matches the previous maintenance of traffic sheets.

LEGEND	
	Construction Signs
	Work Zone
	Pedestrian Channelizer
	Temporary Curb Ramp
	Channelizing Device
	Temporary Accessible Pedestrian Path
	Temporary Traverse Pavement Marking, Removable, 6 IN., White

NOT FOR CONSTRUCTION

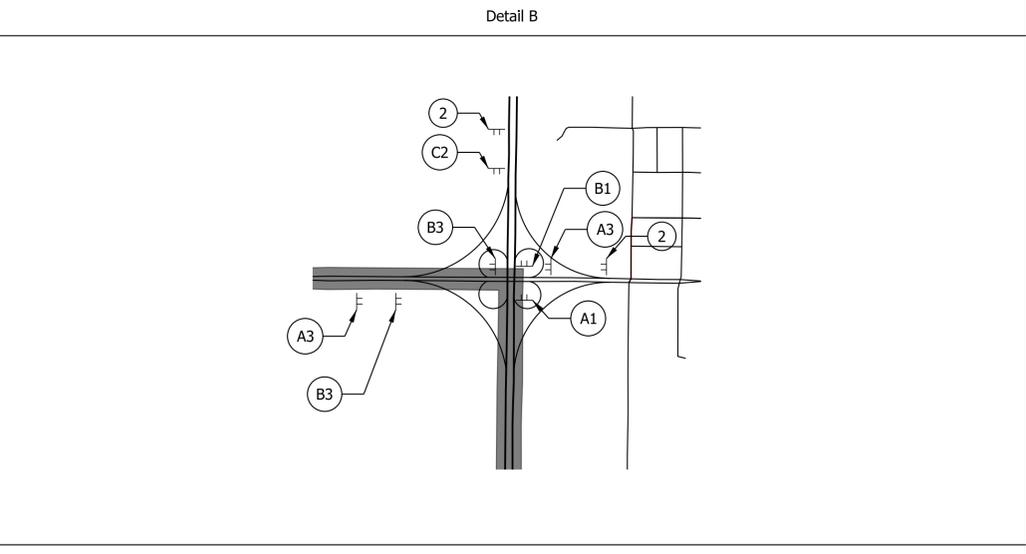
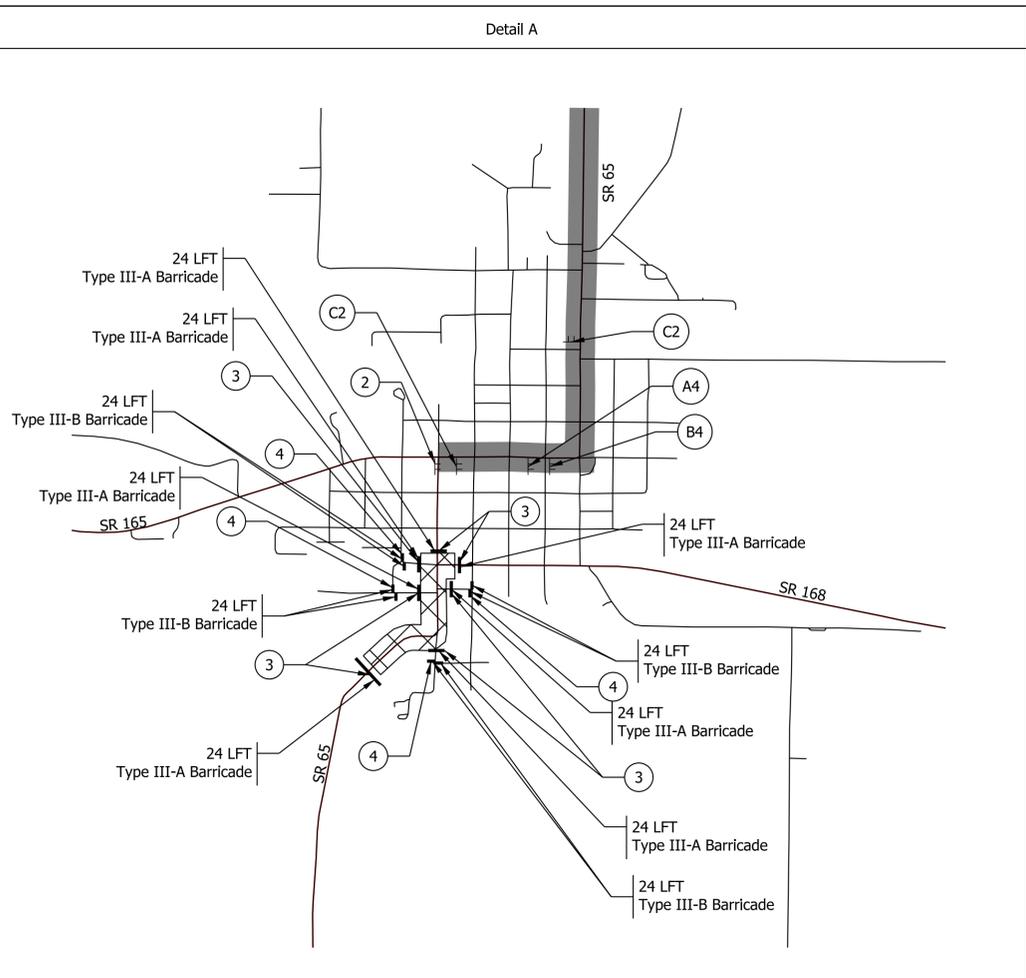
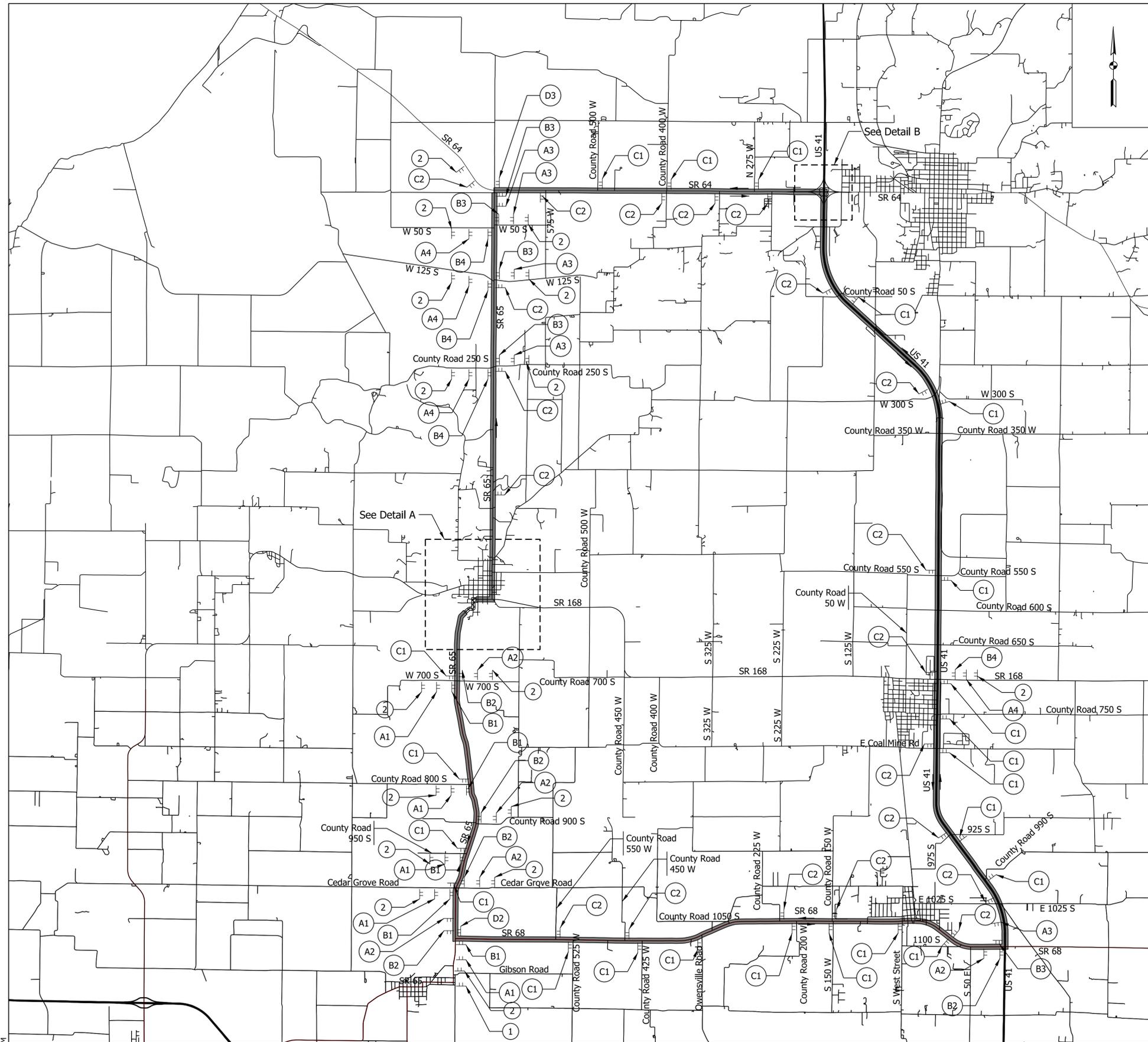
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MHK	DRAWN: MHK	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

PEDESTRIAN MAINTENANCE OF TRAFFIC PHASE IVB- BRUMMITT NE QUAD

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	24 of 114
CONTRACT	PROJECT
R-43253	2001922

Plot: 2/26/2025 2:54 PM



See South Detour Sheet for sign legends.

LEGEND

- Construction Zone
- Detour Route
- Detour Direction
- Traffic Barricade
- Construction Sign

NOTE:
Detour route marker assemblies shall be in accordance with Standard Drawing 801-TCDDT-04.

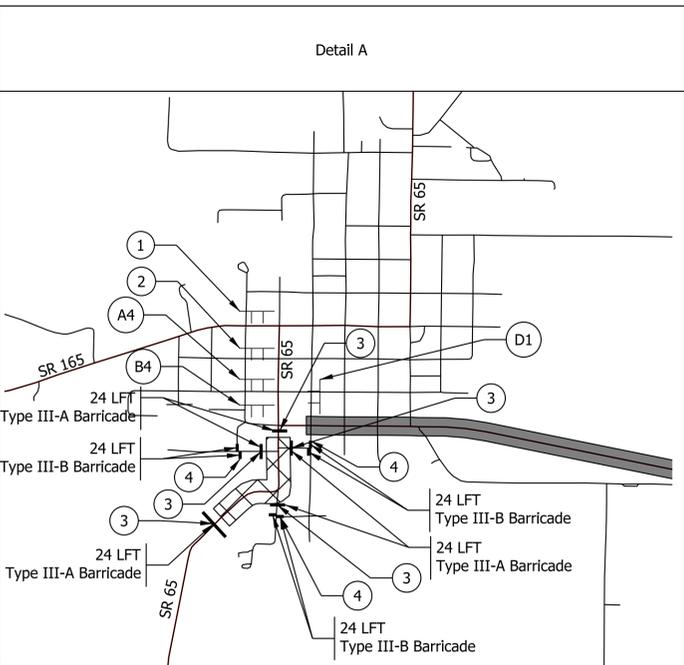
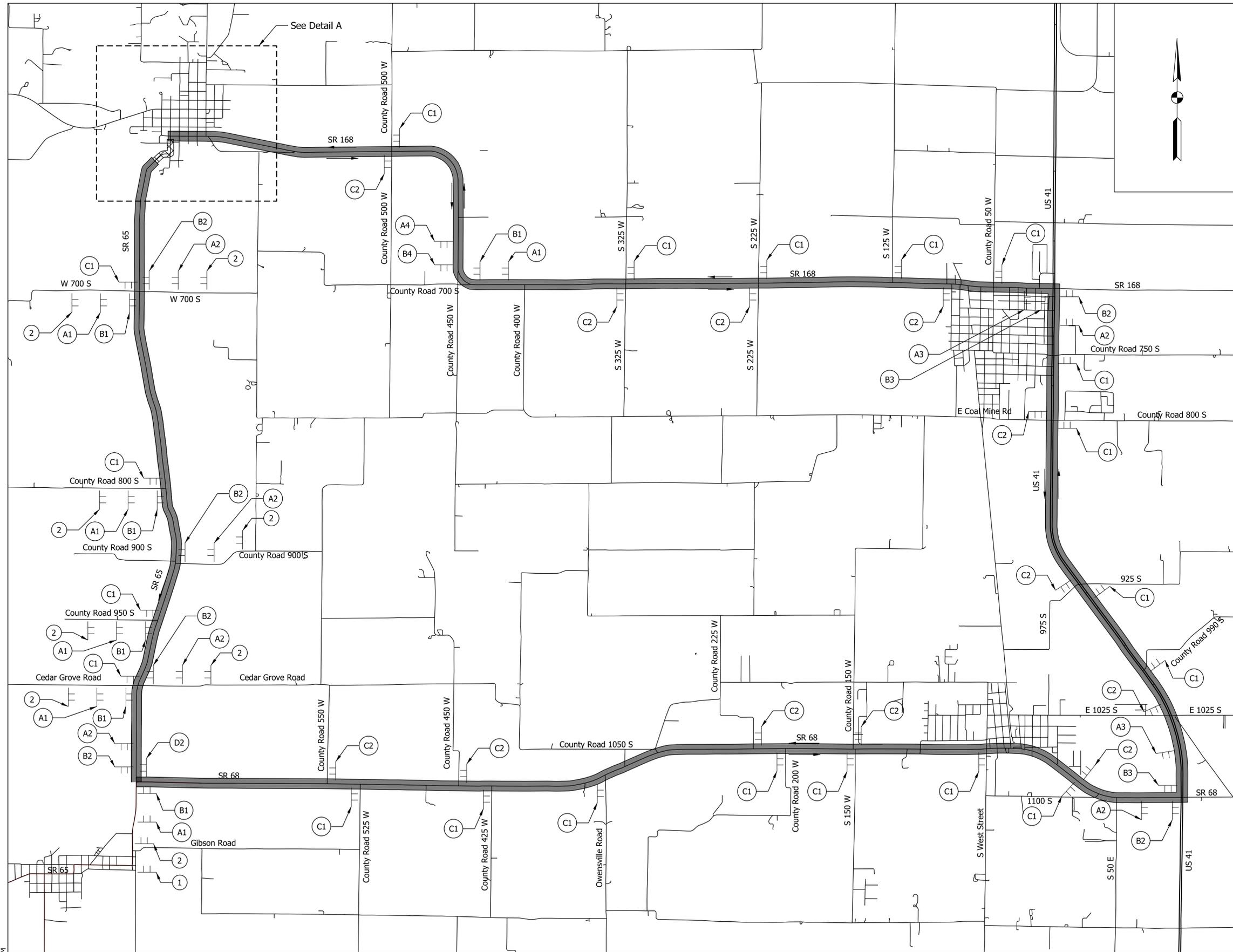
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

SR 65 WALNUT CLOSURE DETOUR MAINTENANCE OF TRAFFIC

HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	25 of 114
CONTRACT	PROJECT
R-43253	2001922



A1	DETOUR NORTH SR 65	M5-1 (R) (21" x 15")	A2	DETOUR NORTH SR 65	M5-1 (L) (21" x 15")	A3	DETOUR SOUTH SR 65	M5-1 (R) (21" x 15")	
A4	DETOUR SOUTH SR 65	M5-1 (L) (21" x 15")	B1	DETOUR NORTH SR 65	M6-1 (R) (21" x 15")	B2	DETOUR NORTH SR 65	M6-1 (L) (21" x 15")	
B3	DETOUR SOUTH SR 65	M6-1 (R) (21" x 15")	B4	DETOUR SOUTH SR 65	M6-1 (L) (21" x 15")	C1	DETOUR NORTH SR 65	M6-3 (21" x 15")	
C2	DETOUR SOUTH SR 65	M6-3 (21" x 15")	D1	END DETOUR NORTH SR 65	M6-1 (R) (21" x 15")	D2	END DETOUR SOUTH SR 65	M6-1 (L) (21" x 15")	
D3	END DETOUR NORTH SR 65	M6-3 (21" x 15")			1	ROAD CLOSED AHEAD			
						2	DETOUR AHEAD		
								3	ROAD CLOSED
								4	

R11-2 w/ Type B Lights on Type III-A Barricades (48" x 30")

R11-4 w/ Type B Lights on Type III-B Barricades (60" x 30")

XW20-3 w/ Type A Lights (36" x 36")

XW20-2 w/ Type A Lights (36" x 36")

2/26/2025 2:55 PM
 IP_PWP:d0443600\SH RD Detour 01.dgn

LEGEND

	Construction Zone		Construction Sign
	Detour Route		
	Detour Direction		
	Traffic Barricade		

NOTE:
 Detour route marker assemblies shall be in accordance with Standard Drawing 801-TCDT-04.

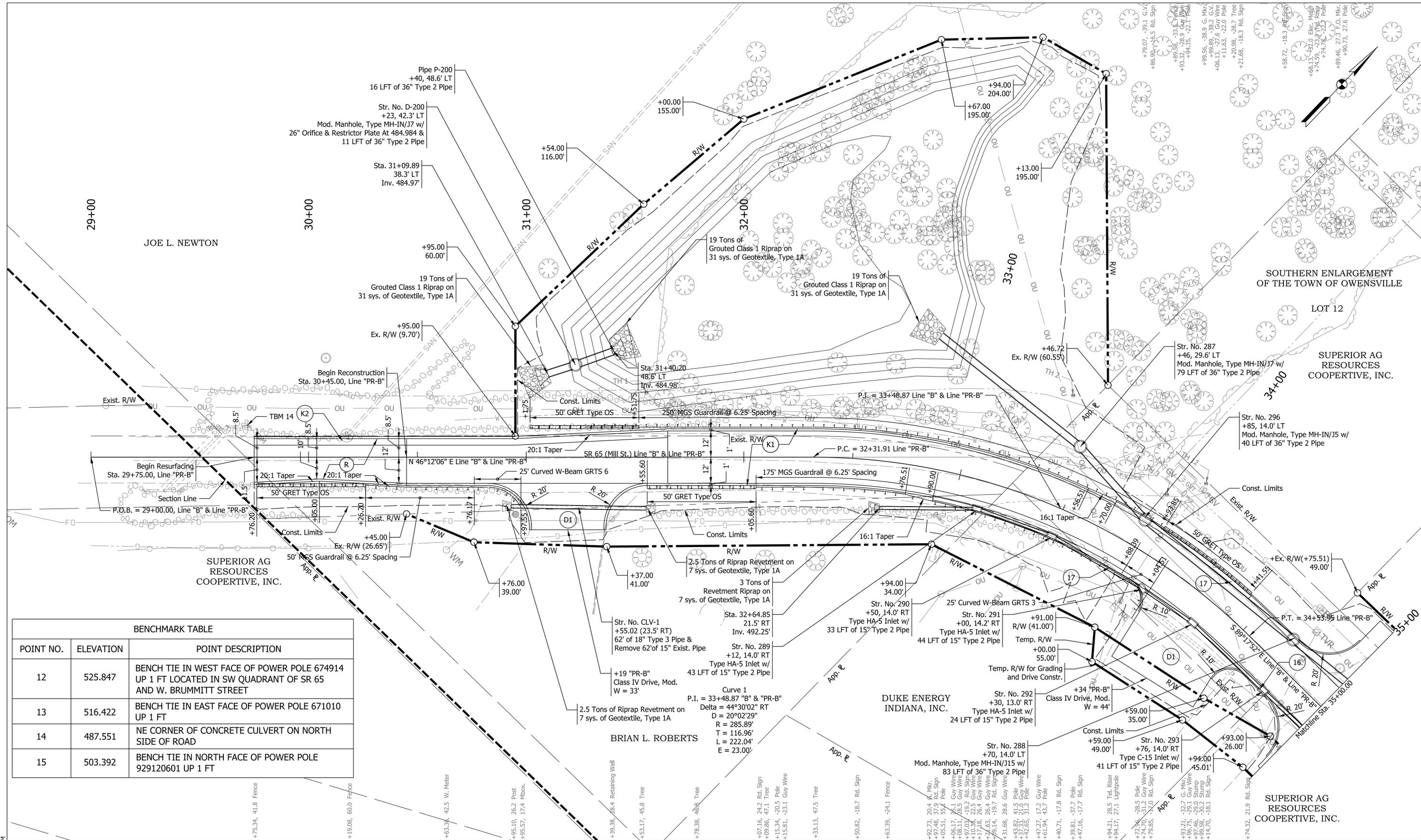
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA
 DEPARTMENT OF TRANSPORTATION

**SR 65 SOUTH CLOSURE DETOUR
 MAINTENANCE OF TRAFFIC**

HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	26 of 114
CONTRACT	PROJECT
R-43253	2001922



BENCHMARK TABLE

POINT NO.	ELEVATION	POINT DESCRIPTION
12	525.847	BENCH TIE IN WEST FACE OF POWER POLE 674914 UP 1 FT LOCATED IN SW QUADRANT OF SR 65 AND W. BRUMMITT STREET
13	516.422	BENCH TIE IN EAST FACE OF POWER POLE 671010 UP 1 FT
14	487.551	NE CORNER OF CONCRETE CULVERT ON NORTH SIDE OF ROAD
15	503.392	BENCH TIE IN NORTH FACE OF POWER POLE 929120601 UP 1 FT

LEGEND

(F) Sidewalk, Concrete	(CR) Curb Ramp, Concrete	(R) Mill and Overlay	(D1) HMA for Approaches, Type B
(13) Curb, Concrete	(26) Sodding, Nursery	(C1) PCCP for Approaches, 6"	(22) Curb Center, Concrete D
(16) Curb and Gutter, Concrete	(K1) Full Depth Reconstruction	(C2) PCCP for Approaches, 9"	(AC) Adjust Casting To Grade
(17) Curb and Gutter, Concrete, 4"	(K2) HMA for Shoulder Widening	(DND) Do Not Disturb	

NOT FOR CONSTRUCTION

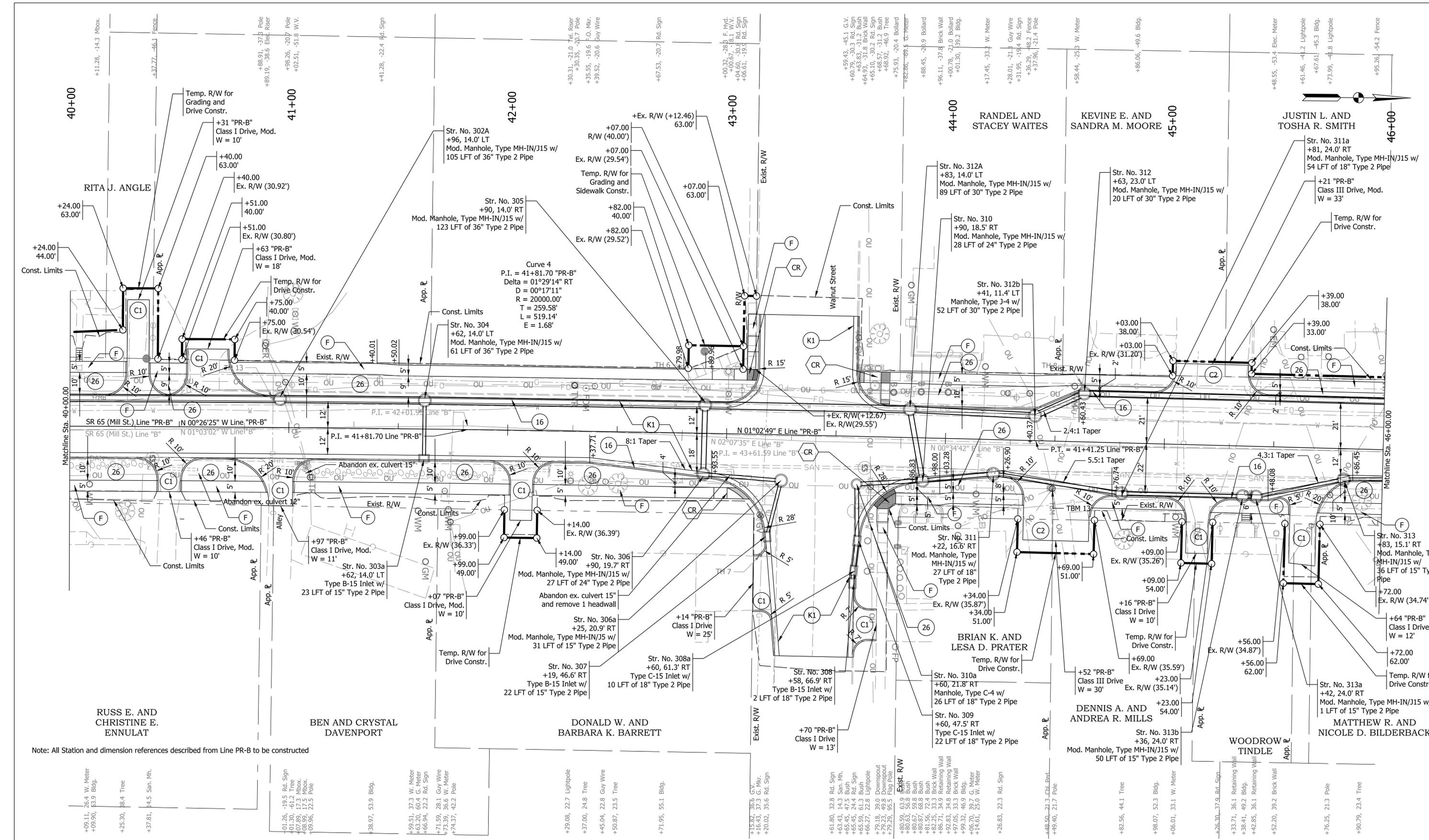
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-B"
STA. 29+00.00 TO STA. 35+00.00

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	29 of 114
CONTRACT	PROJECT
R-43253	2001922

PLOT: 3/25/2025 4:26 PM
 IP_PWP:d0480870\SHR RD Plan_01 Env.dgn



Note: All Station and dimension references described from Line PR-B to be constructed

PLOT: 2/26/2025 2:59 PM

LEGEND	
(F)	Sidewalk, Concrete
(13)	Curb, Concrete
(16)	Curb and Gutter, Concrete
(17)	Curb and Gutter, Concrete, 4"
(CR)	Curb Ramp, Concrete
(26)	Sodding, Nursery
(K1)	Full Depth Reconstruction
(K2)	HMA for Shoulder Widening
(R)	Mill and Overlay
(C1)	PCCP for Approaches, 6"
(C2)	PCCP for Approaches, 9"
(D1)	HMA for Approaches, Type B
(22)	Curb Center, Concrete D
(AC)	Adjust Casting To Grade
(DND)	Do Not Disturb

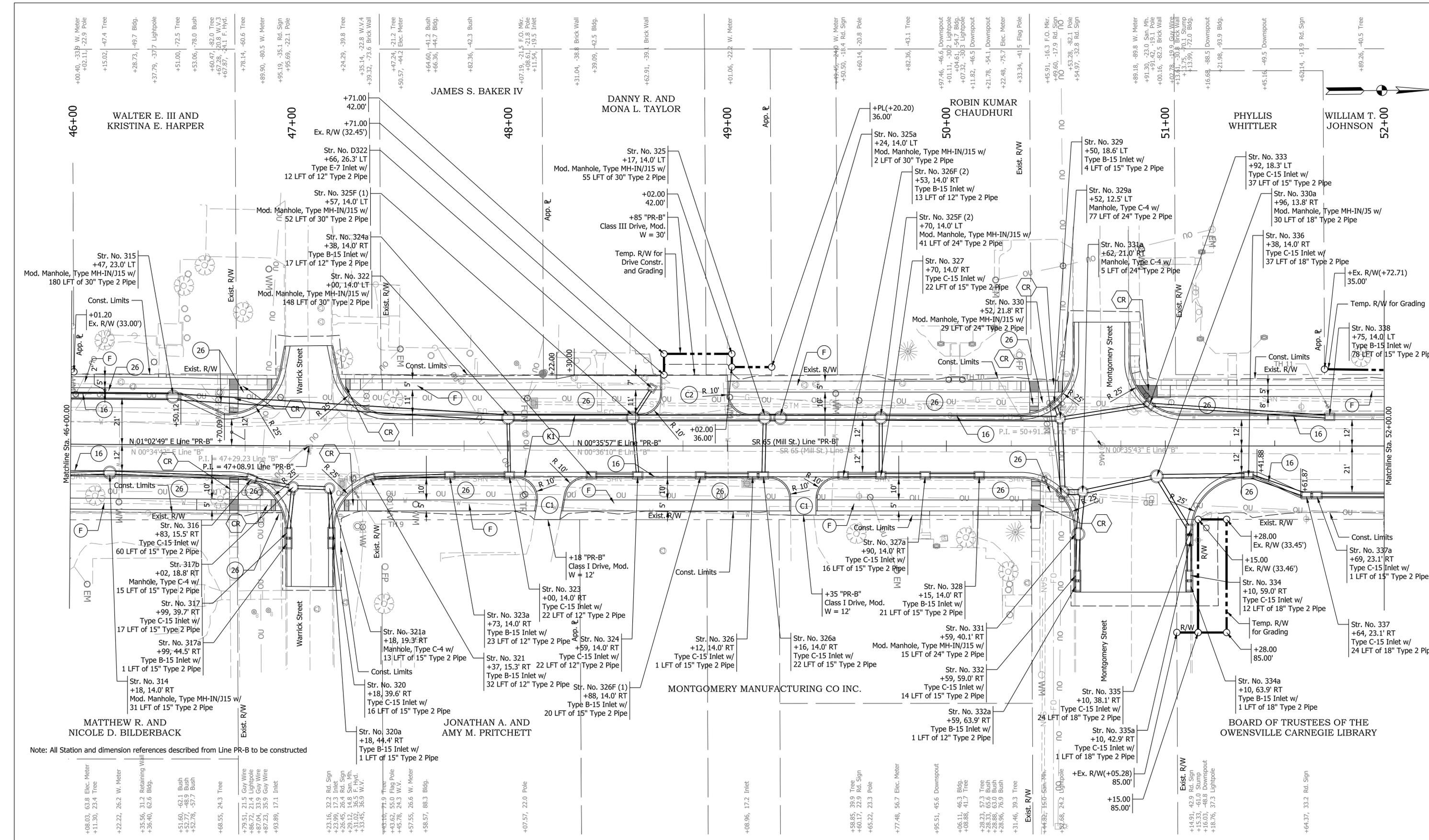
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-B"
STA. 40+00.00 TO STA. 46+00.00

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 5'	2001922
SURVEY BOOK	SHEETS
	33 of 114
CONTRACT	PROJECT
R-43253	2001922



LEGEND

(F)	Sidewalk, Concrete	(CR)	Curb Ramp, Concrete	(R)	Mill and Overlay	(D1)	HMA for Approaches, Type B
(13)	Curb, Concrete	(26)	Sodding, Nursery	(C1)	PCCP for Approaches, 6"	(22)	Curb Center, Concrete D
(16)	Curb and Gutter, Concrete	(K1)	Full Depth Reconstruction	(C2)	PCCP for Approaches, 9"	(AC)	Adjust Casting To Grade
(17)	Curb and Gutter, Concrete, 4"	(K2)	HMA for Shoulder Widening	(DND)	Do Not Disturb		

PLOT: 2/26/2025 3:01 PM
IP_PWP:d0443600\SH RD Plan_04.dgn

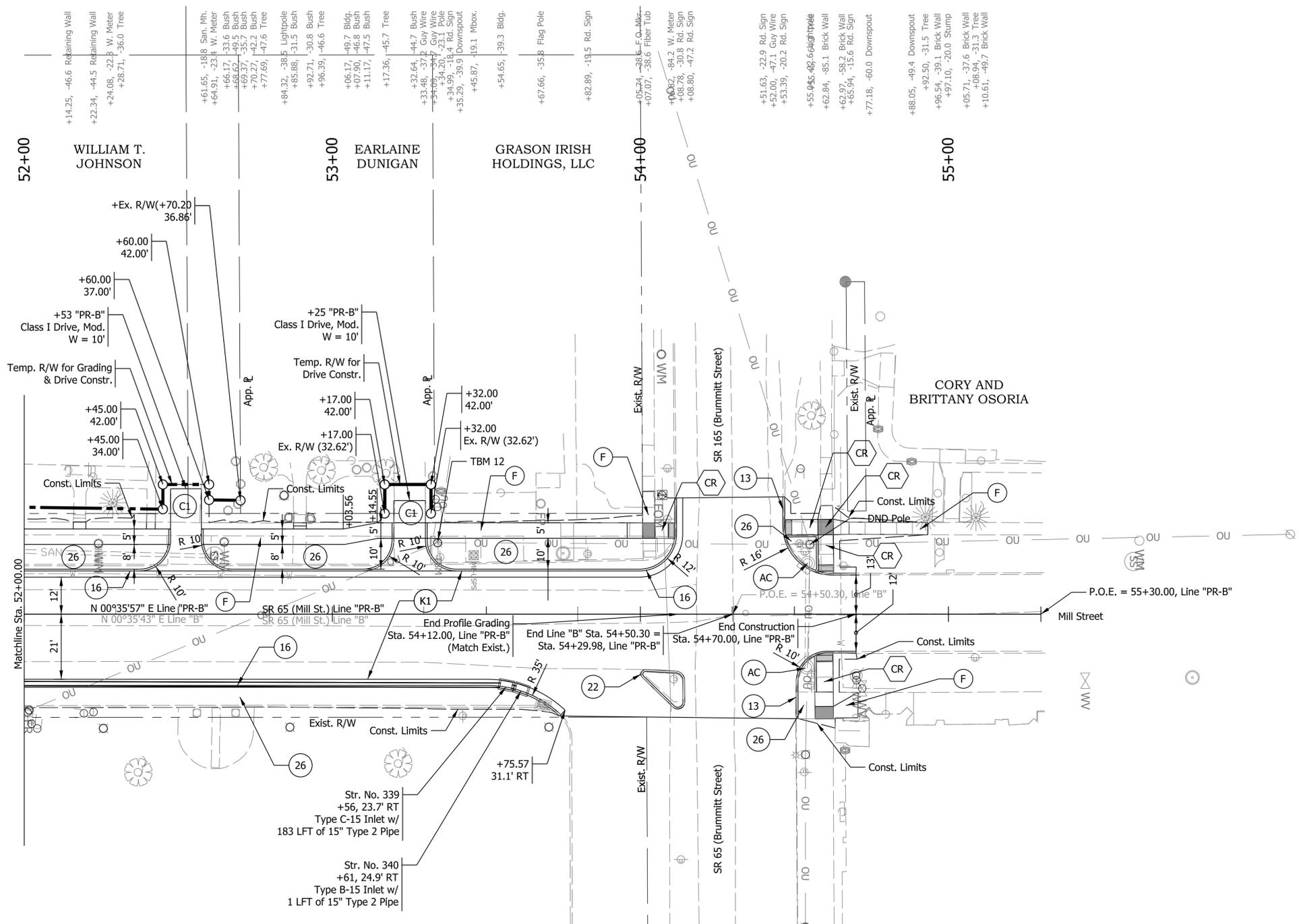
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-B"
STA. 46+00.00 TO STA. 52+00.00

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 5'	2001922
SURVEY BOOK	SHEETS
	35 of 114
CONTRACT	PROJECT
R-43253	2001922



BOARD OF TRUSTEES OF THE OWENSVILLE CARNEGIE LIBRARY

FIRST GENERAL BAPTIST CHURCH OF OWENSVILLE

Note: All Station and dimension references described from Line PR-B to be constructed

- +01.09, 31.8 Elec. Riser
- +02.36, 35.4 Guy Wire
- +04.22, 37.0 Guy Wire
- +19.36, 32.1 Guy Wire
- +22.48, 31.7 Guy Wire
- +25.68, 37.0 Lightpole
- +36.89, 51.2 Tree
- +56.04, 31.9 Brick Wall
- +68.63, 31.9 Brick Wall
- +86.24, 36.6 Lightpole
- +21.31, 48.4 Tree
- +42.36, 32.9 Rd. Sign
- +68.44, 29.5 Inlet
- +70.30, 36.6 Lightpole
- +10.83, 21.0 Rd. Sign
- +13.18, 79.5 Rd. Sign
- +53.19, 51.1 Rd. Sign
- +55.56, 101.4 Pole
- +55.44, 23.1 Rd. Sign
- +55.46, 19.6 Rd. Sign
- +53.61, 45.5 Lightpole
- +66.40, 44.1 Downspout
- +69.98, 33.6 Retaining Wall
- +70.04, 19.9 F. Hyd.
- +70.12, 21.4 W.V.
- +72.01, 24.0 W.V.
- +79.63, 32.4 Bldg.
- +08.71, 14.0 Rd. Sign
- +14.25, -46.6 Retaining Wall
- +22.34, -44.5 Retaining Wall
- +24.08, -22.8 W. Meter
- +28.71, -36.0 Tree
- +61.65, -18.8 San. Mh.
- +64.91, -23.4 W. Meter
- +66.17, -33.6 Bush
- +68.62, -49.5 Bush
- +69.37, -35.7 Bush
- +70.27, -42.2 Bush
- +77.69, -47.6 Tree
- +84.32, -38.5 Lightpole
- +85.68, -31.5 Bush
- +92.71, -30.8 Bush
- +96.39, -46.6 Tree
- +06.17, -49.7 Bldg.
- +07.90, -46.8 Bush
- +11.17, -47.5 Bush
- +17.36, -45.7 Tree
- +32.64, -44.7 Bush
- +33.48, -37.2 Guy Wire
- +34.09, -34.7 Guy Wire
- +34.20, -23.1 Pole
- +34.99, -18.4 Rd. Sign
- +35.29, -39.9 Downspout
- +45.87, -19.1 Mbox.
- +54.65, -39.3 Bldg.
- +67.66, -35.8 Flag Pole
- +62.89, -13.5 Rd. Sign
- +05.24, -38.6 F.O. Mkr.
- +07.07, -38.6 Fiber Tub
- +08.82, -84.2 W. Meter
- +08.78, -30.8 Rd. Sign
- +08.80, -47.2 Rd. Sign
- +51.63, -22.9 Rd. Sign
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- +53.39, -20.2 Rd. Sign
- +55.04, -42.6 Lightpole
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- +62.97, -58.2 Brick Wall
- +65.94, -15.6 Rd. Sign
- +77.18, -60.0 Downspout
- +88.05, -49.4 Downspout
- +92.50, -31.5 Tree
- +96.54, -39.1 Brick Wall
- +97.10, -20.0 Stump
- +05.71, -37.6 Brick Wall
- +08.94, -31.3 Tree
- +10.61, -49.7 Brick Wall

Plot: 2/26/2025 3:02 PM

IP_PWP:d0443600\ShT RD Plan_05.dgn

LEGEND

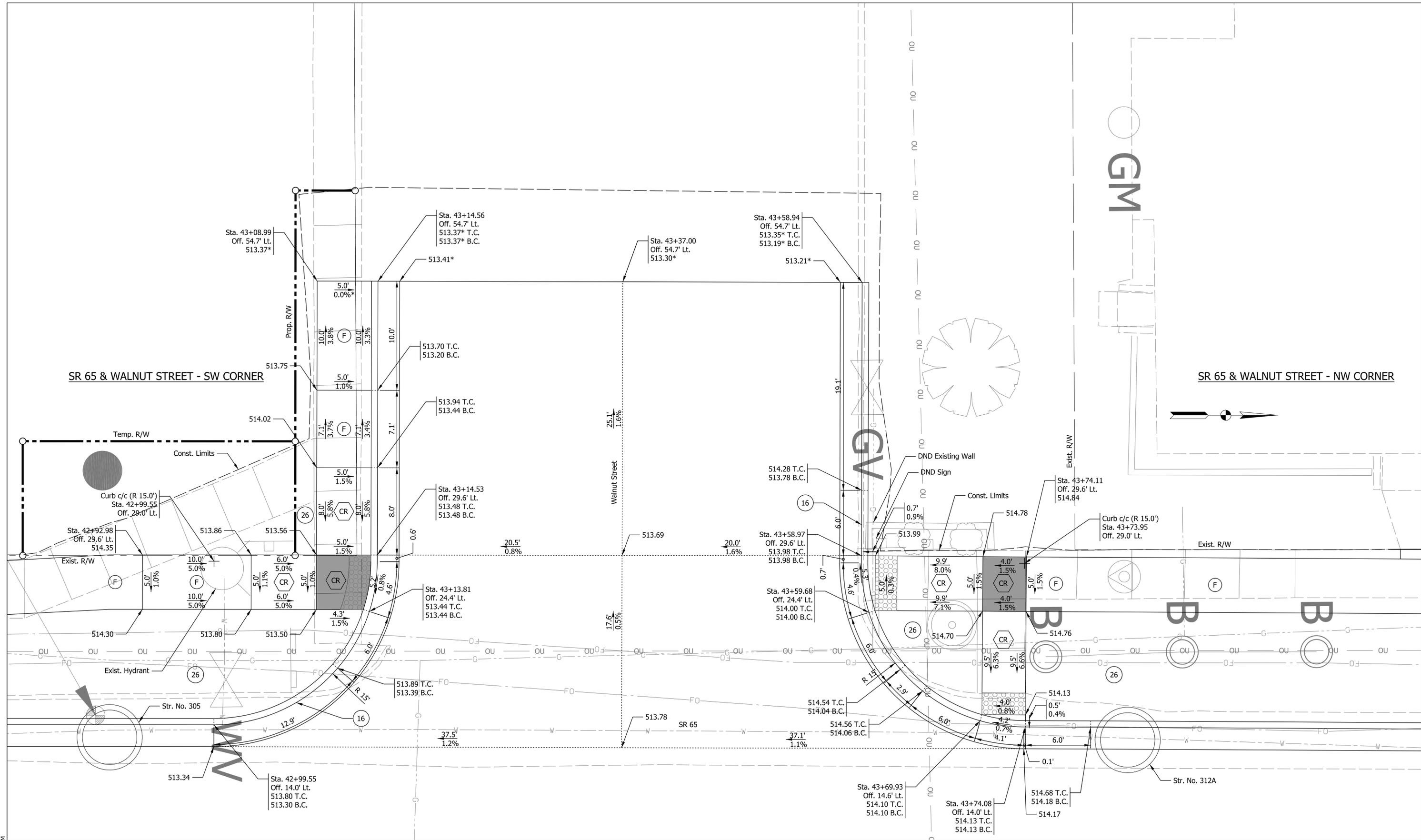
(F)	Sidewalk, Concrete	(CR)	Curb Ramp, Concrete	(R)	Mill and Overlay	(D1)	HMA for Approaches, Type B
(13)	Curb, Concrete	(26)	Sodding, Nursery	(C1)	PCCP for Approaches, 6"	(22)	Curb Center, Concrete D
(16)	Curb and Gutter, Concrete	(K1)	Full Depth Reconstruction	(C2)	PCCP for Approaches, 9"	(AC)	Adjust Casting To Grade
(17)	Curb and Gutter, Concrete, 4"	(K2)	HMA for Shoulder Widening	(DND)	Do Not Disturb		

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN SHEET LINE "PR-B" STA. 52+00.00 TO STA. 55+30.00	

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 5'	2001922
SURVEY BOOK	SHEETS
	37 of 114
CONTRACT	PROJECT
R-43253	2001922



LEGEND:

(F) Sidewalk, Concrete	(26) Sodding, Nursery	(22) Center Curb, Concrete D
(13) Curb, Concrete	(CR) Curb Ramp, Concrete	Turning Space
(16) Curb and Gutter, Concrete	(K1) Full Depth Reconstruction with HMA	(DWS) Detectable Warning Surface
	(AC) Adjust Casting to Grade	

NOT FOR CONSTRUCTION

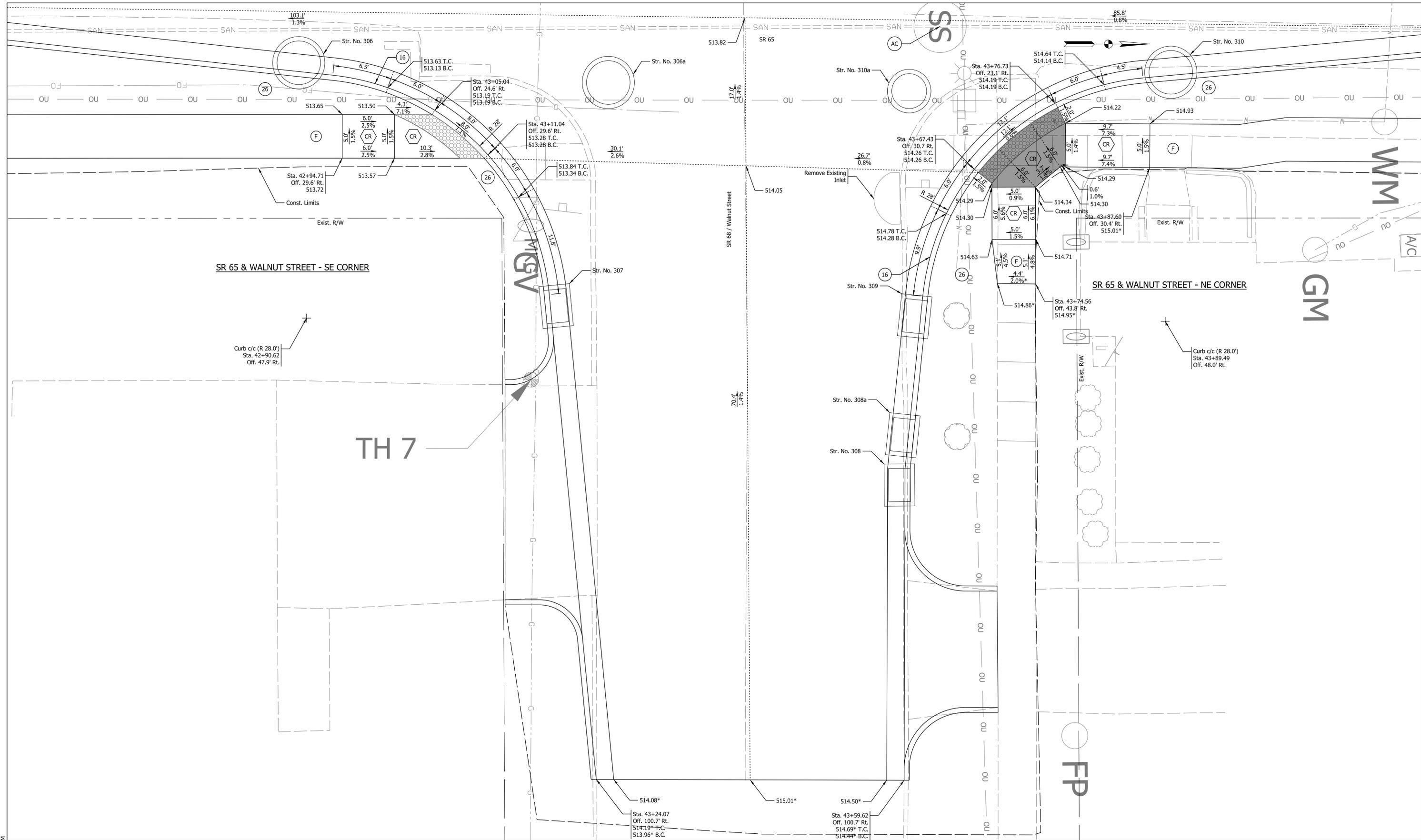
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

**CURB RAMP AND INTERSECTION DETAILS
SR 65 AND WALNUT STREET**

HORIZONTAL SCALE	BRIDGE FILE
1" = 4'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	40 of 114
CONTRACT	PROJECT
R-43253	2001922

PLOT: 2/28/2025 8:18 AM
 IP_PWP:d0443600\SH RD Intersection Detail_04_02.dgn



SR 65 & WALNUT STREET - SE CORNER

SR 65 & WALNUT STREET - NE CORNER

TH 7

WMM

GM

FP

LEGEND:

(F)	Sidewalk, Concrete	(26)	Sidewalk, Concrete	[Shaded Area]	Turning Space
(13)	Curb, Concrete	(CR)	Curb Ramp, Concrete	[Dotted Area]	Detectable Warning Surface
(16)	Curb and Gutter, Concrete	(K1)	Full Depth Reconstruction with HMA		
		(AC)	Adjust Casting to Grade		

NOT FOR CONSTRUCTION

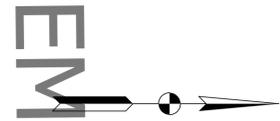
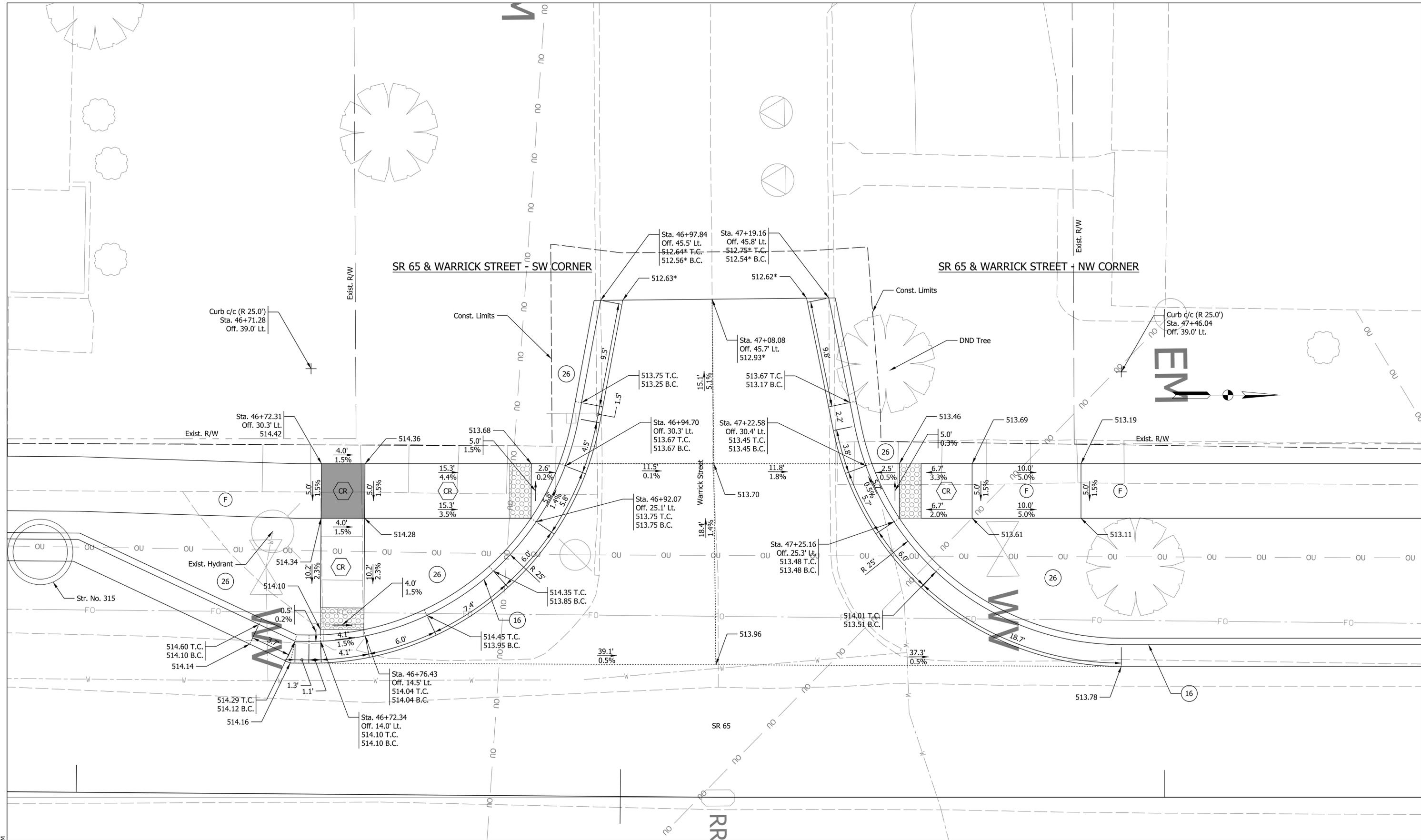
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA
DEPARTMENT OF TRANSPORTATION

**CURB RAMP AND INTERSECTION DETAILS
SR 65 AND SR 168**

HORIZONTAL SCALE	BRIDGE FILE
1" = 5'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	41 of 114
CONTRACT	PROJECT
R-43253	2001922

Plot: 2/28/2025 8:19 AM



8:19 AM
2/28/2025
IP_PWP:d0443600\Sht RD Intersection Detail_04_04.dgn

LEGEND:			
(F)	Sidewalk, Concrete	(22)	Center Curb, Concrete D
(13)	Curb, Concrete	(CR)	Curb Ramp, Concrete
(16)	Curb and Gutter, Concrete	(K1)	Full Depth Reconstruction with HMA
		(AC)	Adjust Casting to Grade
		(26)	Sodding, Nursery
		(Grey Box)	Turning Space
		(Dashed Circle)	Detectable Warning Surface

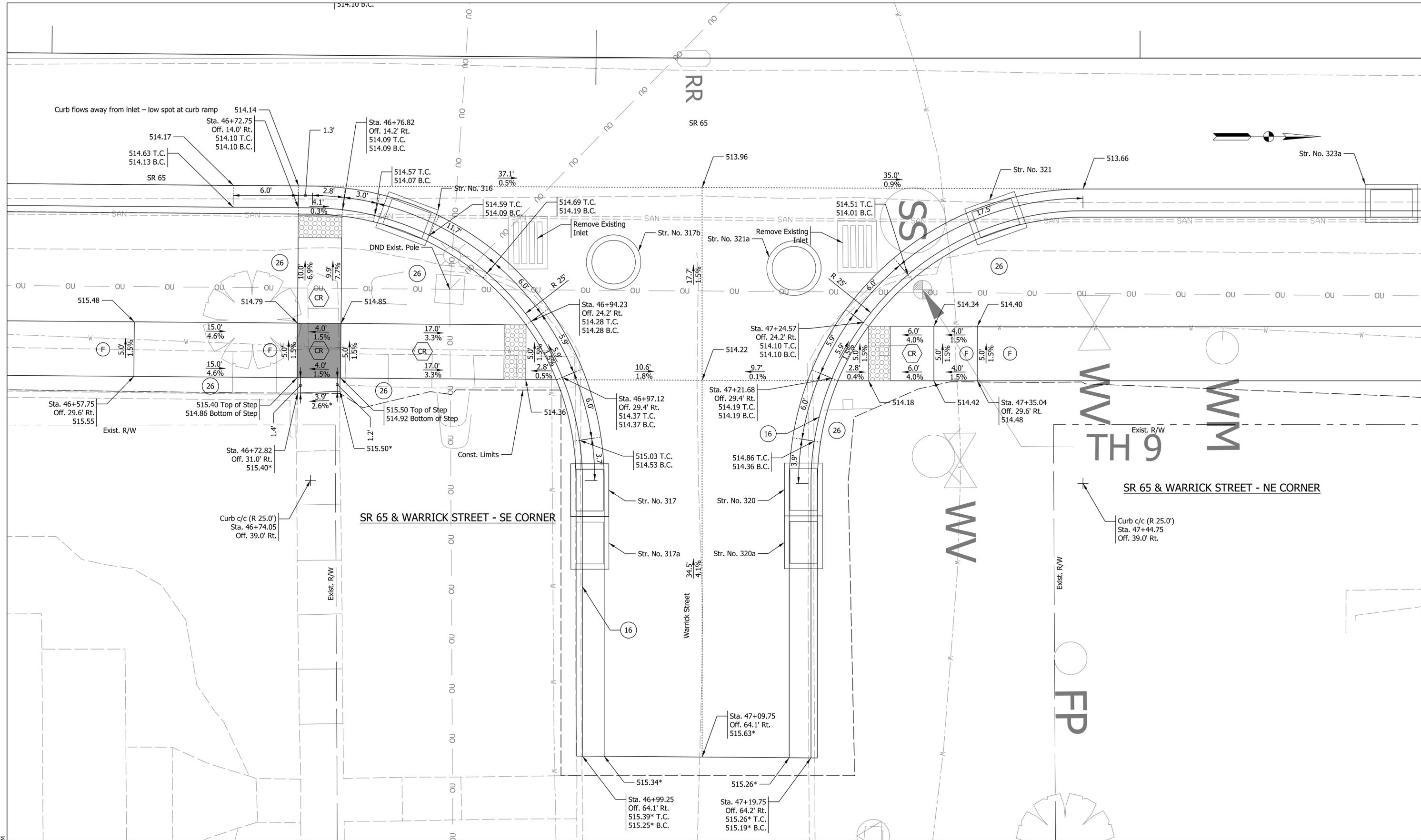
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: SAC _____	DRAWN: SAC _____	
CHECKED: BEA _____	CHECKED: BEA _____	

INDIANA DEPARTMENT OF TRANSPORTATION

**CURB RAMP AND INTERSECTION DETAILS
SR 65 AND WARRICK STREET WEST**

HORIZONTAL SCALE 1" = 4'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 2001922
SURVEY BOOK	SHEETS 42 of 114
CONTRACT R-43253	PROJECT 2001922



LEGEND:

(F) Sidewalk, Concrete	(26) Sodding, Nursery	(22) Center Curb, Concrete D
(13) Curb, Concrete	(CR) Curb Ramp, Concrete	Turning Space
(16) Curb and Gutter, Concrete	(K1) Full Depth Reconstruction with HMA	Detectable Warning Surface
	(AC) Adjust Casting to Grade	

NOT FOR CONSTRUCTION

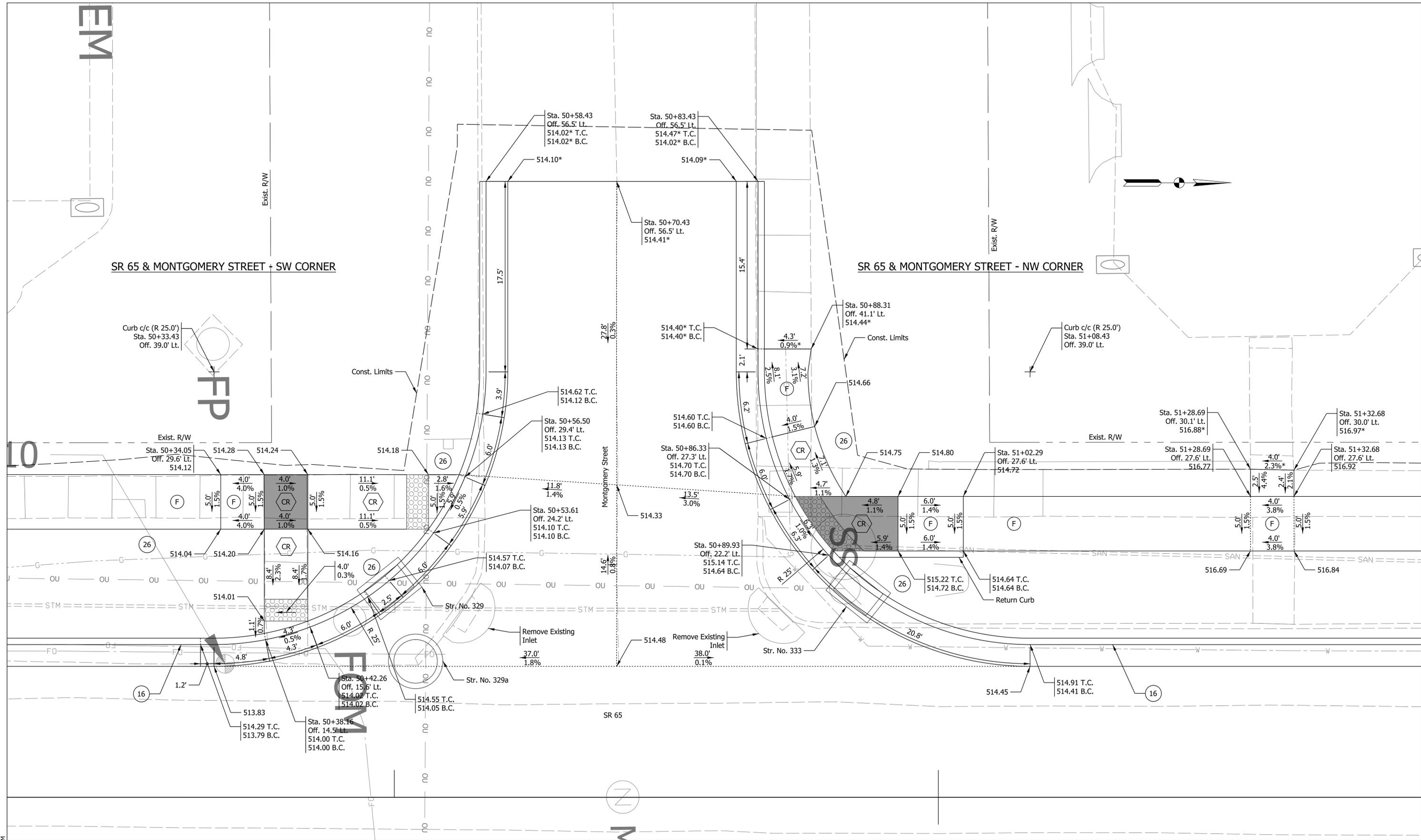
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA
DEPARTMENT OF TRANSPORTATION

CURB RAMP AND INTERSECTION DETAILS
SR 65 AND WARRICK STREET EAST

HORIZONTAL SCALE	BRIDGE FILE
1" = 4'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	43 of 114
CONTRACT	PROJECT
R-43253	2001922

Plot: 2/28/2025 8:20 AM



8:21 AM
2/28/2025
IP_PWP:d0443600\Sht RD Intersection Detail_04_06.dgn

LEGEND:	
(F) Sidewalk, Concrete	(26) Sodding, Nursery
(13) Curb, Concrete	(CR) Curb Ramp, Concrete
(16) Curb and Gutter, Concrete	(K1) Full Depth Reconstruction with HMA
	(AC) Adjust Casting to Grade
	(22) Center Curb, Concrete D
	(Grey Box) Turning Space
	(Square with X) Detectable Warning Surface

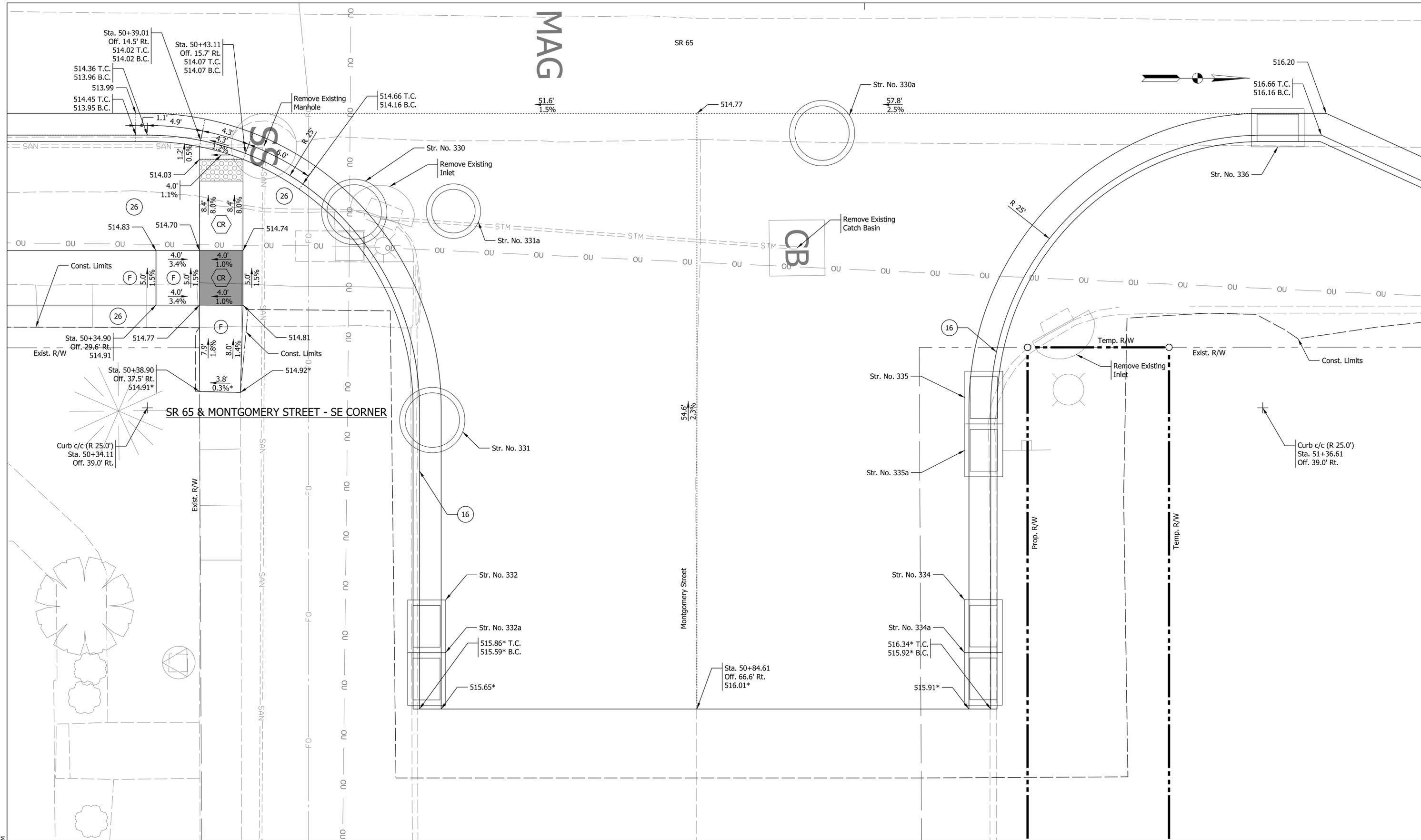
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

**CURB RAMP AND INTERSECTION DETAILS
SR 65 AND MONTGOMERY STREET WEST**

HORIZONTAL SCALE 1" = 4'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 2001922
SURVEY BOOK	SHEETS 44 of 114
CONTRACT R-43253	PROJECT 2001922



LEGEND:

(F) Sidewalk, Concrete	(26) Sodding, Nursery	(22) Center Curb, Concrete D
(13) Curb, Concrete	(CR) Curb Ramp, Concrete	Turning Space
(16) Curb and Gutter, Concrete	(K1) Full Depth Reconstruction with HMA	Detectable Warning Surface
	(AC) Adjust Casting to Grade	

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: HEW _____	DRAWN: HEW _____	
CHECKED: BEA _____	CHECKED: BEA _____	

INDIANA DEPARTMENT OF TRANSPORTATION

**CURB RAMP AND INTERSECTION DETAILS
SR 65 AND MONTGOMERY STREET EAST**

HORIZONTAL SCALE 1" = 4'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 2001922
SURVEY BOOK	SHEETS 45 of 114
CONTRACT R-43253	PROJECT 2001922

Plot: 2/28/2025 8:21 AM

SR 65

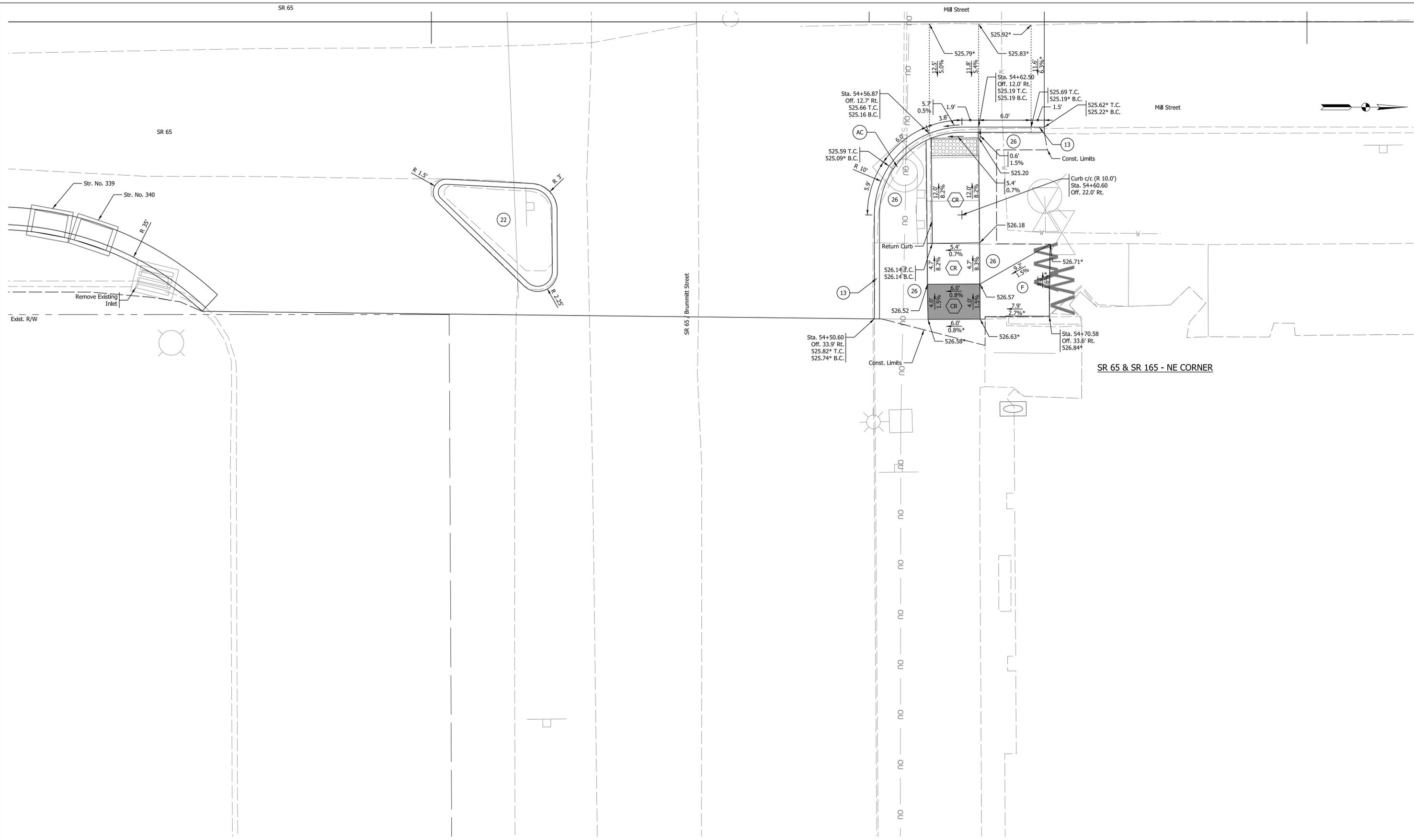
Mill Street

Mill Street

SR 65

SR 65 / Brummitt Street

SR 65 & SR 165 - NE CORNER



Plot: 2/28/2025 8:23 AM

LEGEND:

(F) Sidewalk, Concrete	(26) Sidewalk, Concrete	Turning Space
(13) Curb, Concrete	(CR) Curb Ramp, Concrete	Detectable Warning Surface
(16) Curb and Gutter, Concrete	(K1) Full Depth Reconstruction with HMA	
(AC) Adjust Casting to Grade		

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: HEW _____	DRAWN: HEW _____	
CHECKED: BEA _____	CHECKED: BEA _____	

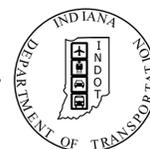
**INDIANA
DEPARTMENT OF TRANSPORTATION**

**CURB RAMP AND INTERSECTION DETAILS
SR 65 AND SR 165 EAST**

HORIZONTAL SCALE 1" = 5'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 2001922
SURVEY BOOK	SHEETS 47 of 114
CONTRACT R-43253	PROJECT 2001922

PROJECT	DESIGNATION
2001922	2001922
CONTRACT	
R-43253	

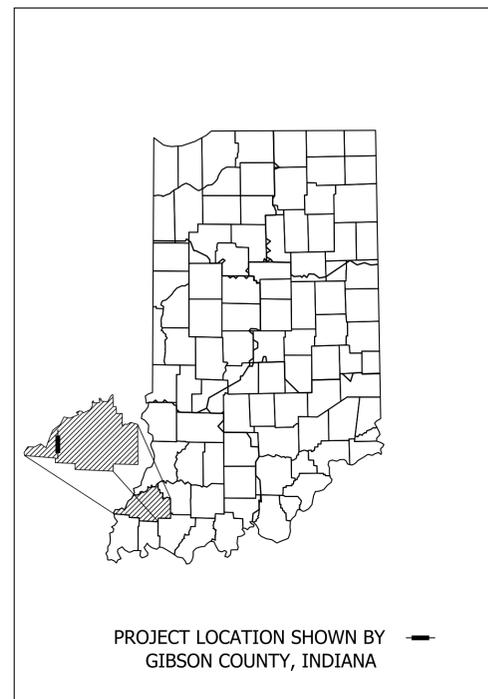
INDIANA DEPARTMENT OF TRANSPORTATION



RIGHT OF WAY PLANS

ROUTE: SR 65 FROM: RP 19+85 TO: RP 20+27
 PROJECT NO. 2001922 P.E. 2001922 R/W CONST.

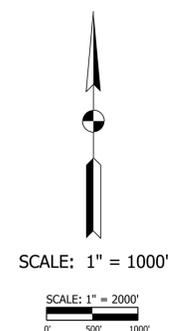
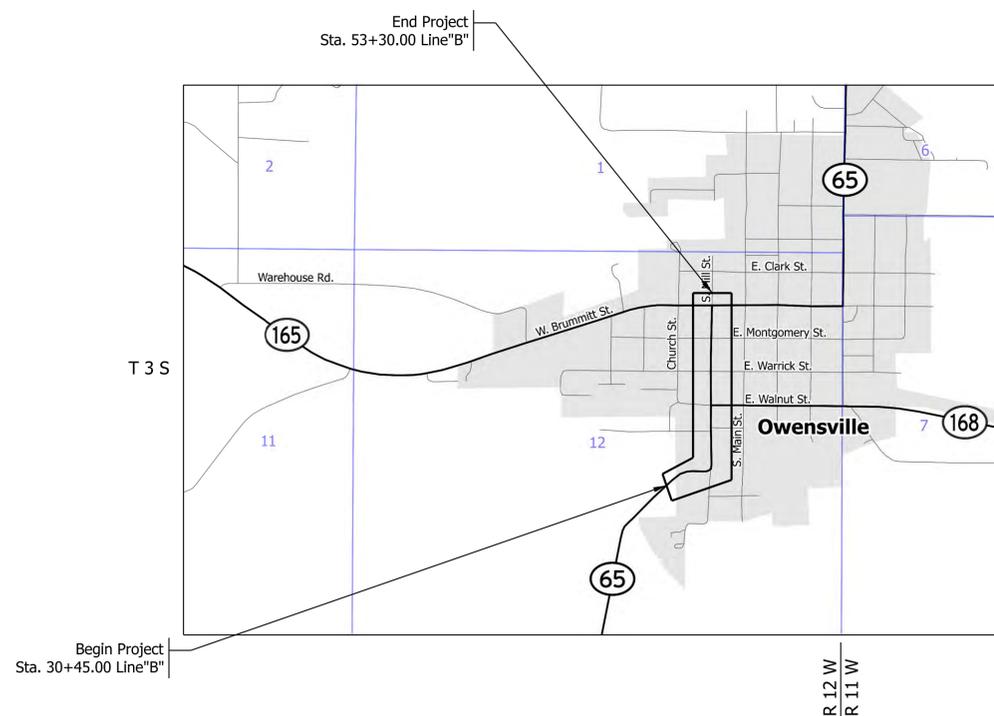
TRAFFIC DATA		
A.A.D.T.	(2025)	2620 V.P.D.
A.A.D.T.	(2045)	3668 V.P.D.
D.H.V	(2045)	454 V.P.H.
DIRECTIONAL DISTRIBUTION		51/49 %
TRUCKS		10 % A.A.D.T. 4.5 % D.H.V.
DESIGN DATA		
DESIGN SPEED	20, 25, 30, 35 M.P.H.	
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)	
FUNCTIONAL CLASSIFICATION	MAJOR COLLECTOR	
RURAL/URBAN	URBAN/RURAL	
TERRAIN	LEVEL	
ACCESS CONTROL	NONE	



LATITUDE: 36° 16' 10" N LONGITUDE: 87° 41' 34" W

GROSS LENGTH:	0.46 MI.
NET LENGTH:	0.46 MI.
MAX. GRADE:	N/A %

HMA Overlay Minor Structural
 From: 0.26 mi. S. SR 168 (S. Limits Owensville) To: SR 165
 Section 12, T-3-S, R-12-W, Montgomery Township, Gibson County, Indiana



INDIANA DEPARTMENT OF TRANSPORTATION
 STANDARD SPECIFICATIONS DATED 2024
 TO BE USED WITH THESE PLANS

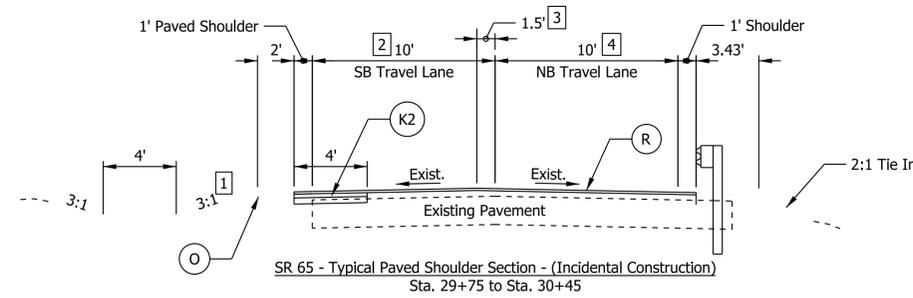


WSP USA Inc.
 115 W. Washington Street
 Suite 1270S
 Indianapolis, IN 46204
 TEL: 317-972-1706

APPROVED: _____ CHIEF, DIVISION OF REAL ESTATE DATE _____
 RECOMMENDED FOR APPROVAL: _____ MANAGER, R/W ENGINEERING SECTION DATE _____

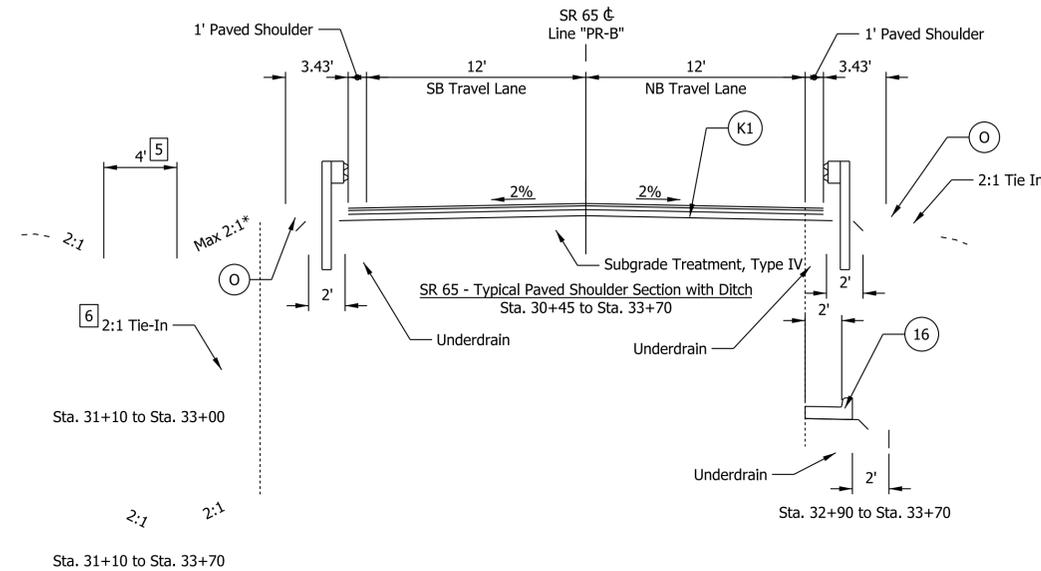
SURVEY BOOK	SHEETS	
	1	of 20
CONTRACT	PROJECT	
R-43253	2001922	

- 1 Ditch Slope
Varies - See Ditch Profile
- 2 SB Travel Lane
10' to 12' - Sta. 30+05 to Sta. 30+45

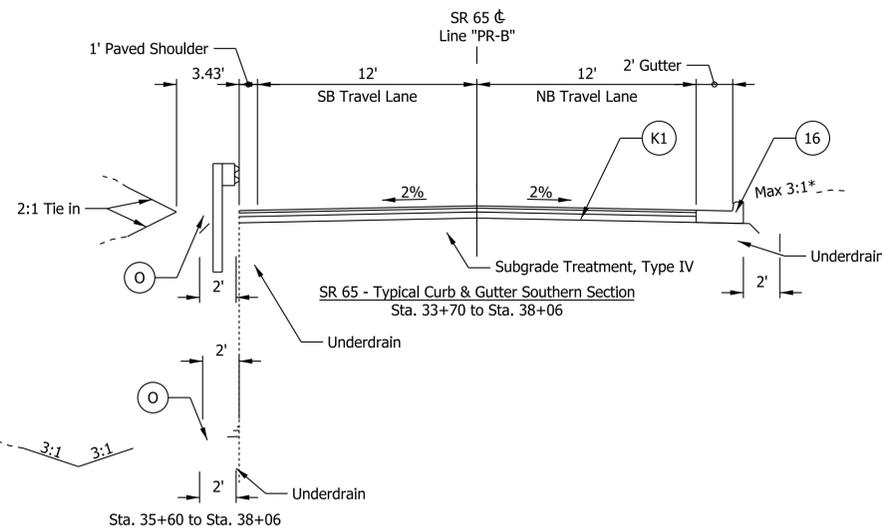


- 3 Crown Shift Right
Varies 1.5' to 0' - Sta. 29+75 to Sta. 30+05
- 4 NB Travel Lane
10' to 12' - Sta. 30+05 to Sta. 31+45

- 5 Ditch Width
4' to 8' - Sta. 30+80 to Sta. 31+10
- 6 Ditch Slope
Varies for Pond Tie-In - See Cross Sections



- 1 Crown Shift Right
Varies 1' to 0.50' - Sta. 30+45 to Sta. 31+45
- 3 NB Travel Lane
10' to 12' - Sta. 30+45 to Sta. 30+86



*Varies. Max. Value Provided. See Cross Sections for additional details.

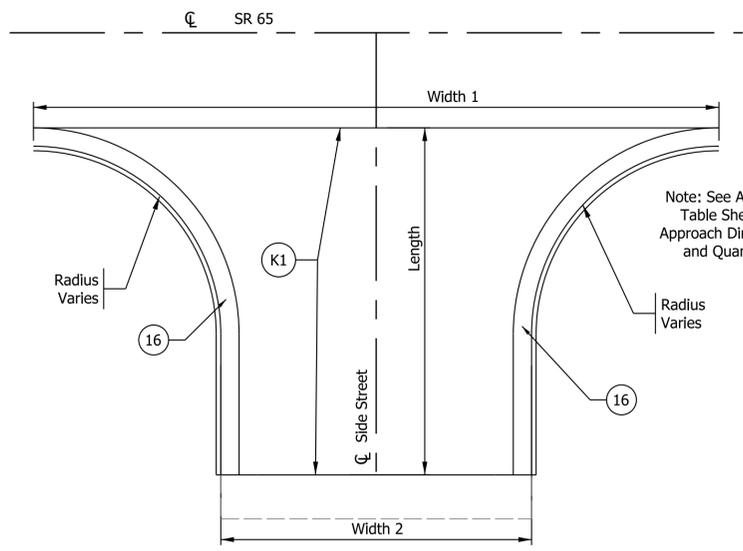
LEGEND	
(F) Sidewalk, Concrete	(K1) Full Depth Reconstruction with HMA: 165 lb/sys QC/QA-HMA, 3, 58H, Surface, 9.5 mm on 275 lb/sys QC/QA-HMA, 3, 58H, Intermediate, 19.0 mm on 440 lb/sys QC/QA-HMA, 3, 58S, Base, 19.0 mm on Drainage Layer Consisting of 250 lb/sys QC/QA-HMA, 4, 58E, Intermediate, OG, 19.0 mm on Separation Layer Consisting of 4.0 in. Compacted Aggregate, No. 53 on Subgrade Treatment, Type IV
(13) Curb, Concrete	(K2) HMA for Shoulder Widening: 165 lb/sys QC/QA-HMA, 3, 58H, Surface, 9.5 mm on 275 lb/sys QC/QA-HMA, 3, 58H, Intermediate, 19.0 mm on 440 lb/sys QC/QA-HMA, 3, 58S, Base, 19.0 mm on
(16) Curb and Gutter, Concrete	(O) Compacted Aggregate, No. 53
(26) Sodding, Nursey	(R) Mill and Overlay for Incidental Construction 1.5" Asphalt Milling 165 lb/sys QC/QA-HMA, 3, 58H, Surface, 9.5 mm on existing pavement

NOT FOR CONSTRUCTION

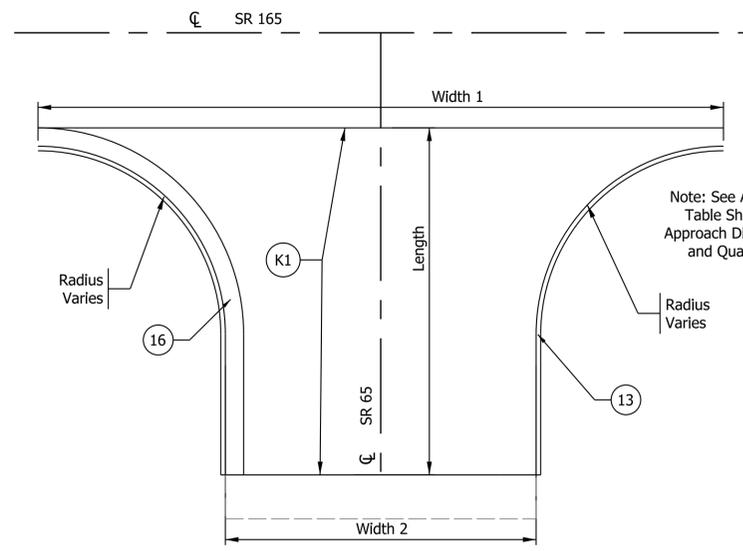
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: EPF	DRAWN: EPF	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS	

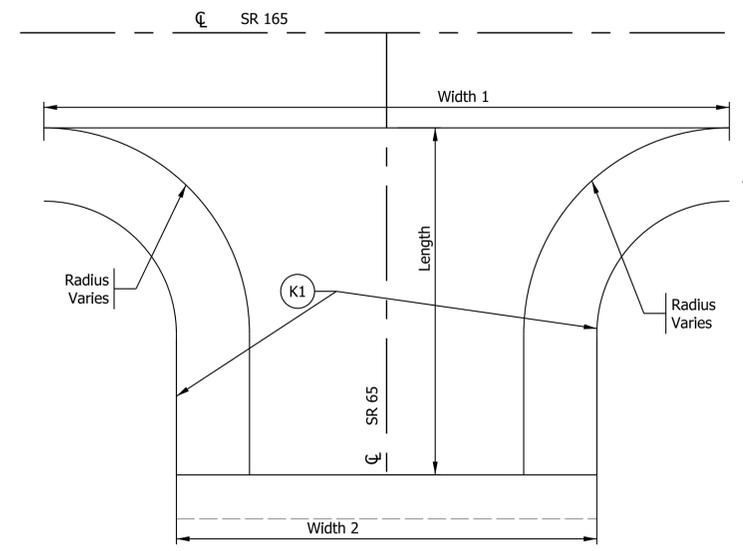
HORIZONTAL SCALE	BRIDGE FILE
1" = 5'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	3 of 20
CONTRACT	PROJECT
R-43253	2001922



TYPICAL SIDE STREET APPROACH DETAIL
(NOT TO SCALE)

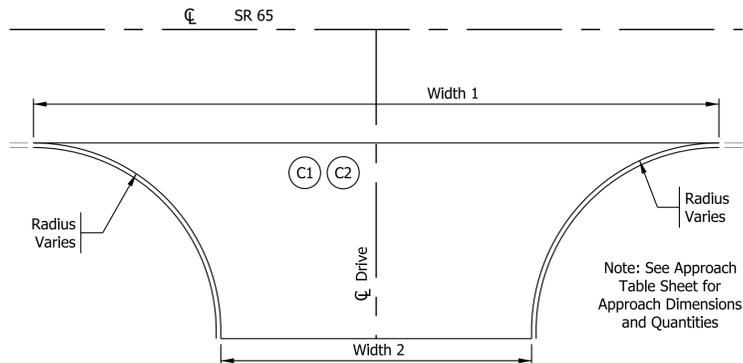


TYPICAL SIDE STREET APPROACH DETAIL
(NOT TO SCALE)

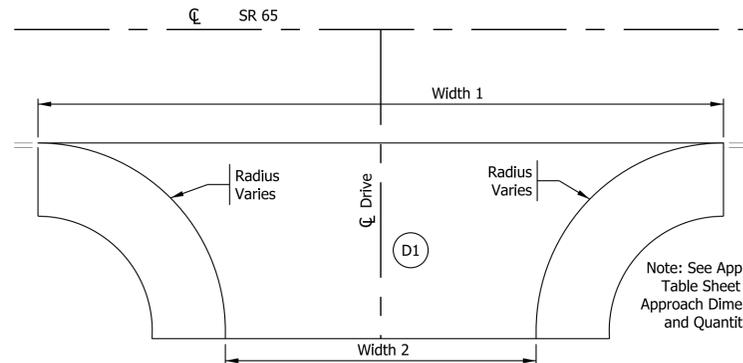


TYPICAL SIDE STREET APPROACH DETAIL
(NOT TO SCALE)

Note: See Approach Table Sheet for Approach Dimensions and Quantities

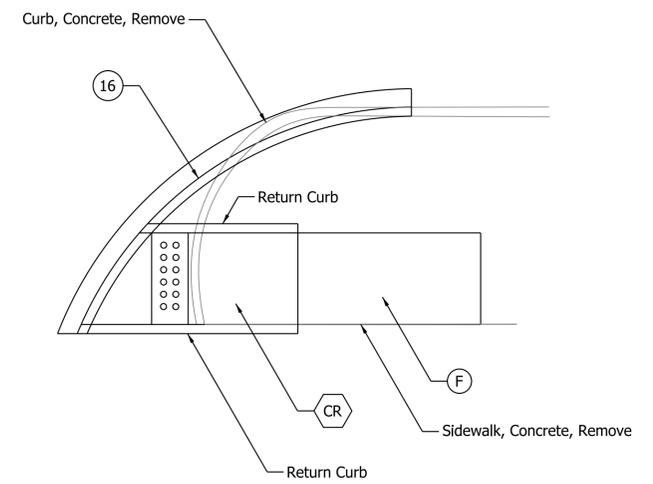


TYPICAL DRIVEWAY APPROACH DETAIL
(NOT TO SCALE)

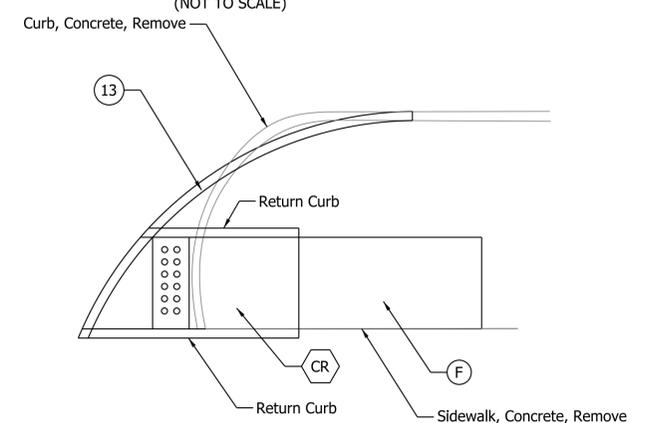


TYPICAL DRIVEWAY APPROACH DETAIL
(NOT TO SCALE)

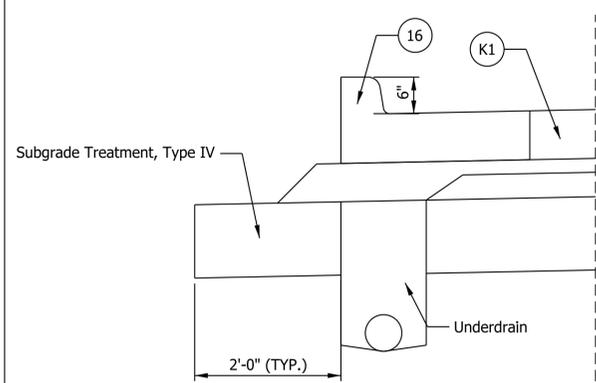
Note: See Approach Table Sheet for Approach Dimensions and Quantities



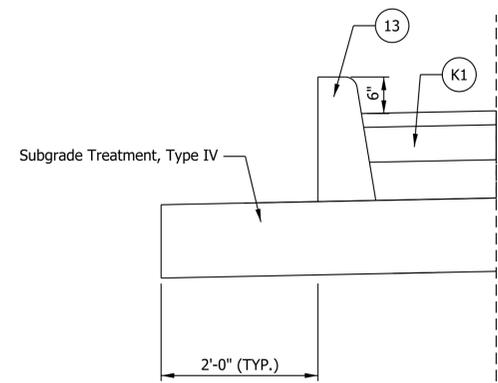
TYPICAL CURB RAMP DETAIL
(NOT TO SCALE)



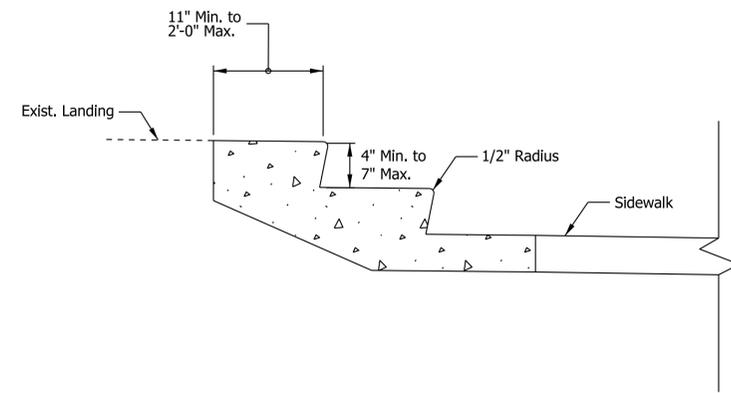
TYPICAL CURB RAMP DETAIL
(NOT TO SCALE)



CURB AND GUTTER, CONCRETE - HMA ADJACENT
(NOT TO SCALE)



CURB, CONCRETE - HMA ADJACENT
(NOT TO SCALE)



CONCRETE STEPS DETAIL
(NOT TO SCALE)

Plot: 2/10/2025 1:27 PM

- LEGEND**
- (F) Sidewalk, Concrete
 - (13) Curb, Concrete
 - (16) Curb and Gutter, Concrete

(K1) Full Depth Reconstruction with HMA:
165 lb/sys QC/QA-HMA, 3, 58H, Surface, 9.5 mm on
275 lb/sys QC/QA-HMA, 3, 58H, Intermediate, 19.0 mm on
440 lb/sys QC/QA-HMA, 3, 58S, Base, 19.0 mm on
Drainage Layer Consisting of
250 lb/sys QC/QA-HMA, 4, 58E, Intermediate, OG, 19.0 mm on
Separation Layer Consisting of
4.0 in. Compacted Aggregate, No. 53 on
Subgrade Treatment, Type IV

(C1) PCCP for Approaches, 6 IN. on
Dense Graded Subbase, 6" on
Subgrade Treatment, Type II

(C2) PCCP for Approaches, 9 IN. on
Dense Graded Subbase, 6" on
Geogrid, Type IB
Subgrade Treatment, Type II

(D1) HMA for Approaches, Type B composed of:
165 lb/syd HMA Surface Type B, on
275 lb/syd HMA, Intermediate, Type B, on
660 lb/syd HMA Base, Type B, on
Subgrade Treatment, Type II on
Geogrid, Type IB

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS DETAILS

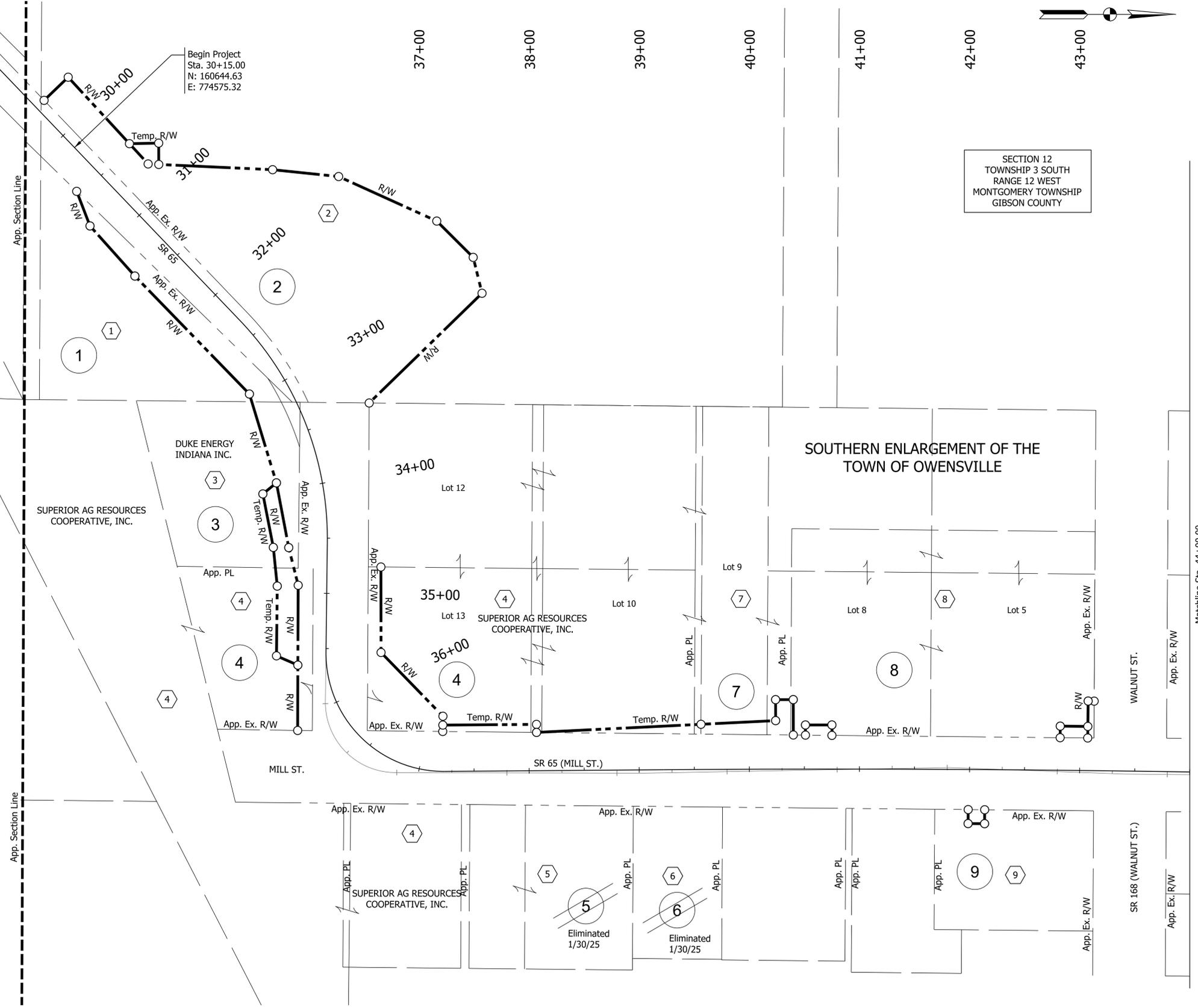
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N/A	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	4 of 20
CONTRACT	PROJECT
R-43253	2001922



4
SUPERIOR AG RESOURCES
COOPERATIVE, INC.

Begin Project
Sta. 30+15.00
N: 160644.63
E: 774575.32

SECTION 12
TOWNSHIP 3 SOUTH
RANGE 12 WEST
MONTGOMERY TOWNSHIP
GIBSON COUNTY



- 1 BRIAN L. ROBERTS
- 2 JOE L. NEWTON
- 3 DUKE ENERGY INDIANA INC.
- 4 SUPERIOR AG RESOURCES COOPERATIVE, INC.
- 5 SCOTT ST. CLAIRE, SR.
- 6 S & G ENTERPRISE INC.
- 7 RITA J. ANGLE
- 8 THE OWENSVILLE MONTGOMERY TOWNSHIP PARK AND RECREATION DEPARTMENT
- 9 DONALD W. BARRET AND BARBARA K. BARRETT

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____ DATE _____
DESIGNED: _____	DRAWN: _____
CHECKED: _____	CHECKED: _____

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**S.R. 65 RECONSTRUCTION
PLAT NO. 1**

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION 2001922
SURVEY BOOK	SHEETS 5 of 20
CONTRACT R-43253	PROJECT 2001922



45+00

46+00

47+00

48+00

49+00

50+00

51+00

52+00

53+00

54+00

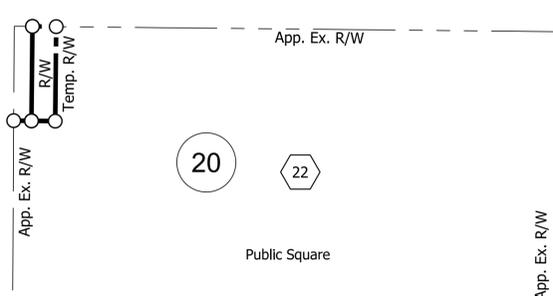
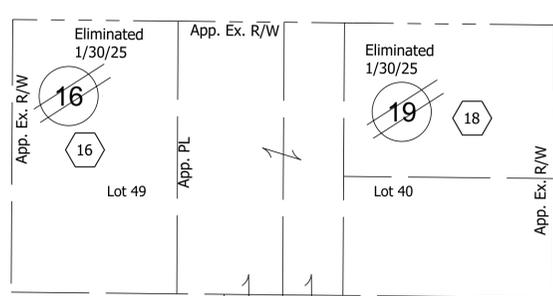
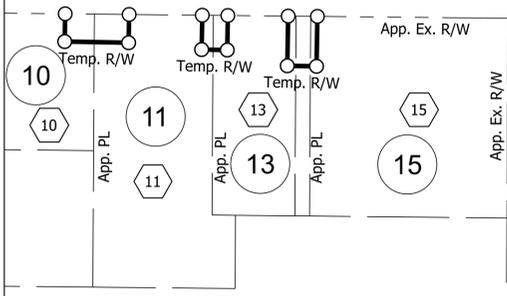
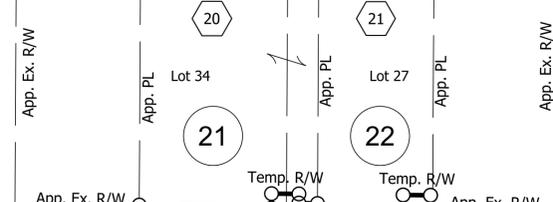
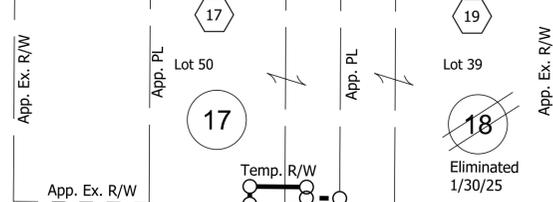
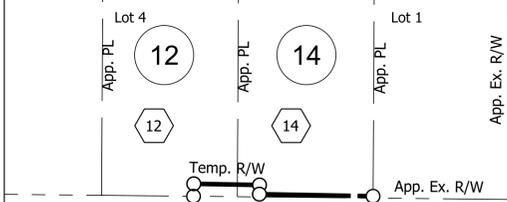
55+00

SECTION 12
TOWNSHIP 3 SOUTH
RANGE 12 WEST
MONTGOMERY TOWNSHIP
GIBSON COUNTY

Matchline Sta. 44+00.00

SOUTHERN ENLARGEMENT OF THE
TOWN OF OWENSVILLE

PLAT OF THE TOWN OF OWENSVILLE



SR 65 (MILL ST.)

SR 165 (W. BRUMMITT ST.)

SR 65 (W. BRUMMITT ST.)

End Project
Sta. 54+70.22
N: 162727.67
E: 775154.77

- 10 BRIAN K. AND LESA D. PRATER
- 11 DENNIS A. AND ANDREA R. MILLS
- 12 KEVIN E. AND SANDRA M. MOORE
- 13 WOODROW TINDLE
- 14 DENNIS A. & ANDREA R. MILLS
- 15 MATTHEW R. & NICOLE D. BILDERBACK
- 16 JONATHAN A. AND AMY M. PRITCHETT
- 17 DANNY R. AND MONA L. TAYLOR
- 18 MONTGOMERY MANUFACTURING CO INC.
- 19 ROBIN KUMAR CHAUDHURI
- 20 WILLIAM T. JOHNSON
- 21 EARLAINE DUNIGAN
- 22 BOARD OF TRUSTEES OF THE OWENSVILLE CARNEGIE LIBRARY

NOT FOR
CONSTRUCTION

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____

DESIGNED: _____ DRAWN: _____

CHECKED: _____ CHECKED: _____

INDIANA
DEPARTMENT OF TRANSPORTATION

S.R. 65 RECONSTRUCTION
PLAT NO. 1

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
N/A	2001922
SURVEY BOOK	SHEETS
	\$PAGE\$ of \$TOTAL\$
CONTRACT	PROJECT
R-43253	2001922

LEGEND

	ALIGNMENT	APP. APPARENT
	APPARENT CENTERLINE	EX. EXISTING
	APPARENT SECTION LINE	R/W RIGHT-OF-WAY
	EXISTING R/W	P.L. PROPERTY / BOUNDARY LINE
	APPARENT R/W	APP. LOT LINE APPARENT LOT LINE
	APPARENT PROPERTY LINE	APP. PLAT LINE APPARENT SUBDIVISION / PLAT LINE
	APPARENT LOT LINE	B.A. BEGIN ALIGNMENT
	APPARENT PLAT / SUBDIVISION LINE	E.A. END ALIGNMENT
	SURVEY LINE / APP. DEED LINE	P.C. POINT OF CURVATURE
	TOPOGRAPHIC FEATURES	P.I. POINT OF INTERSECTION
	CHAINLINK FENCE	P.T. POINT OF TANGENCY
	L.C.R.S. LIMITS / MATCH LINES	P.O.T. POINT ON TANGENT
	POINT / MONUMENT NUMBER I.D.	STA. ALIGNMENT STATION
	ETICA SURVEY CONTROL	EQ. STATION EQUATION
	- BLUE CAP STAMPED "ETICA GROUP CONTROL FIRM #0121"	L LENGTH
	ALIGNMENT POINT (RECOVERED, CALC., OR SET)	R RADIUS
	- WASHER STAMPED "ETICA GROUP ALIGNMENT FIRM #0121"	T TANGENT
	RECOVERED MISC. MONUMENT	A.G. ABOVE GRADE
	APP. USPLSS SECTION CORNER LOCATION	B.A. BELOW GRADE
	RECOVERED RIGHT-OF-WAY MONUMENT	RT. RIGHT
		DIA. DIAMETER
		LT. LEFT
		MON. MONUMENT
		USPLSS SECTION CORNER DELINEATION SYMBOL "##" INDICATES SECTION NUMBER

LOCATION CONTROL ROUTE SURVEY

FOR INDIANA DEPARTMENT OF TRANSPORTATION
STATE ROAD 65, TOWN OF OWENSVILLE
GIBSON COUNTY, INDIANA
DES 2001922

APPARENT PROPERTY LINE NOTE (OUTSIDE LIMITS)
ADJOINING SURVEY TRACTS' APPARENT PROPERTY LINES THAT FALL OUTSIDE THESE SURVEY LIMITS WERE NOT ESTABLISHED AND THEREFORE ARE TYPICALLY NOT SHOWN ON THIS LC.R.S. IF SAID "BACK" OR "REAR" APPARENT PROPERTY LINES ARE SHOWN ON THIS SURVEY, THEY ARE SHOWN FOR REFERENCE AND ILLUSTRATION ONLY.

PROJECT LOCATION



VICINITY MAP (NOT TO SCALE)



STATE MAP (NOT TO SCALE)

ALIGNMENT POINT REFERENCES (NOT TO SCALE)

510 SEE METADATA TABLE CENTER OF WATER SHUT OFF VALVE 33.27 MAG NAIL WITH "ETICA ALIGN" WASHER MAG NAIL 6 IN. A.G. IN NW FACE OF POWER POLE 65.1 246° 23.8W	511 SEE METADATA TABLE MAG NAIL WITH "ETICA ALIGN" WASHER WEST CORNER OF INLET AT BACK OF CURB 23° 23.5P	512 SEE METADATA TABLE POST FOR STOP SIGN 38.21 MAG NAIL WITH "ETICA ALIGN" WASHER SW CORNER OF POST FOR NO PARKING SIGN 51.51
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CONTROL POINT REFERENCES (NOT TO SCALE)

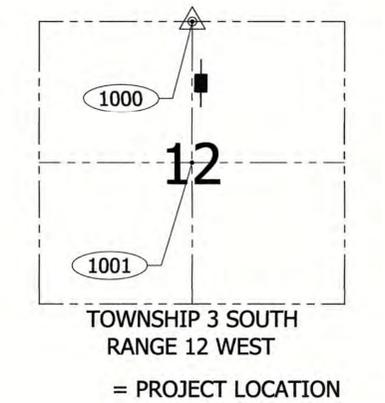
105 SEE METADATA TABLE SW CORNER OF POST FOR NO PARKING SIGN 359° 7.58 CUT "C" IN CONC. WALK EAST FACE OF FLAG POLE 246° 17.6P POST OFFICE	106 SEE METADATA TABLE MAG NAIL 12 IN. A.G. IN NE FACE OF POWER POLE 67.1423 386° 56.16T N. MILL ST. CONC. WALK EAST FACE OF POST FOR 20 MPH SIGN 186° 70.72T	207 SEE METADATA TABLE SE CORNER OF POST FOR JCT 1168 SIGN 331° 88.68T SW CORNER OF CONC. WALK 98° 48.46T 5/8 IN. DIA. REBAR WITH "ETICA CONTROL" CAP MAG NAIL 12 IN. A.G. IN WEST FACE OF POWER POLE 67.1033 180° 63.34T	208 SEE METADATA TABLE SE CORNER OF HOUSE AT 308 S. MILL ST. 321° 24.52T CUT "C" IN CONC. WALK BENCH TIE 12 IN. A.G. IN EAST FACE OF POWER POLE 67.1010 30° 94.00P TOP NUT OF FIRE HYDRANT 185° 67.64T
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USPLSS SECTION CORNER REFERENCES (NOT TO SCALE)

1000 SEE METADATA TABLE

NO REFERENCES AVAILABLE

SURVEY POINT METADATA TABLE (SEE SURVEYOR'S REPORT FOR COORDINATE SYSTEM INFORMATION)						
POINT NO.	NORTHING	EASTING	LATITUDE	LONGITUDE	POINT DESCRIPTION	MONUMENT DESCRIPTION
105	162632.5848	775134.0594	N038°16'20.0891"	W087°41'33.8055"	ETICA SURVEY CONTROL	CUT X
207	162128.2540	775169.6309	N038°16'15.1038"	W087°41'33.3566"	ETICA SURVEY CONTROL	5/8 IN. DIA. REBAR W/ "ETICA CONTROL" CAP
208	161625.2008	775117.3868	N038°16'10.1309"	W087°41'34.0087"	ETICA SURVEY CONTROL	CUT X
510	161966.4086	775146.8083	N038°16'13.5039"	W087°41'33.6418"	STA. 47+29.23, P.I. LINE "B"	RAILROAD SPIKE 0.40" B.G.
511	162328.4831	775150.6179	N038°16'17.0831"	W087°41'33.5961"	STA. 50+91.33, P.I. LINE "B"	MAG SPIKE RECOVERED FLUSH
512	162687.4350	775154.3481	N038°16'20.6314"	W087°41'33.5514"	STA. 54+50.30, P.O.T. LINE "B"	MAG NAIL W/ "ETICA ALIGN" WASHER
1257	162073.4886	775114.9975	N038°16'14.5622"	W087°41'34.0413"	STA. 48+35.97, 32.94" LT	5/8" DIA. REBAR W/ KUESTER CAP 1" B.G.
1428	162725.1170	775047.0510	N038°16'21.0034"	W087°41'34.8971"	STA. 35+45.06, 1,852.10 LT	5/8" DIA. REBAR W/ KUESTER CAP 1" B.G.



USPLSS SECTION MAP (MAP NOT TO SCALE)

SR 165 (BRUMMITT STREET) E.A. = STA. 54+50.30 "B"

GRASON IRISH HOLDINGS LLC INSTR. NO. 20160001045

APP. P.L. LOT 27 EARLAINE DUNIGAN INSTR. NO. 20130003644 APP. P.L.

WILLIAM T. JOHNSON INSTR. NO. 20180000196 (TDD) INSTR. NO. 200500001946 (WD)

PLAT OF THE TOWN OF OWENSVILLE PLAT BOOK 1, PAGE 3

ROBERT L. MOORE INSTR. NO. 200400004806

SR 65 (MILL STREET) 53+00

APP. R/W

SR 65 35° 43' E 358.97'

PLAT OF THE TOWN OF OWENSVILLE PLAT BOOK 1, PAGE 3

NEPL

P.I. = STA. 50+91.33 "B"

W. MONTGOMERY ST.

DEREK J MAGGARD INSTR. NO. 201900002657 LOT 39

NEPL

PLAT OF THE TOWN OF OWENSVILLE PLAT BOOK 1, PAGE 3

DANNY R. AND MONA L. TAYLOR INSTR. NO. 19980000365

LOT 50 APP. P.L.

JAMES S. BAKER IV INSTR. NO. 199700006264

NEPL

PLAT OF THE TOWN OF OWENSVILLE PLAT BOOK 1, PAGE 3

LOT 49 JONATHAN A. AND AMY M. PRITCHETT INSTR. NO. 201100001833

SR 65 36° 10' E 362.09'

LINE "B"

APP. R/W

W. WARRICK ST.

WALTER E. III AND KRISTINA E. HARPER INSTR. NO. 200300009241 LOT 1

APP. P.L.

JUSTIN L. AND TOSHA R. SMITH INSTR. NO. 201400005147

APP. R/W

SOUTHERN ENLARGEMENT PLAT BOOK 1, PAGE 46

KEVIN E. AND SANDRA M. MOORE INSTR. NO. 199400004741

APP. P.L.

LOT 4 JOSH DILBECK INSTR. NO. 201900004815

SR 65 (MILL STREET) 46+00

APP. R/W

SR 65 34° 42' E 367.64'

APP. R/W

C & K PARTNERSHIP, L.P. INSTR. NO. 201600004704

SEE SURVEYOR'S REPORT

WOODROW TINDLE INSTR. NO. 201900003254

NEPL

DENNIS A. AND ANDREA R. MILLS INSTR. NO. 201500004294

APP. P.L.

BRIAN K. AND LESA D. PRATER INSTR. NO. 199900002007

SR 65 34° 42' E 367.64'

APP. R/W

WALNUT ST.

SEE SURVEYOR'S REPORT

P.I. = STA. 43+61.59 "B"

SEE SHEET NO. 1 FOR CONTINUATION

<p>SURVEY STARTED 02/18/2022</p> <p>SURVEY COMPLETED 03/29/2022</p> <p>ROUTE PLAT SHEETS 2 OF 3</p>	<p>PREPARED BY: Etica GROUP</p> <p>8720 CASTLE CREEK PARKWAY EAST DR. SUITE 400 INDIANAPOLIS, IN 46250</p>	<p>Redact Statement: I affirm, under the penalties of perjury, that I have taken reasonable care to redact each Social Security Number in this document, unless required by law.</p> <p>Signature: <i>Rodney J. Kelly</i></p> <p>Printed Name: <u>RODNEY J. KELLY</u></p>	<p>FIELD SURVEYOR STATEMENT</p> <p>THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF TITLE 865 I.A.C. 1-12-20 THROUGH 1-12-25 REGARDING ROUTE SURVEYS, EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT A PART OF THIS SURVEY.</p>	<p>PREPARED BY: <i>Rodney J. Kelly</i> RODNEY J. KELLY 03/29/2022</p> <p>LEAD CREW: BLC CHECKED: CLC</p> <p>DRAWN: CDM</p>	<p>INDIANA DEPARTMENT OF TRANSPORTATION LOCATION CONTROL ROUTE SURVEY</p> <p>STATE ROAD 65 - LINE "B"</p>	<p>SCALE SEE ABOVE</p> <p>COUNTY GIBSON</p> <p>SURVEY BOOK ---</p> <p>CONTRACT ---</p>	<p>BRIDGE FILE ---</p> <p>DESIGNATION 2001922</p> <p>SHEETS 8 20</p> <p>ETICA PROJECT NUMBER 210045.03</p>
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LOCATION CONTROL ROUTE SURVEY

FOR INDIANA DEPARTMENT OF TRANSPORTATION
STATE ROAD 65, TOWN OF OWENSVILLE
GIBSON COUNTY, INDIANA
DES 2001922

Surveyor's Report

The Etica Group, Inc. (Etica Group) Project No. 210045.03

General Description and Location:

In the town of Owensville, a route within Section 12, Township 3 South, Range 12 West, Montgomery Township, Gibson County, Indiana along State Road 65.

Intent of Survey:

The purpose of this survey was to provide the client (WSP USA) a Location Control Route Survey (LCRS) plat as defined in Title 865 of the Indiana Administrative Code as a "Route Survey" using the Indiana Geospatial Coordinate System (InGCS). This is NOT a boundary survey (as defined in Title 865 as an "original survey" or a "retracement survey") and should therefore not be treated as such. The survey limits, along State Road 65, beginning approximately 450 feet west of the intersection of State Road 65 and South Mill Street and ending approximately 2,400 linear feet to the intersection of State Road 65 and State Road 165 (Brummitt Street).

Project Control and Basis of Bearings:

InGCS "Gibson" zone:
Unless noted otherwise, all bearings, distances, areas, and coordinates shown hereon are based upon the Indiana Geospatial Coordinate System's (InGCS) "Gibson" zone per NAD 83 (2011) epoch 2010.00 and are reported in U.S. Survey Feet and decimal parts thereof. The zone was developed to minimize the differences between ground-measured horizontal distances and the corresponding grid coordinate (map) distances within the counties bearing these zone names.

The InGCS zone coordinates and associated metadata are listed to facilitate a seamless workflow between various geospatial industries (surveying, civil engineering, construction, environmental, GIS, etc.), to provide a means by which to enable the re-establishment of the survey lines, i.e. alignments (should any monuments on them be destroyed), and to provide a mechanism for the perpetuation of any boundary corner monuments included in this survey, e.g., USPLS, Grant, Donations, local subdivisions and/or tracts. To preserve the relative positional relationships of these surveyed lines and/or boundary corner monuments in future surveying, construction, maintenance, etc. activities, it is recommended to utilize the positions associated with the physical "Survey Network Control Points" listed in the "Reported Project Points Metadata Table" that were set and/or found during this survey.

Metadata:

(N.A.D 1983) InGCS "Gibson" Zone Parameters
Geometric Datum: NAD 83(2011) epoch 2010.00
Units: U.S. Survey Feet
Projection Type: Transverse Mercator
Central Meridian: 87°39'00" west longitude
Central Meridian scale factor: 1.000013
Latitude of Grid Origin: 38°09'00" north latitude
False Northing: 36,000,000 m (118,110.00 U.S. Ft)
False Easting: 240,000,000 m (787,400.00 U.S. Ft)

The control system parameters used for this survey were provided by INDOT. Etica Group verified the system with an GPS observation on NGS / InGCS Monument PID "JA1598" (Des. L 356).

Published Coordinates	Observed Coordinates	Delta(Δ) Value
Northing: 126,343.1877	Northing: 126,343.1916	+ 0.0039
Easting: 839,428.1639	Easting: 839,428.1959	+ 0.0320

The monument was verified utilizing the INDOT Continuously Operating Reference Station (INCOBS) a real-time kinematic (RTK) correction service over the internet.

Referenced Documents, Surveys, and Plans:

All record descriptions, recorded documents, and platted land (plats and subdivisions) were obtained from the Office of the Recorder of Gibson County, Indiana, unless noted otherwise. Resources and referenced documents found or obtained for this survey that were used to help establish the apparent boundary lines, plat lines, survey lines, alignments, and / or right-of-way lines for this survey are as follows:

- Construction, Road, and Right-of-Way Plans
- County Surveyor's Section Corner Records, References and Misc. Documents.
- Recorded Surveys
- Subdivision Plats
- Deeds (as referenced on LCRSP)
- Gibson County GIS
- Google Earth

*This may not be a comprehensive list of all documents researched for or found for this project.

INDOT Research and Documentation:

- The Indiana Department of Transportation (INDOT) research request form was submitted (said request included construction plans, right-of-way plans, and grant/deeds) for this survey per INDOT's website.

- Indiana Department of Transportation Location Route Survey
 - o Instrument No. 20060005769, performed by USI, Consultants (RW-4897)*

*No right-of-way transfer documents (grants or warranty deeds) were received from INDOT upon multiple requests.

Alignments:

The bearing of Line B per project Indiana Department of Transportation Location Route Survey Instrument No. 20060005769, performed by USI, Consultants (RW-4897)* North 00 Degrees 55 Minutes 02 Seconds East equates/translates to North 00 Degrees 36 Minutes 10 Seconds East between Warrick Street and Montgomery Street. Stationing was established at Points 510 being held P.I. 47+29.23 "Line B" and 511 being held at P.I. 50+91.33 "Line B". A best fit between the right-of-way plan geometric points as referenced from this alignment and the recovered monuments

to correlate Line "D" into this survey's coordinate system / basis of bearing. There were minimal discrepancies between this information stated and the monuments/bearing of the USI Location Control Route Survey "Line B".

Right-of-way monuments recovered overall fell within reason of the right-of-way lines based on recovered monuments and physical possession and therefore Line "B" was held. See the "Observations and Opinions - Availability and Condition of Reference Monuments" section of this report and the metadata tables regarding the information pertaining to the recovered right-of-way monuments.

General Notes:

Etica Group is a sub consultant to WSP USA.

Existing monuments recovered and discussed within this report (said points depicted on the plat pages, alignment, and control references and in the coordinate table) represent true and accurate locations as depicted, corresponding with distances and/or azimuths/bearings (if referenced).

All control points set were referenced in accordance with Indiana Department of Transportation Design Manual for Location Surveys Part III. Other points recovered but not shown, may be close, but were not within the estimated uncertainty nor did they have any impact on the establishment of lines as shown herein. Secondary control points may have been set for the use in locating or validating other evidence when establishing rights of way or apparent property/boundary lines.

As used in this survey, the term "certify" means to state or declare a professional opinion of conditions regarding those facts or findings which are the subject of the certification and does not constitute a warranty or guarantee, either expressed or implied. The surveyor can only certify to those items that are visible and/or things that can be observed, measured, or counted.

US Public Land Survey System (USPLSS) Section Line Determination and Recovered Monuments:

The recovery of apparent section corner monuments for Section 12, Township 3 South, Range 12 West, worthy of mention are as follows:

Point No. 1000: A Stone Monument was recovered 30" below grade per Township 3 South Range 12 West Index for Section Corner Perpetuation Program tie sheet. There is minimal uncertainty with this point and fit generally well in relationship to survey and deed descriptions obtained near the section line for which this monument was utilized.

Point No. 1001: A 2" Diameter Iron Pipe was recovered 12" above grade per Township 3 South Range 12 West Index for Section Corner Perpetuation Program tie sheet. There is minimal uncertainty with this point and fit generally well in relationship to survey and deed descriptions obtained near the section line for which this monument was utilized.

The apparent south line of the northwest corner of Section 12 was established by utilizing the geometry in the legal descriptions describing Instrument 20180002139 (Newton), 201300003658 (Roberts), and the surveys recorded as instrument no. 202100003071, 202100004266, and 199900009066, along with physical monuments as noted on the survey with respect to those parcels.

Lines of Possession / Right-of-way Notes

Regarding "lines of occupation or possession" in the above section, this term refers to lines which are marked by visible, obvious, well-defined, and maintained, man-made or placed objects or limits, such as, but not limited to fences, hedges, mow lines, field-divide lines, woods lines, retaining walls, etc. The uncertainty cited for a line of occupation is general in nature and in not intended to be specific for every point along the line. Therefore, portions of the occupation lines may vary from the surveyed lines by distances greater or less than the uncertainty cited in this report or shown on the plat of survey.

The existing right-of-way lines, parcel ownership lines, and ownership information shown on this plat are preliminary and subject to change based on verification/validation of title. This information is shown only to help orient the user of this plat as it relates to project centerlines and is not intended to establish existing right-of-way lines or ownership lines. The source used to plot apparent right-of-way lines and depict (not define) land ownership shown on this plat is from the last/current deeds of record obtained from the appropriate County Recorder's Office or other state agencies when the research was performed, as well as from other sources cited on this plat. Where no record right-of-way could be found, the edge of pavement shown may possibly act as an easement by prescription. The designer/consultant responsible for Right-of-Way Engineering is to complete the final determination of this information. It is always presumed that there are rights associated to all areas where right-of-way monuments or possession exist; however, documentation is not always provided to show the extent of those rights. No title search or encumbrance report was performed for this project.

Any depiction of possible intrusion, trespass, invasion, and/or possible encroachment into the possessions or rights of another is not a matter of this survey. An attorney and/or title company should be consulted in all matters with respect to any rights of possession, matters of title, and all things with respect to any encumbrances if depicted on this survey.

Apparent Boundary Lines / Lines of Possession / Right-of-Way Notes:

The recorded last deed of record descriptions of nearby tracts located within the survey limits, recorded subdivision plats, and supplemental documents (as listed on the LCRS) were used to establish the apparent boundary lines and right-of-way lines.

Regarding "lines of occupation or possession" in the above section, this term refers to lines which are marked by visible, obvious, well-defined, and maintained, man-made or placed objects or limits, such as, but not limited to fences, hedges, mow lines, field-divide lines, woods lines, retaining walls, etc. The uncertainty cited for a line of occupation is general in nature and in not intended to be specific for every point along the line. Therefore, portions of the occupation lines may vary from the surveyed lines by distances greater or less than the uncertainty cited in this report or shown on the plat of survey. When fences or other lines of occupation are shown on the plat, they have been located only at the ends or specific locations noted; therefore, for the purpose of this survey, such lines are assumed to run straight between said locations but may vary slightly from such straight line.

The existing right-of-way lines, parcel ownership lines, and ownership information shown on this plat are preliminary and subject to change based on verification/validation of title. This information is shown only to help orient the user of this plat

as it relates to project centerlines and is not intended to establish existing right-of-way lines or ownership lines. The source used to plot existing right-of-way lines and ownership lines shown on this plat is from the last/current deeds of record obtained from the appropriate County Recorder's Office or other state agencies when the research was performed, as well as from other sources cited on this plat. Where no record right-of-way could be found, the edge of pavement shown may possibly act as an easement by prescription. The designer/consultant responsible for Right-of-Way Engineering is to complete the final determination of this information. It is always presumed that there are rights associated to all areas where right-of-way monuments or possession exist; however, documentation is not always provided to show the extent of those rights. No title search or encumbrance report was performed for this project.

Observation and Opinions:

In accordance with Title 865, Article 1, Rule 12, Sections 1 through 30 of the Indiana Administrative Code, the following observations and opinions are submitted regarding the various uncertainties in the locations of the lines and corners established on this survey because of A.) availability and condition of reference monuments, B.) occupation or possession lines, C.) clarity or ambiguity of record descriptions and the relationship between their common lines, and D.) the relative positional accuracy of the measurements.

A) - Availability and condition of reference monuments:

The following is a discussion on what monuments were recovered and found in relation to establishing the apparent boundary lines of the subject tracts, dead lines, roadway centerlines and alignments, apparent plat lines, and/or existing right-of-way (if any), hereon referred to as "survey lines". The number and/or letter associated with each monument represents their monument I.D. as depicted on the survey plat.

- Point No. 510: A railroad spike 0.40' below grade was recovered.
- Point No. 511: A mag spike without a washer was recovered.
- Point No. 1250: A 5/8" diameter rebar with a cap that reads "Greg Kissel 2070076" was recovered.
- Point No. 1251: A 5/8" diameter rebar with a cap that reads "Greg Kissel 2070076" was recovered.
- Point No. 1252: A 5/8" diameter rebar with a cap that reads "Greg Kissel 2070076" was recovered.
- Point No. 1253: A 5/8" diameter rebar with a cap that reads "Greg Kissel 2070076" was recovered.
- Point No. 1257: A 5/8" diameter rebar 0.20 below grade.
- Point No. 1401: A 5/8" diameter rebar with a destroyed cap 0.1' above grade.
- Point No. 1402: A 5/8" diameter rebar with a cap that reads "Hennessee" 0.1' below grade.
- Point No. 1404: A 5/8" diameter rebar with a cap that reads "Kuester" 0.1' below grade was recovered.
- Point No. 1428: A 5/8" diameter rebar with a cap that reads "Kuester" 0.1' below grade was recovered.

Bearing and/or distance relationships to recovered monuments on the adjoining properties, roadways, or as located outside the survey limits are shown on this survey for the purpose of establishing a history of monuments and to assist in establishing the "survey lines" as located within the survey limits or subject tracts only. This survey does not represent a survey of any type of said adjoining properties, roadways, or corridors.

B) - Occupation of possession lines:

Regarding "lines of occupation or possession", this term refers to lines which are marked by visible, obvious, well-defined, and maintained, manufactured or placed objects or limits, such as, but not limited to fences, hedges, mow lines, field-division lines, woods lines, retaining walls, etc. The uncertainty cited for a line of occupation is general in nature and in not intended to be specific for every point along the line. Therefore, portions of the occupation lines may vary from the surveyed lines by distances greater or less than the uncertainty cited in this report or shown on the plat of survey. When fences or other lines of occupation are shown on the plat, they have been located only at the ends or specific locations noted; therefore, for the purpose of this survey, such lines are assumed to run straight between said locations but may vary slightly from such straight line.

Lines of possession or lack thereof are denoted with No Evidence of Possession Line (NEPL) or Apparent Possession Line (App. PL).

C) - Clarity or ambiguity of record descriptions:

There is uncertainty and ambiguity between the adjacent tracts of the alignment, particularly in two areas.

The southeast corner of Lot 8 of the Southern Enlargement Plat, Plat Book 1, Page 46 was established by a recovered 5/8" iron rod with a "Keuster" cap (Point No. 1404). The southeast corner of Lot 13 of the Southern Enlargement Plat, Plat Book 1, Page 46 also Superior AG Resources Cooperative, Inc. instrument no. 20080000092 was held to a 5/8" iron rod with a "Greg Kissel #2070076" cap (Point No. 1253). This creates an uncertainty of approximately 18.8 feet on east side and 20.4 feet on west side of Lot 9 (Rita J. Angle, Instr. No. 20030001207). Lot 9 on plat is shown as 66 feet. This also creates an error for the right of way of Walnut Street. The platted width is shown as 66 feet. The calculated width is 41.23 feet at Walnut Street and Mill Street. Lot 5 of the Southern Enlargement Plat appears to be a parking lot off the south side of Walnut Street which would fall inside the 66 foot right of way. No other evidence or documents were recovered to firmly establish these lots and rights-of-way.

There is uncertainty between Walnut Street and Warrick Street. The properties of C&K Partnership, L.P. instrument no. 20160004704 and Woodrow Tindle, instrument no. 20190003254. C&K Partnership, L.P. property begins 4 poles (66 feet) south of the southwest corner of Lot 49 of the Original Plan of the Town of Owensville. Woodrow Tindle property is stated as beginning 182 feet south of southwest corner of Lot 49 of the Original Plan of the Town of Owensville. The south deed line of C&K Partnership, L.P. property is 108 feet south of north deed line. This discrepancy places the mathematical location the southwest corner of C&K Partnership, L.P. property at 174 feet causing a gap of 8 feet between said properties. This scenario physically causes a shift in the blocks on the east side at Walnut Street and Warrick, not agreeing

with platted right-of-way as depicted on westerly approached of the same roads.

D) - The relative positional accuracy of the measurements

The relative positional accuracy of this survey is within the specifications for an Urban Survey: 0.07 feet (21 millimeters) plus 50 parts per million as defined in IAC 865 1-12 and not exceeding 0.5 feet.

Existing Right-of-Way:

There are sections of right-of-way as shown within the survey limits that is considered "existing" and denoted as such on the LCRS. The existing right-of-way was based Highway Plans coupled with monuments recovered and observed with field evidence.

Apparent Right-of-Way:

There are sections of right-of-way denoted within the survey limits that is considered "apparent." Apparent right-of-way is right-of-way established by documents that may not be formal or legal regarding public dedications or transfer of land or title or established by field observed evidence distinguishing use and rights of the public way. The apparent right-of-way as shown on this LCRS was determined based upon Highway, Bridge and / or Right-of-Way plans as listed and documented throughout this report. Apparent right-of-way was established by station and offset referenced from Line "D".

Easements and Regulated Drainage Note:

Locations of any easements shown are based on the last/current deeds of record or recorded plats as obtained from the appropriate County Recorder's Office, as well as from other sources cited on this plat, and should not be interpreted that all possible easements have been recovered because of an exhaustive title search. This LCRS was prepared without benefit of current evidence of source of title (Title Work or Encumbrance Report not performed) for the survey area and therefore subject to any statement of facts revealed by examination of such documents. Easements, legal drains, covenants, right-of-way, etc. shown hereon does not constitute all encumbrances upon these surveyed tracts.

Per the Gibson County Surveyor's Office, GIS Division, no regulated drain is located along the SR 165 route of our project location.

Field Measurements:

Field measurements for this survey were made in accordance with specifications outlined in 865 I.A.C. 1-12. The decimal places shown hereon for measurements and coordinates do not necessarily indicate the precision of the work, but to allow for closure and adjustment by others if desired.

Certificate of Survey:

This survey, to the best of my knowledge and belief, is executed per the provision of Title 865 I.A.C. 1-12-20 through 1-12-25 regarding route surveys, except that any data shown regarding the location or description of the existing parcels is not a part of this survey. Data and information were collected from February 18 to March 29, 2022. The data collected was gathering using electronic total stations applying standard radial surveying techniques and by global positioning equipment, utilizing the INDOT Continuously Operating Reference Station (INCOBS).

SURVEY STARTED		
02/18/2022		
SURVEY COMPLETED		
03/29/2022		
ROUTE PLAT SHEETS		
3	OF	3

PREPARED BY:
Etica GROUP
8720 CASTLE CREEK PARKWAY EAST DR. SUITE 400
INDIANAPOLIS, IN 46250

Redact Statement:
I affirm, under the penalties of perjury, that I have taken reasonable care to redact each Social Security Number in this document, unless required by law.
Signature: *Rodney J. Kelly*
Printed Name: RODNEY J. KELLY

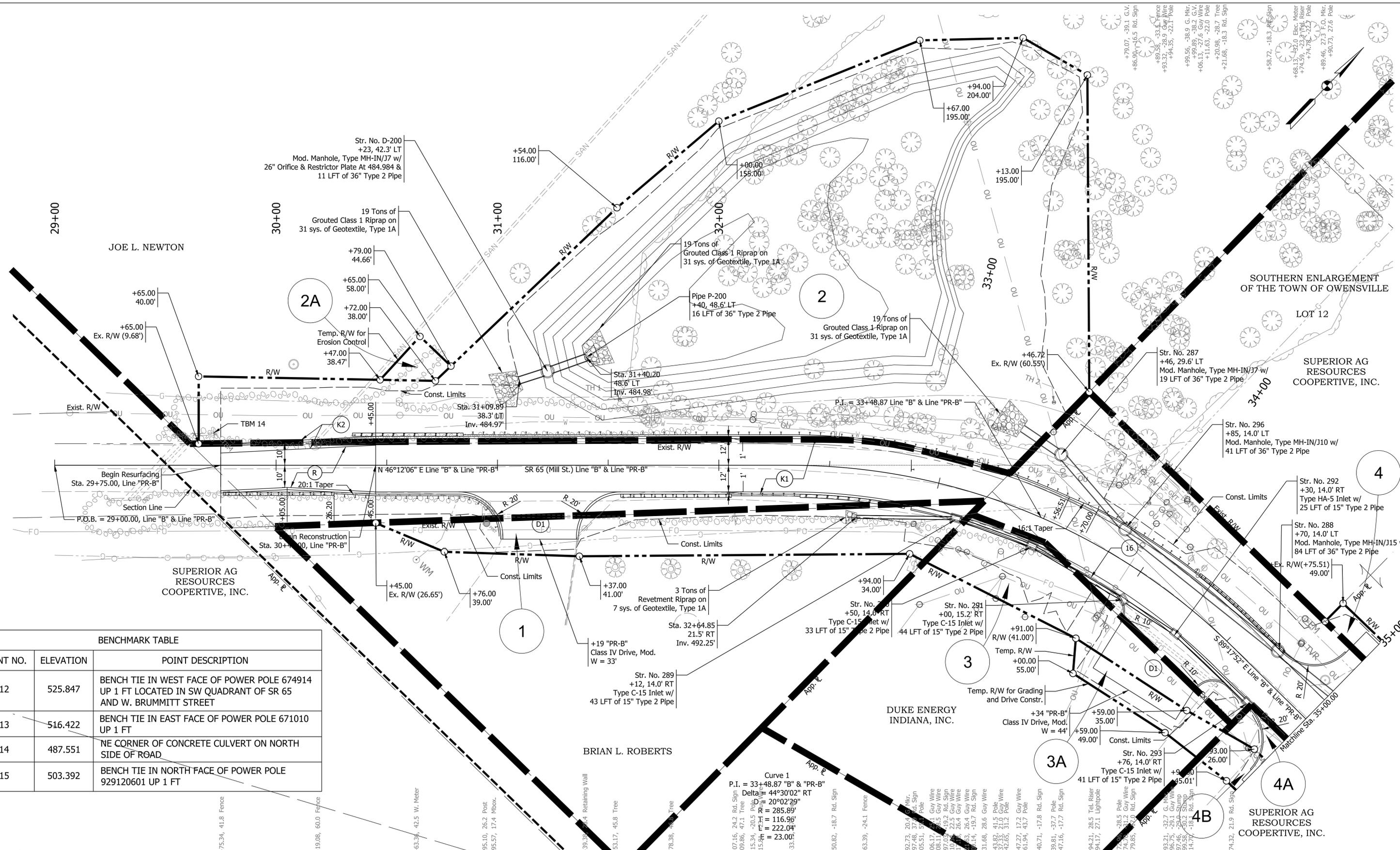
FIELD SURVEYOR STATEMENT
THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF TITLE 865 I.A.C. 1-12-20 THROUGH 1-12-25 REGARDING ROUTE SURVEYS, EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT A PART OF THIS SURVEY.



PREPARED BY: *Rodney J. Kelly* 03/29/2022
RODNEY J. KELLY
LEAD CREW: BLC CHECKED: CLC
DRAWN: CDM

INDIANA DEPARTMENT OF TRANSPORTATION
LOCATION CONTROL ROUTE SURVEY
SURVEYOR'S REPORT AND CERTIFICATION

SCALE	BRIDGE FILE
SEE ABOVE	---
COUNTY	DESIGNATION
GIBSON	2001922
SURVEY BOOK	SHEETS
---	9 20
CONTRACT	ETICA PROJECT NUMBER
---	210045.03



BENCHMARK TABLE		
POINT NO.	ELEVATION	POINT DESCRIPTION
12	525.847	BENCH TIE IN WEST FACE OF POWER POLE 674914 UP 1 FT LOCATED IN SW QUADRANT OF SR 65 AND W. BRUMMITT STREET
13	516.422	BENCH TIE IN EAST FACE OF POWER POLE 671010 UP 1 FT
14	487.551	NE CORNER OF CONCRETE CULVERT ON NORTH SIDE OF ROAD
15	503.392	BENCH TIE IN NORTH FACE OF POWER POLE 929120601 UP 1 FT

LEGEND			
(F)	Sidewalk, Concrete	(CR)	Curb Ramp, Concrete
(13)	Curb, Concrete	(26)	Sodding, Nursery
(16)	Curb and Gutter, Concrete	(K1)	Full Depth Reconstruction
		(K2)	HMA for Shoulder Widening
(R)	Mill and Overlay	(C1)	PCCP for Approaches, 6"
(C1)	PCCP for Approaches, 6"	(C2)	PCCP for Approaches, 9"
(D1)	HMA for Approaches, Type B	(DND)	Do Not Disturb
(22)	Curb Center, Concrete D		
(AC)	Adjust Casting To Grade		

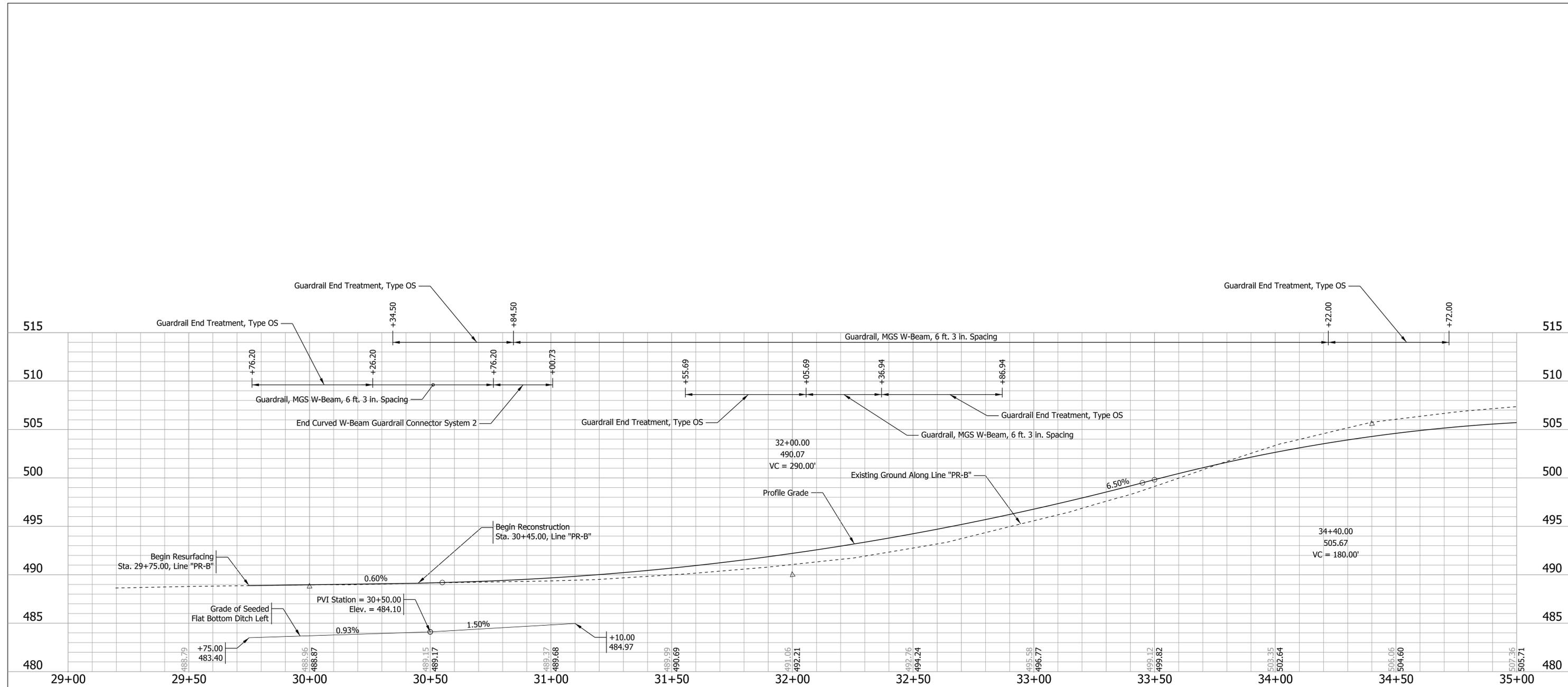
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-B"
STA. 30+45.00 TO STA. 35+00.00

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 5'	2001922
SURVEY BOOK	SHEETS
	10 of 20
CONTRACT	PROJECT
R-43253	2001922

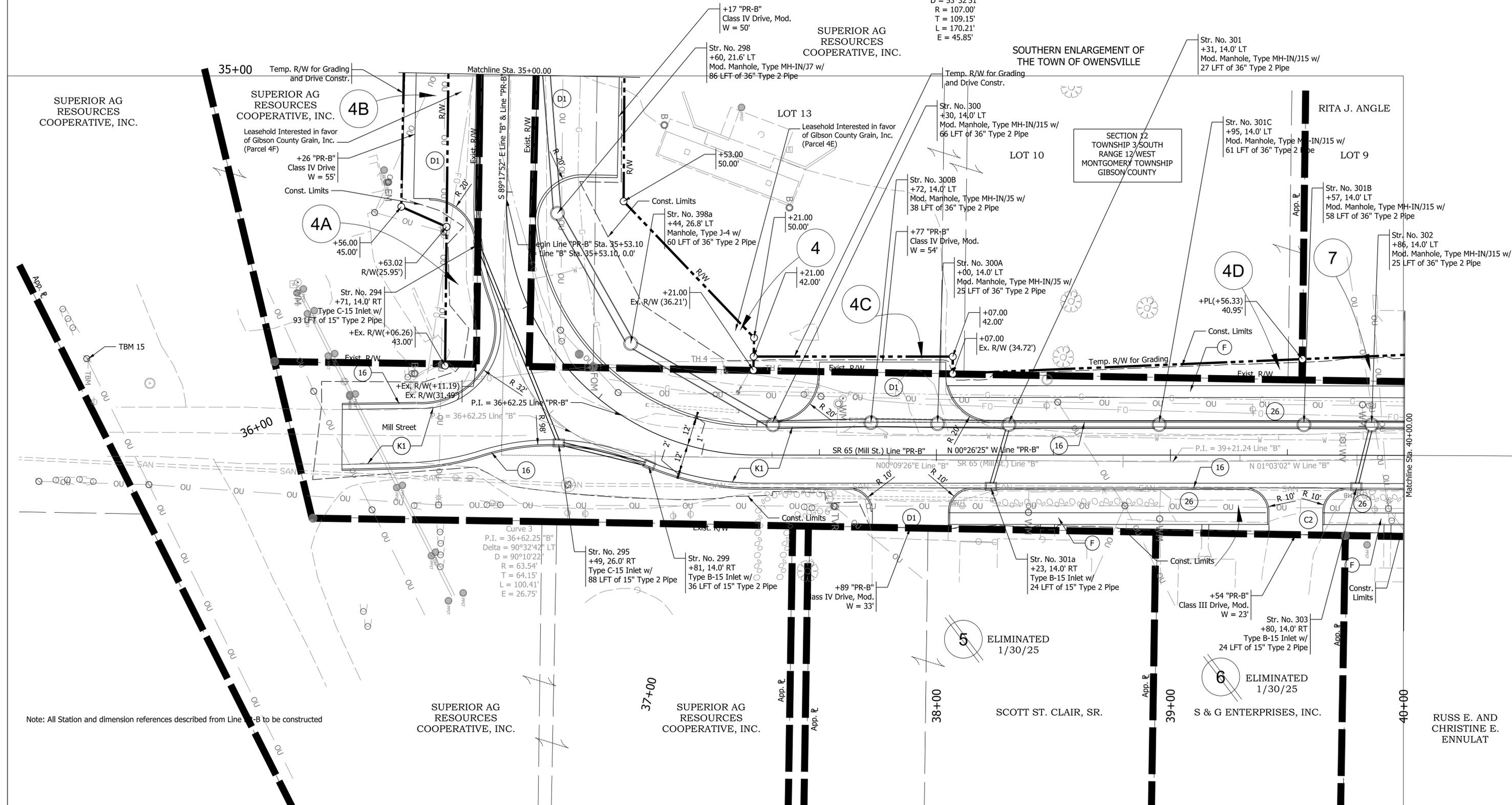


Note: Guardrail North of SR 65 plotted higher than Guardrail South of SR 65.

Plot: 2/10/2025 1:29 PM

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NOT FOR CONSTRUCTION	RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
	DESIGNED: HEW _____	DRAWN: HEW _____	PROFILE SHEET LINE "PR-B" STA. 30+45.00 TO STA. 35+00.00		VERTICAL SCALE 1" = 5'	DESIGNATION 2001922
	CHECKED: BEA _____	CHECKED: BEA _____			SURVEY BOOK	SHEETS 11 of 20
			CONTRACT R-43253			PROJECT 2001922



Note: All Station and dimension references described from Line PR-B to be constructed

All R/W described from Line PR-B", except as described.

LEGEND			
(F)	Sidewalk, Concrete	(CR)	Curb Ramp, Concrete
(13)	Curb, Concrete	(R)	Mill and Overlay
(16)	Curb and Gutter, Concrete	(C1)	PCCP for Approaches, 6"
		(C2)	PCCP for Approaches, 9"
		(K1)	Full Depth Reconstruction
		(K2)	HMA for Shoulder Widening
		(D1)	HMA for Approaches, Type B
		(22)	Curb Center, Concrete D
		(AC)	Adjust Casting To Grade
		(DND)	Do Not Disturb

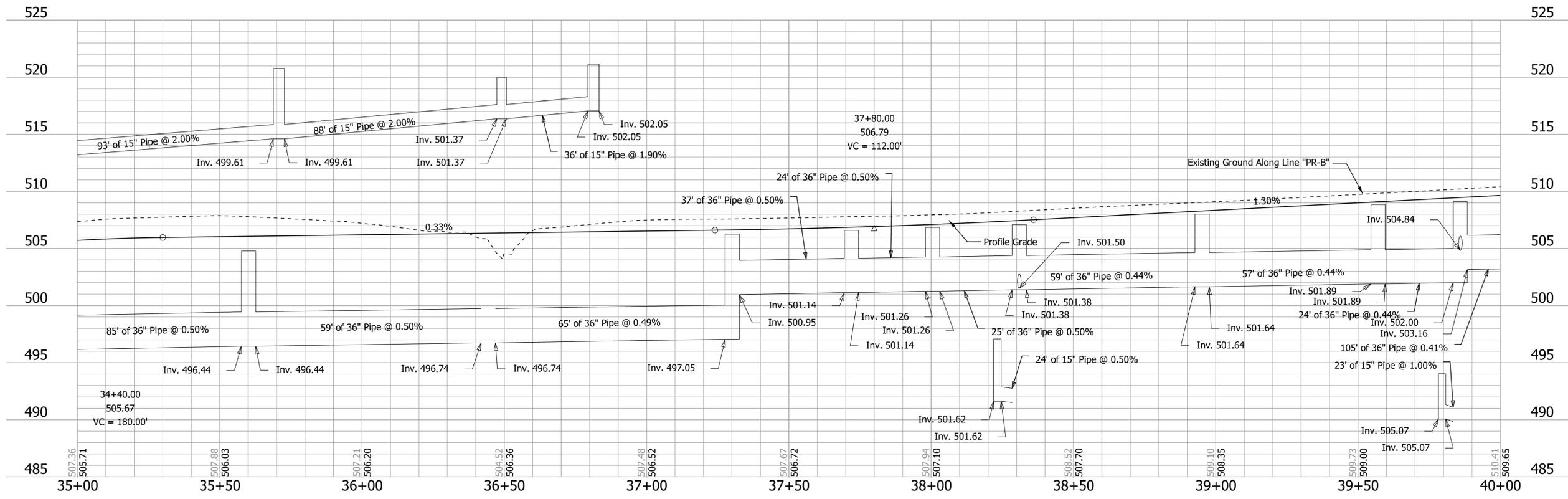
NOT FOR CONSTRUCTION	RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
	DESIGNED: SAC _____	DRAWN: SAC _____	
	CHECKED: BEA _____	CHECKED: BEA _____	

INDIANA DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-B"
STA. 35+00.00 TO STA. 40+00.00

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 2001922
SURVEY BOOK	SHEETS 12 of 20
CONTRACT R-43253	PROJECT 2001922

Plot: 2/10/2025 1:30 PM
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Plot: 2/10/2025 1:30 PM

IP_PWP:d0443604\RW Sht RD Profile_02.dgn

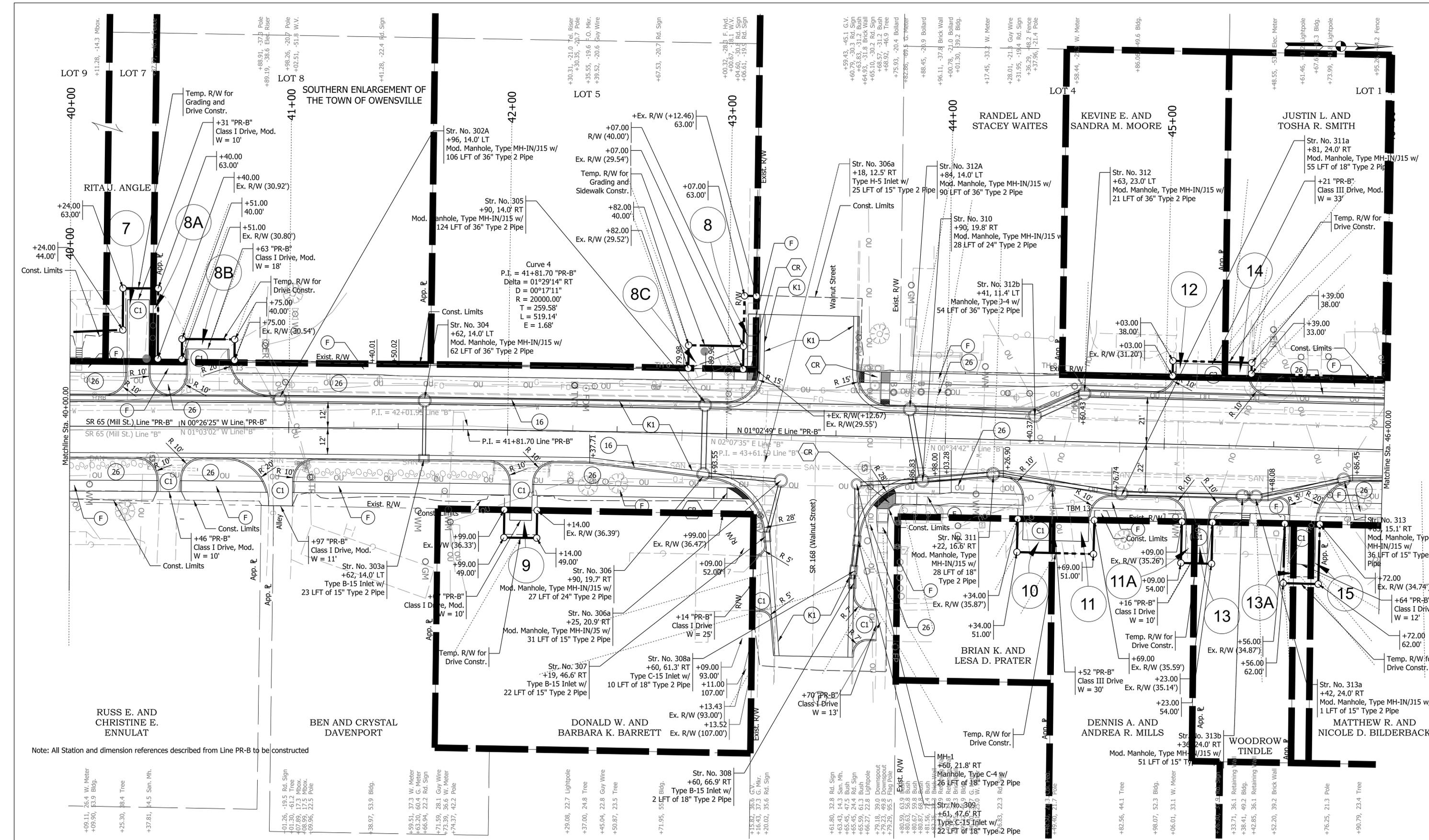
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: SAC _____	DRAWN: SAC _____	
CHECKED: BEA _____	CHECKED: BEA _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

**PROFILE SHEET
LINE "PR-B"
STA. 35+00.00 TO STA. 40+00.00**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 2001922
SURVEY BOOK	SHEETS 13 of 20
CONTRACT R-43253	PROJECT 2001922



Note: All Station and dimension references described from Line PR-B to be constructed

Plot: 2/10/2025 1:30 PM

LEGEND	
(F)	Sidewalk, Concrete
(13)	Curb, Concrete
(16)	Curb and Gutter, Concrete
(CR)	Curb Ramp, Concrete
(26)	Sodding, Nursery
(K1)	Full Depth Reconstruction
(K2)	HMA for Shoulder Widening
(R)	Mill and Overlay
(C1)	PCCP for Approaches, 6"
(C2)	PCCP for Approaches, 9"
(D1)	HMA for Approaches, Type B
(22)	Curb Center, Concrete D
(AC)	Adjust Casting To Grade
(DND)	Do Not Disturb

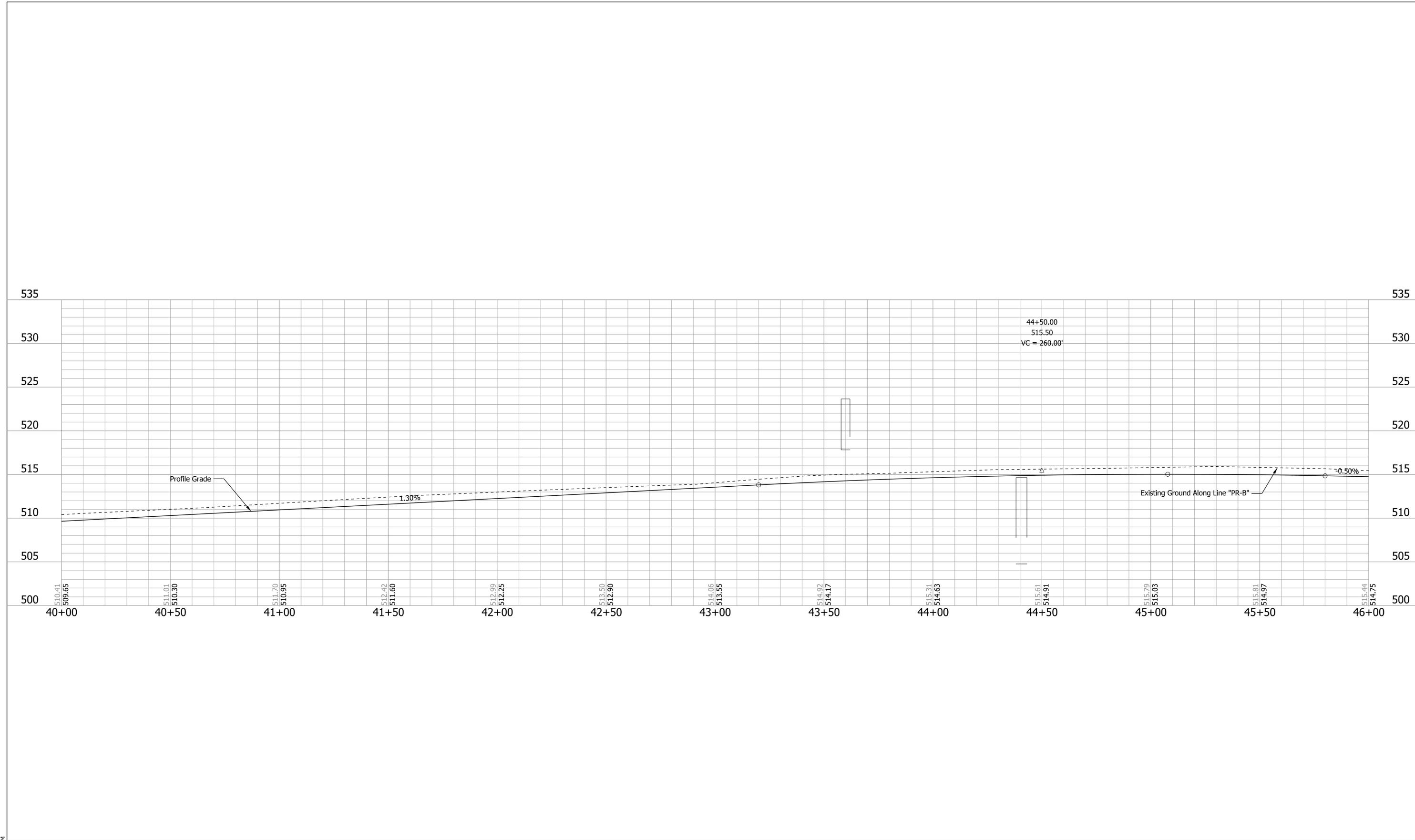
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-B"
STA. 40+00.00 TO STA. 46+00.00

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 5'	2001922
SURVEY BOOK	SHEETS
	14 of 20
CONTRACT	PROJECT
R-43253	2001922



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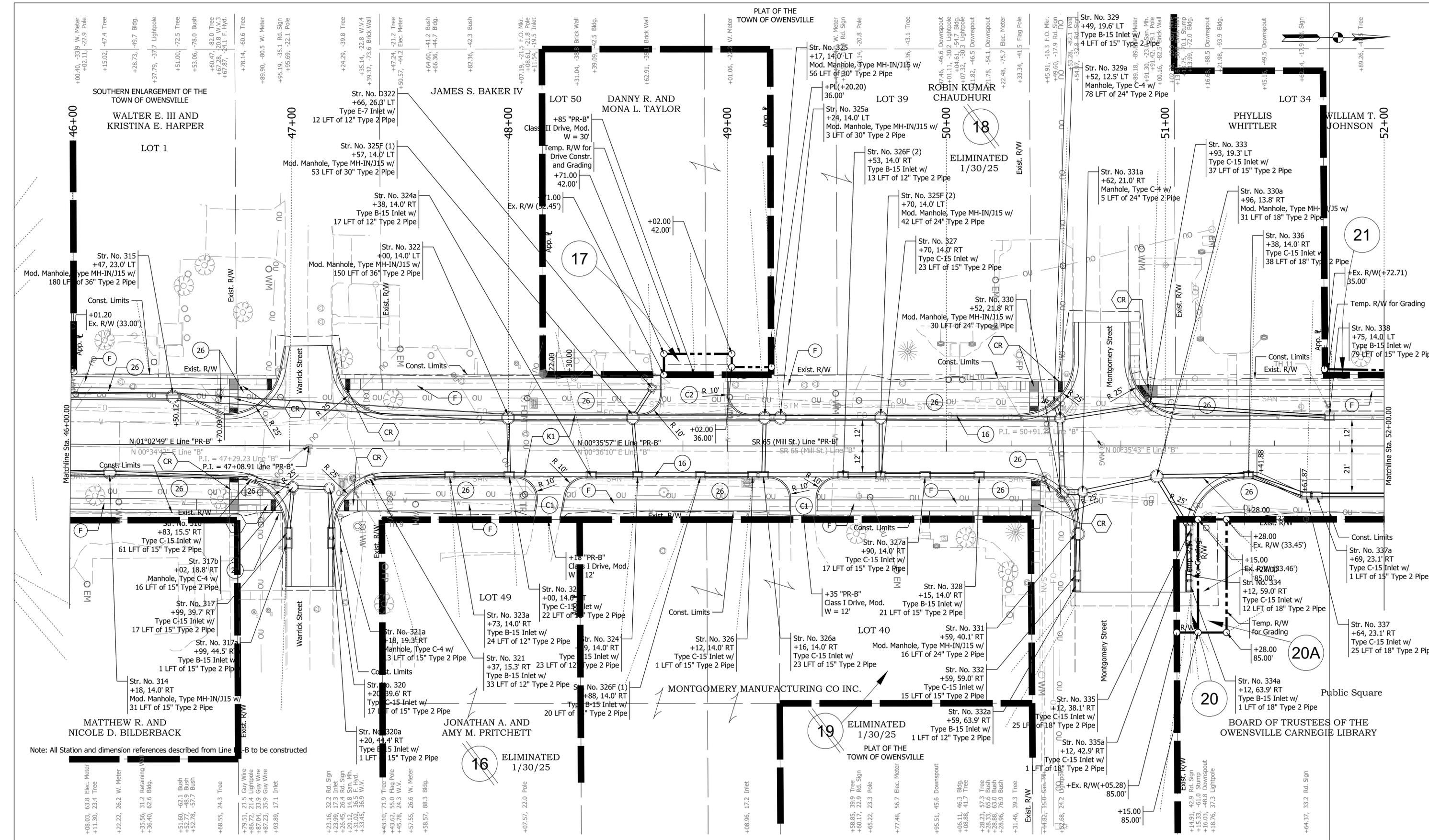
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: HEW _____	DRAWN: HEW _____	
CHECKED: BEA _____	CHECKED: BEA _____	

INDIANA DEPARTMENT OF TRANSPORTATION

PROFILE SHEET
LINE "PR-B"
STA. 40+00.00 TO STA. 46+00.00

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 2001922
SURVEY BOOK	SHEETS 15 of 20
CONTRACT R-43253	PROJECT 2001922



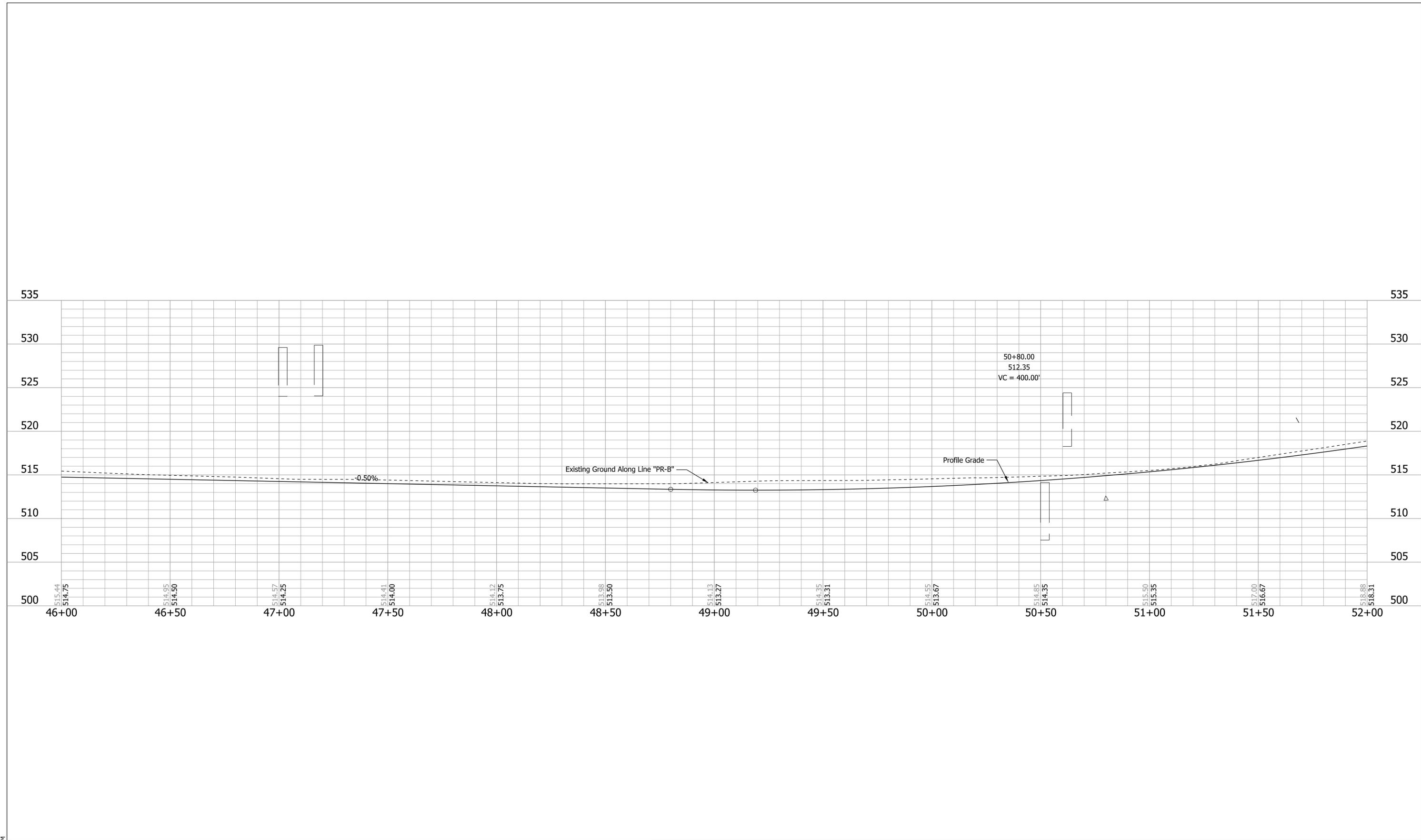
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LEGEND	
(F)	Sidewalk, Concrete
(13)	Curb, Concrete
(16)	Curb and Gutter, Concrete
(CR)	Curb Ramp, Concrete
(26)	Sodding, Nursery
(K1)	Full Depth Reconstruction
(K2)	HMA for Shoulder Widening
(R)	Mill and Overlay
(C1)	PCCP for Approaches, 6"
(C2)	PCCP for Approaches, 9"
(D1)	HMA for Approaches, Type B
(22)	Curb Center, Concrete D
(AC)	Adjust Casting To Grade
(DND)	Do Not Disturb

NOT FOR CONSTRUCTION	RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
	DESIGNED: HEW	DRAWN: HEW	
	CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN SHEET LINE "PR-B" STA. 46+00.00 TO STA. 52+00.00	

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 5'	2001922
SURVEY BOOK	SHEETS
	16 of 20
CONTRACT	PROJECT
R-43253	2001922



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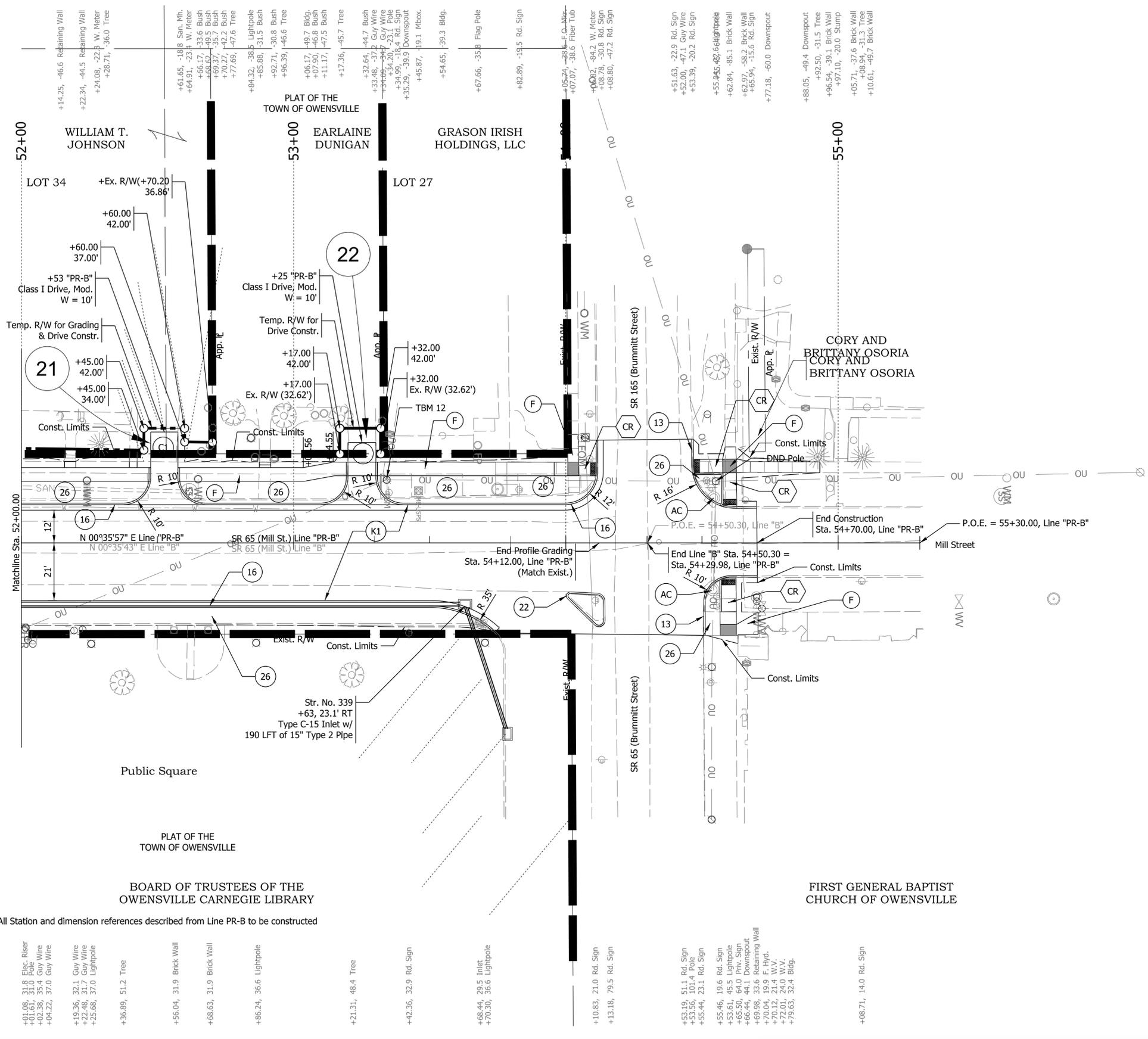
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RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: HEW _____	DRAWN: HEW _____	
CHECKED: BEA _____	CHECKED: BEA _____	

INDIANA DEPARTMENT OF TRANSPORTATION

PROFILE SHEET
LINE "PR-B"
STA. 46+00.00 TO STA. 52+00.00

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 2001922
SURVEY BOOK	SHEETS 17 of 20
CONTRACT R-43253	PROJECT 2001922



Note: All Station and dimension references described from Line PR-B to be constructed

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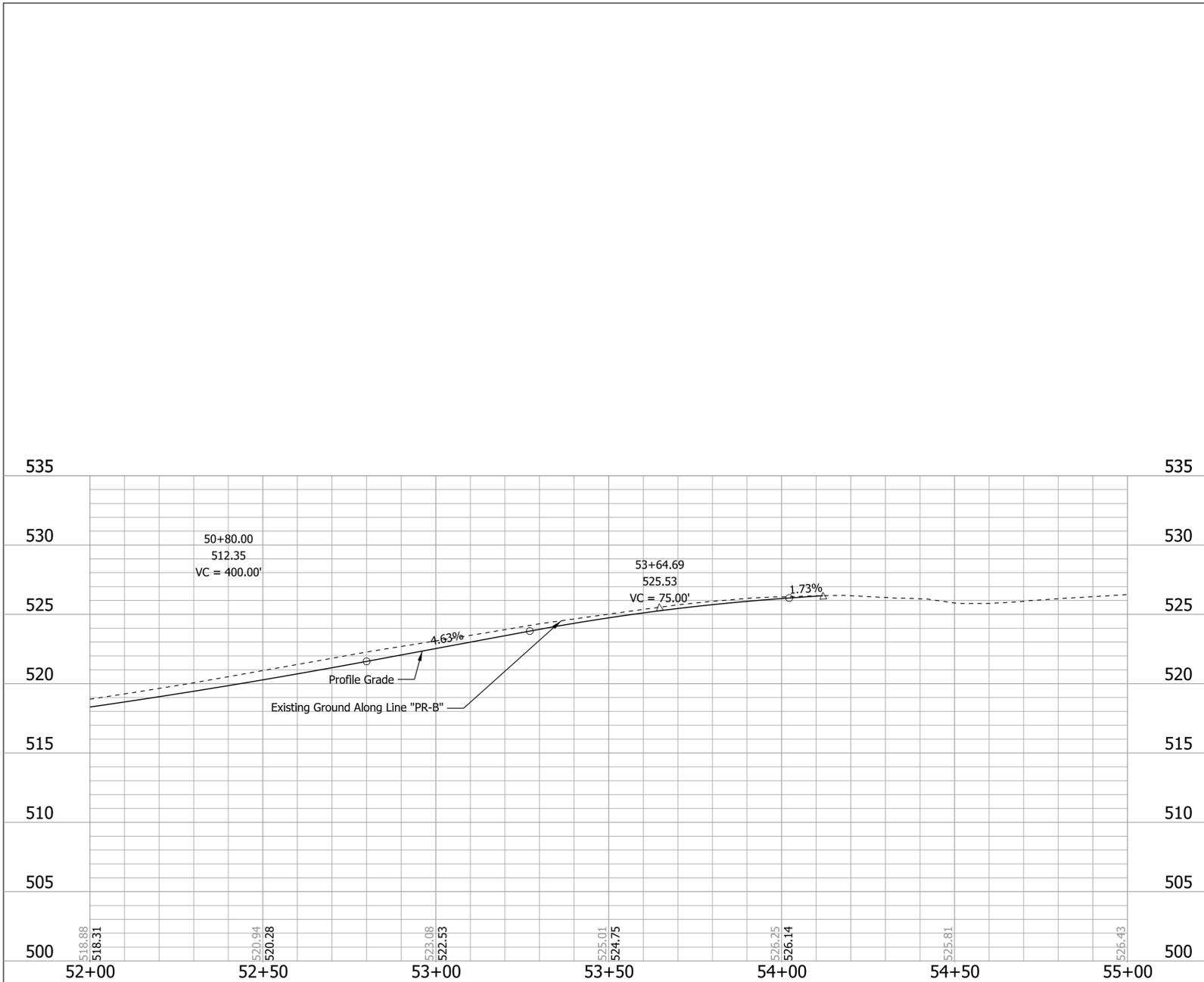
LEGEND	
(F) Sidewalk, Concrete	(CR) Curb Ramp, Concrete
(13) Curb, Concrete	(26) Sodding, Nursery
(16) Curb and Gutter, Concrete	(K1) Full Depth Reconstruction
	(K2) HMA for Shoulder Widening
(R) Mill and Overlay	(C1) PCCP for Approaches, 6"
(C2) PCCP for Approaches, 9"	(D1) HMA for Approaches, Type B
(22) Curb Center, Concrete D	(AC) Adjust Casting To Grade
(DND) Do Not Disturb	

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: HEW	DRAWN: HEW	
CHECKED: BEA	CHECKED: BEA	

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN SHEET LINE "PR-B" STA. 52+00.00 TO STA. 55+30.00	

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION
1" = 5'	2001922
SURVEY BOOK	SHEETS
	18 of 20
CONTRACT	PROJECT
R-43253	2001922



Plot: 2/10/2025 1:32 PM

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NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: HEW _____	DRAWN: HEW _____	
CHECKED: BEA _____	CHECKED: BEA _____	

INDIANA DEPARTMENT OF TRANSPORTATION

PROFILE SHEET
LINE "PR-B"
STA. 52+00.00 TO STA. 55+30.00

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION 2001922
SURVEY BOOK	SHEETS 19 of 20
CONTRACT R-43253	PROJECT 2001922

PAVEMENT QUANTITIES AND APPROACH TABLE

LOCATION	BEGIN STATION	END STATION	DESCRIPTION	Existing Material Type	WIDTH 1	WIDTH 2	LENGTH	RADIUS	AREA	HMA FOR APPROACHES, TYPE B SURFACE												
										HMA SURFACE 165 LBS/SYS	HMA INTERMEDIATE 275 LBS/SYS	HMA BASE 605 LBS/SYS	ASPHALT FOR TACK COAT	JOINT ADHESIVE, SURFACE	JOINT ADHESIVE, INTERMEDIATE	LIQUID ASPHALT SEALANT	PCCP FOR APPROACHES, 6 IN.	PCCP FOR APPROACHES, 9 IN.	DENSE GRADED SUBBASE 6 IN.	GEOGRID, TYPE IB	SUBGRADE TREATMENT, TYPE II	
										TONS	TONS	TONS	TONS	LFT	LFT	LFT	SYS	SYS	CYD	SYS	SYS	
Line "PR-B"																						
RT	36+00	36+79	Mill Street	HMA	141	24	93	30/100	364	30.0	50.0	110.1	0.25	165	165	329						
RT	37+49	38+16	Class I Drive	Gravel	54	32	21	25/10	84								84		14		78.1	
LT	37+49	38+29	Class III Drive	Gravel	70	40	25	15/25	130									130	22	130	122.4	
RT	39+22	39+74	Class I Drive	HMA	52	22	20	20/10	75								75		13		60.6	
LT	40+16	40+45	Class I Drive	Gravel	30	10	39	10/10	57								57		9		59.4	
RT	40+20	40+61	Class I Drive	Gravel	40	10	20	20/10	46								46		8		37.3	
LT	40+45	40+87	Class I Drive	HMA	43	13	24	10/20	59								59		10		49.7	
RT	40+72	41+12	Class I Drive	HMA	40	10	20	20/10	46								46		8		37.3	
LT	43+02	43+72	Walnut Street	HMA	70	42	72	13/13	333	27.4	45.7	100.6	0.23	112	112	224						
RT	42+53	44+18	Walnut Street	HMA	167	32	93	28/28	464	38.3	63.8	140.3	0.32	199	199	398						
RT	44+34	44+51	Class I Drive	Gravel	19	13	15	15	31								31		5		27.1	
RT	44+51	44+75	Class I Drive	Concrete	24	14	15	10	32								32		5		20.9	
LT	44+95	45+47	Class I Drive	Gravel	52	32	14	10/10	67								67		11		48.9	
RT	45+01	45+31	Class I Drive	Gravel	30	10	27	10/10	43								43		7		41.2	
LT	46+86	47+31	Warrick Street	HMA	45	17	36	10/10	88	7.2	12.0	26.5	0.06	62	62	124						
RT	46+84	47+35	Warrick Street	HMA	51	17	32	15/15	74	6.1	10.2	22.4	0.05	68	68	136						
RT	48+04	48+35	Class I Drive	Gravel	31	10	35	10/10	53								53		9		54.5	
LT	48+63	49+07	Class I Drive	Gravel	44	24	26	10/10	86								86		14		77.3	
RT	49+20	49+50	Class I Drive	HMA	30	10	19	10/10	35								35		6		29.8	
LT	50+48	50+93	Montgomery Street	HMA	45	21	27	10/10	70	5.8	9.6	21.1	0.05	66	66	132						
RT	50+47	51+27	Montgomery Street	HMA	79	49	62	12/15	343	28.3	47.1	103.7	0.23	128	128	256						
LT	52+38	52+68	Class I Drive	Concrete	30	10	26	10/10	43								43		7		40.9	
LT	54+00	54+58	SR 165	HMA	59	31	56	12/12	199	16.4	27.4	60.2	0.14	90	90	180						
RT	53+73	54+61	SR 165	HMA	78	73	77	15/10	589	48.6	81.0	178.1	0.40	151	151	302						
Mainline	30+45	55+30		HMA					7968	657.4	90.4	2410.3	5.42	7455	7455	7455					9713	
Paved Shoulder	30+45	37+84							1287	106.2	14.6	389.4	0.88								1565.4	
TOTAL										12665	972	452	3563	9.0	8496	8496	9536		130	148	130	12064

* Match Existing Side Road and Driveway Dimensions. Contractor to Field Verify Existing Dimensions Prior to Construction. See Details in Typical Sections.

** Contractor to Field Verify Existing Material Type Prior to Construction.

Plot: 2/10/2025 1:32 PM

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____
 DESIGN ENGINEER _____ DATE _____
 DESIGNED: _____ DRAWN: _____
 CHECKED: _____ CHECKED: _____

**INDIANA
DEPARTMENT OF TRANSPORTATION**

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION
	2001922
SURVEY BOOK	SHEETS
	20 of 20
CONTRACT	PROJECT
R-43253	2001922

APPENDIX C

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (317) 694-8283

Eric Holcomb, Governor
Michael Smith, Commissioner

January 30, 2024

Sample Early Coordination Letter

Re: Early Coordination Letter, Des. No.: 2001922, HMA Overlay and Minor Structural, SR 65, 0.26 Mile South of SR 168 (S. Limits Owensville) to SR 165 Gibson County, Indiana
KEG No. 21-1055.04

Dear Interested Party,

The Indiana Department of Transportation (INDOT), with federal funding, intends to proceed with a project involving the aforementioned roadway in Gibson County. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

This project is located on SR 65, approximately 0.26 miles south of SR 168 (S. Limits Owensville) to SR 165 in Gibson County. This section of SR 65 is a two-lane *Rural & Urban-Major Collector*. The existing SR 65 cross section consists of two 11' travel lanes bordered by 1' usable and paved shoulders. The draft need is due to the deterioration of the existing pavement of SR 65 along with need for corrective drainage measures. The draft purpose is to upgrade the deteriorating asphalt pavement and improve drainage conditions along SR 65. The approximate existing right-of-way (ROW) is 35 feet left and 30 feet right of the centerline of SR 65.

The proposed project is anticipated to full depth pavement replacement on SR 65 from approximately 400 feet west of Mill Street to the intersection of SR 165. The remaining south portion of the project limits will be milling and repaving 1.5 inches of surface. The full depth pavement replacement will include new curb and gutter, sidewalks, curb ramps, and an enclosed drainage system; a 10-foot parking lane will be added on both sides of SR 65 from Warrick Street to SR 168, with a curb and gutter separating the parking lane from the sidewalk; adjusting the curve design speed to 20 miles per hour (MPH) and roadway geometrics at Mill Street and SR 65; adjusting the design at SR 168 and SR 65 to include a 30-foot radii with 10:1 tapers and a two-foot offset; the storm sewer on SR 65 within the project limits will be redesigned to be enclosed and included replacement of the trunkline and the addition of inlets; and a new detention pond is also proposed near the project's southern terminus on the west side of SR 65. The project will require the acquisition of 1.81 acre of permanent ROW and 0.24 acre of temporary ROW. The project will be approximately 0.47 miles in length. The proposed method of traffic maintenance is anticipated to include temporary road closures with an official detour, with flagging operations utilized during milling work. Approximately 1.1 acres of trees will be cleared as part of this project. The project is anticipated to begin construction in Summer 2025.

Land use in the vicinity of the project is predominantly residential with intermittent tracts of forested acreage. Kaskaskia Engineering Group, LLC will perform waters and wetlands determinations to identify water resources that may be present. The project is anticipated to qualify for the Rangewide Programmatic Agreement for the Indiana bat and northern long-eared bat by

completing the Information for Planning and Consultation (IPaC). Coordination will occur with INDOT Cultural Resources Office (CRO) to evaluate the project area for archaeological and historic resources and for Section 106 compliance. The results of this investigation will be forwarded to the State Historic Preservation Officer (SHPO) for review and concurrence as appropriate.

Please provide your response within thirty (30) calendar days from the date of this letter. However, should you find that an extension to the response time is necessary; a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact me, at 630-332-9157 or ckelly@kaskaskiaeng.com, or Dakota Risse, INDOT Project Manager at 812-362-7778 or darisse@indot.in.gov. Thank you in advance for your input.

Sincerely,



Chad Kelly
Environmental Scientist
Kaskaskia Engineering Group, LLC

Attachment -

- Early Coordination Letter Recipient List
- Maps (Location, Aerial, Topographic)
- Photo Log

Attachments omitted
to avoid duplication

cc: Eric Arthur, WSP
Dakota Risse, INDOT Project Manager



Organization and Project Information

Organization Name: Kaskaskia Engineering Group, LLC

First Name: Chad

Last Name: Kelly

Phone: (630) 332-9157

Email: ckelly@kaskaskiaeng.com

Address Line 1: 477 South Third Street

Address Line 2: Suite 280

City: Geneva

State: IL

Zip: 60134

Customer Id: 21-1055.04

Destination Id: 2001922

Project Title: HMA Overlay and Minor Structural

Project Description: The proposed project is anticipated to full depth pavement replacement on SR 65 from approximately 400 feet west of Mill Street to the intersection of SR 165.

Environmental Assessment Report

Geological Hazards:

1. Moderate liquefaction potential

Mineral Resources:

Active or abandoned mineral resources extraction sites:

1. Petroleum Exploration Wells

Disclaimer:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: (812) 855-7428





Farm
Production
and
Conservation

Natural
Resources
Conservation
Service

Indiana State Office
6013 Lakeside Boulevard
Indianapolis, Indiana 46278
317-295-5800

February 10, 2024

Chad Kelly
208 E Main Street #100
Belleville, Illinois 62220

Dear Mr. Kelly:

The proposed HMA Overlay and Minor Structural on SR 65, Gibson County, (Des. No. 2001922) as referred to in your letter received January 30, 2024, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov.

Sincerely,

JOHN ALLEN

Digitally signed by JOHN ALLEN
Date: 2024.02.12 09:41:45 -05'00'

JOHN ALLEN
State Soil Scientist



INDIANA DEPARTMENT OF ENVIRONMENTAL MANAGEMENT

We Protect Hoosiers and Our Environment.

100 N. Senate Avenue • Indianapolis, IN 46204

(800) 451-6027 • (317) 232-8603 • www.idem.IN.gov

Eric J. Holcomb
Governor

Brian C. Rockensuess
Commissioner

February 1, 2024

Kaskaskia Engineering Group, LLC
Attention: Chad Kelly
477 South 3rd Street, Suite 280
Geneva, IL 60134

Dear Chad Kelly:

Re: Wellhead Protection Area
Proximity Determination
Des No 2002051
HMA Overlay and Minor Structural,
SR 65, 0.26 Mile South of
SR 168 (S. Limits Owensville) to SR 165
Gibson County, Indiana

Upon review of the above referenced project site, it has been determined that the proposed project area **is not located within** a Wellhead Protection Area. If the contact information is needed for the WHPA, please contact the reference located at the bottom of the letter for the appropriate information. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases, we use a 3,000-foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/idem/cleanwater/2456.htm> and scroll to the bottom of the page.

The project area **is not located within** a Source Water Assessment Area for a PWSS's surface water intake. The Source Water Assessment Area relates to the surface water drainage area that water could potentially flow and influence water quality for a PWSS's source of drinking water.

In the future, **please consider using this self-service tool** if it suits your needs. The Drinking Water Branch has a self-service tool which allows one to determine wellhead proximity without submitting the application form. Go to <https://www.in.gov/idem/cleanwater/pages/wellhead/> and use the instructions at the bottom of the page.



Please Reduce, Reuse, Recycle

Chad Kelly
Page 2

If you have any additional questions, please feel free to contact me at the address above or at 317-233-9158 and aturnbow@idem.in.gov.

Sincerely,

A handwritten signature in black ink that reads "Alisha Turnbow". The signature is written in a cursive, flowing style.

Alisha Turnbow,
Environmental Manager
Ground Water Section
Drinking Water Branch
Office of Water Quality

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR#: ER-26259

Request Received: January 30, 2024

Requestor:

Chad Kelly
Kaskaskia Engineering Group, LLC
477 South 3rd Street, Suite 280
Geneva, IL 60134

Project:

SR 65 HMA overlay and other improvements, from 0.26 miles south of SR 168 (Owensville town limits) to SR 165, Owensville; KEG #21-1055.04, Des #2001922

County/Site Info: Gibson County

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment:

Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database:

The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish and Wildlife Comments:

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Riparian Habitat

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation Guidelines (and plant lists) can be found online at: <https://www.in.gov/nrc/files/IB-17.pdf>.

Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ratio based on area of impact. Impacts under 0.10 acre in a rural area typically do not require mitigation or additional plantings beyond seeding and stabilizing disturbed areas, though there are exceptions for high quality habitat sites. Impacts under 0.10 acre in an urban area should be mitigated by replacing trees that are 10" diameter-at-breast height (dbh) or greater by planting five trees, 1" to 2" in dbh, for each tree which is removed that is 10" dbh or greater.

Seeding and stabilizing disturbed areas is required regardless of the impact amount and location. If floodway impacts to forested wetland and non-wetland habitat areas combine to be 0.10 acres or more, mitigation should be done and coordinated with the biologist, as needed.

The mitigation site should be located in the floodway, downstream of the one (1) square mile drainage area of that stream (or another stream within the 8-digit HUC, preferably as close to the impact site as possible) and adjacent to existing forested riparian habitat.

B) Pavement Rehabilitation

Pavement rehabilitation projects typically do not have a significant impact on fish, wildlife, and botanical resources if best management practices (BMPs) are in place to limit the migration of polycyclic aromatic hydrocarbons (PAHs) into local waterways. PAHs are a byproduct of asphalt and coal tar-based sealants and negatively impact aquatic systems. The use of sealants that are free of petroleum and coal tar-based products is encouraged whenever possible. Contaminated road runoff can significantly impact the aquatic environment through increased turbidity and release of sediment into the stream which can be harmful to fish and other aquatic organisms, their eggs, and their food supply. Where possible, road runoff should be directed to riprap turnouts and sediment filtration prior to entering a stream to reduce impacts to aquatic species. We recommend the use of pollutant trapping technology such as storm drain inserts to reduce the runoff of roadside pollutants where appropriate.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Southern Indiana as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only. A native herbaceous seed mixture must include at least 5 species of grasses and sedges and 5 species of wildflowers.
2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not cut any trees suitable for Indiana Bat or Northern Long-eared Bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
5. If erosion control blankets are used, they shall be heavy-duty, biodegradable, and net free or use loose-woven/Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Our agency appreciates this opportunity to be of service. Please contact me at RVanVoorhis@dnr.IN.gov or (317) 232-8163 if we can be of further assistance.

Rachel Van Voorhis
 Rachel Van Voorhis
 Environmental Coordinator
 Division of Fish and Wildlife

Date: February 29, 2024

Chad Kelly

From: Falls, Ryan G <RFalls@indot.IN.gov>
Sent: Tuesday, January 30, 2024 12:41 PM
To: Chad Kelly
Cc: Virginia Flynn; Arthur, Eric
Subject: RE: Early Coordination - INDOT Project, Des. No. 2001922, HMA Overlay and Minor Structural on SR 65, Gibson County, Indiana [21-1055.04]

Categories: Saved to SharePoint by Managed Filing,
<https://kaskaskiaeng.sharepoint.com/sites/21-1055.04/Emails>

Chad Kelly,

At this time, our office has no comment on this project. Thank you for the opportunity to respond to early coordination.

Ryan Falls

Senior Environmental Manager Supervisor

Indiana Department of Transportation
 Vincennes District

Cell: 812-582-1387

Email: rfalls@indot.in.gov

[Find us on social media!](#)



From: Chad Kelly <ckelly@kaskaskiaeng.com>
Sent: Tuesday, January 30, 2024 12:39 PM
To: Chad Kelly <ckelly@kaskaskiaeng.com>
Cc: Virginia Flynn <VFlynn@kaskaskiaeng.com>; Arthur, Eric <eric.arthur@wsp.com>
Subject: Early Coordination - INDOT Project, Des. No. 2001922, HMA Overlay and Minor Structural on SR 65, Gibson County, Indiana [21-1055.04]

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Dear Interested Party,

Please find the attached early coordination letter and supporting attachments regarding the above referenced INDOT project.

Thank you,



Chad Kelly (he/him)



United States Department of the Interior



FISH AND WILDLIFE SERVICE
 Indiana Ecological Services Field Office
 620 South Walker Street
 Bloomington, IN 47403-2121
 Phone: (812) 334-4261 Fax: (812) 334-4273

In Reply Refer To:

01/12/2026 14:32:19 UTC

Project Code: 2024-0045619

Project Name: SR 65, HMA Pavement Replacement, DES 2001922 and 2400688

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

To Whom It May Concern:

The purpose of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 et seq.), is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 et seq.), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

The enclosed species list identifies threatened, endangered, proposed, and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Act.

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. You can complete this verification formally or informally or request an updated list by visiting the IPaC website at regular intervals during project planning and implementation.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at [Midwest Section 7 Technical Assistance | U.S. Fish & Wildlife Service](#). This website contains step-by-step instructions to help you determine if your project will have an adverse effect on listed species and to lead you through the Section 7 process.?

We appreciate your concern for threatened and endangered species. Federal agencies should include conservation of threatened and endangered species into their project planning to further the purposes of the Act. Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Other Trust Resources and Activities?

Bald and Golden Eagles?- Although the bald eagle has been removed from the endangered species list, this species and the golden eagle are protected by the Bald and Golden Eagle Act and the Migratory Bird Treaty Act. It is the responsibility of the project proponent to survey the area for any migratory bird nests. If there is an eagle nest on-site while work is on-going, eagles may be disturbed. We recommend avoiding and minimizing disturbance to eagles whenever practicable. If you cannot avoid eagle disturbance, you may seek a [permit](#). A [nest take permit](#) is always required for removal, relocation, or obstruction of an eagle nest. For communication and wind energy projects, please refer to additional guidelines below.?

Migratory Birds?- The Migratory Bird Treaty Act (MBTA) prohibits the taking, killing, possession, transportation, and importation of migratory birds, their eggs, parts, and nests, except when specifically authorized by the Service. The Service has the responsibility under the MBTA to proactively prevent the mortality of migratory birds whenever possible and we encourage implementation of [recommendations that minimize potential impacts to migratory birds](#). Such measures include clearing forested habitat outside the nesting season (generally March 1 to August 31) or conducting nest surveys prior to clearing to avoid injury to eggs or nestlings.?

Communication Towers?- Construction of new communications towers (including radio, television, cellular, and microwave) creates a potentially significant impact on migratory birds, especially some 350 species of night-migrating birds. However, the Service has developed [voluntary guidelines for minimizing impacts](#).?

Transmission Lines?- Migratory birds, especially large species with long wingspans, heavy bodies, and poor maneuverability can also collide with power lines. In addition, mortality can occur when birds, particularly hawks, eagles, kites, falcons, and owls, attempt to perch on uninsulated or unguarded power poles. To minimize these risks, please refer to [guidelines](#) developed by the Avian Power Line Interaction Committee and the Service. Implementation of these measures is especially important along sections of lines adjacent to wetlands or other areas that support large numbers of raptors and migratory birds.?

Wind Energy?- To minimize impacts to migratory birds and bats, wind energy projects should follow the Service's [Wind Energy Guidelines](#). In addition, please refer to the Service's [Eagle Conservation Plan Guidance](#), which provides guidance for conserving bald and golden eagles in the course of siting, constructing, and operating wind energy facilities.

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

PROJECT SUMMARY

Project Code: 2024-0045619

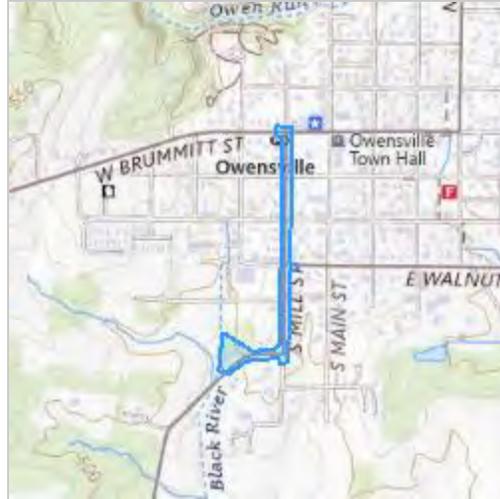
Project Name: SR 65, HMA Pavement Replacement, DES 2001922 and 2400688

Project Type: Road/Hwy - Maintenance/Modification

Project Description: The proposed state project is located on SR 65, from 0.26 mile south of SR 168 (S. Limits Owensville) to SR 165, in Gibson County, Indiana. The proposed project will have full depth pavement replacement on SR 65 from approximately 400 feet west of Mill Street to the intersection of SR 165 - under DES 2001922. The remaining south portion of the project limits will be milling and repaving 1.5 inches of surface. The full depth pavement replacement will include new curb and gutter, sidewalks, curb ramps, and an enclosed drainage system - under DES 2400688. A 10-foot parking lane will be added on both sides of SR 65 from Warrick Street to SR 168, with a curb and gutter separating the parking lane from the sidewalk. The curve design speed will be adjusted to 20 miles per hour (MPH) via roadway geometrics at Mill Street and SR 65. The design at SR 168 and SR 65 will include a 30-foot radii with 10:1 tapers and a two-foot offset. The storm sewer on SR 65 within the project limits will be redesigned to be enclosed and include replacement of the trunkline and the addition of inlets. A new detention pond is also proposed near the project's southern terminus on the west side of SR 65. There is suitable summer bat habitat within the project limits. Approximately 1.1 acres of trees will be cleared within 100 feet of the existing roadway in the southwest quadrant during the inactive season. Dominant species of trees to be removed include silver maple (*Acer saccharinum*) and eastern black walnut (*Juglans nigra*). INDOT personnel from the Vincennes District stated on January 16, 2024, that a review of the USFWS database did not indicate the presence of any endangered bat species in or within 0.5 mile of the project area. No permanent lighting changes are anticipated; however, temporary lighting changes are possible due to the potential for nighttime construction. No mitigation will be required. Construction is anticipated to begin in Fall of 2026.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.27093925,-87.69265883711087,14z>



Counties: Gibson County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> There is proposed critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/9743	Proposed Threatened

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and Golden Eagles are protected under the Bald and Golden Eagle Protection Act ² and the Migratory Bird Treaty Act (MBTA) ¹. Any person or organization who plans or conducts activities that may result in impacts to Bald or Golden Eagles, or their habitats, should follow appropriate regulations and consider implementing appropriate avoidance and minimization measures, as described in the various links on this page.

1. The [Bald and Golden Eagle Protection Act](#) of 1940.
2. The [Migratory Birds Treaty Act](#) of 1918.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

There are Bald Eagles and/or Golden Eagles in your [project](#) area.

Measures for Proactively Minimizing Eagle Impacts

For information on how to best avoid and minimize disturbance to nesting bald eagles, please review the [National Bald Eagle Management Guidelines](#). You may employ the timing and activity-specific distance recommendations in this document when designing your project/activity to avoid and minimize eagle impacts. For bald eagle information specific to Alaska, please refer to [Bald Eagle Nesting and Sensitivity to Human Activity](#).

The FWS does not currently have guidelines for avoiding and minimizing disturbance to nesting Golden Eagles. For site-specific recommendations regarding nesting Golden Eagles, please consult with the appropriate Regional [Migratory Bird Office](#) or [Ecological Services Field Office](#).

If disturbance or take of eagles cannot be avoided, an [incidental take permit](#) may be available to authorize any take that results from, but is not the purpose of, an otherwise lawful activity. For assistance making this determination for Bald Eagles, visit the [Do I Need A Permit Tool](#). For assistance making this determination for golden eagles, please consult with the appropriate Regional [Migratory Bird Office](#) or [Ecological Services Field Office](#).

Ensure Your Eagle List is Accurate and Complete

If your project area is in a poorly surveyed area in IPaC, your list may not be complete and you may need to rely on other resources to determine what species may be present (e.g. your local FWS field office, state surveys, your own surveys). Please review the [Supplemental Information on Migratory Birds and Eagles](#), to help you properly interpret the report for your specified location, including determining if there is sufficient data to ensure your list is accurate.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to bald or golden eagles on your list, see the "Probability of Presence Summary" below to see when these bald or golden eagles are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

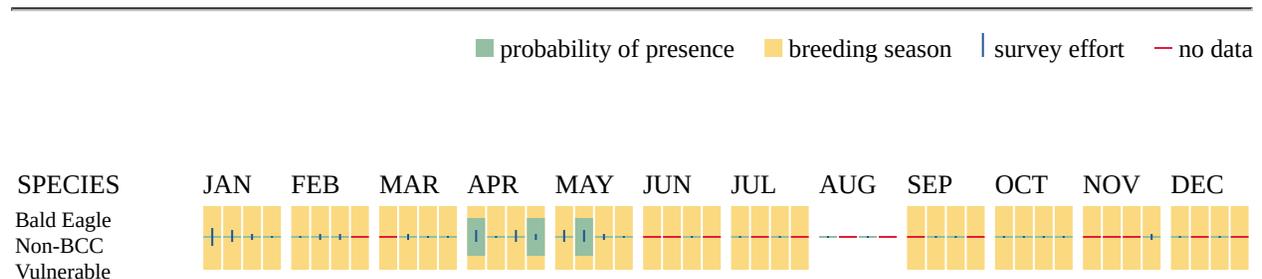
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide avoidance and minimization measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

MIGRATORY BIRDS

The Migratory Bird Treaty Act (MBTA) ¹ prohibits the take (including killing, capturing, selling, trading, and transport) of protected migratory bird species without prior authorization by the Department of Interior U.S. Fish and Wildlife Service (Service).

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the "Probability of Presence Summary" below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
<p>Bald Eagle <i>Haliaeetus leucocephalus</i></p> <p>This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.</p> <p>https://ecos.fws.gov/ecp/species/1626</p>	Breeds Sep 1 to Jul 31
<p>Chimney Swift <i>Chaetura pelagica</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p> <p>https://ecos.fws.gov/ecp/species/9406</p>	Breeds Mar 15 to Aug 25
<p>Field Sparrow <i>Spizella pusilla</i></p> <p>This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA</p> <p>https://ecos.fws.gov/ecp/species/9446</p>	Breeds Mar 1 to Aug 15
<p>Grasshopper Sparrow <i>Ammodramus savannarum perpallidus</i></p> <p>This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA</p> <p>https://ecos.fws.gov/ecp/species/8329</p>	Breeds Jun 1 to Aug 20
<p>Lesser Yellowlegs <i>Tringa flavipes</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p> <p>https://ecos.fws.gov/ecp/species/9679</p>	Breeds elsewhere
<p>Prothonotary Warbler <i>Protonotaria citrea</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p> <p>https://ecos.fws.gov/ecp/species/9439</p>	Breeds Apr 1 to Jul 31
<p>Red-headed Woodpecker <i>Melanerpes erythrocephalus</i></p> <p>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.</p> <p>https://ecos.fws.gov/ecp/species/9398</p>	Breeds May 10 to Sep 10
<p>Rusty Blackbird <i>Euphagus carolinus</i></p> <p>This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA</p> <p>https://ecos.fws.gov/ecp/species/9478</p>	Breeds elsewhere
<p>Semipalmated Sandpiper <i>Calidris pusilla</i></p> <p>This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA</p> <p>https://ecos.fws.gov/ecp/species/9603</p>	Breeds elsewhere

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

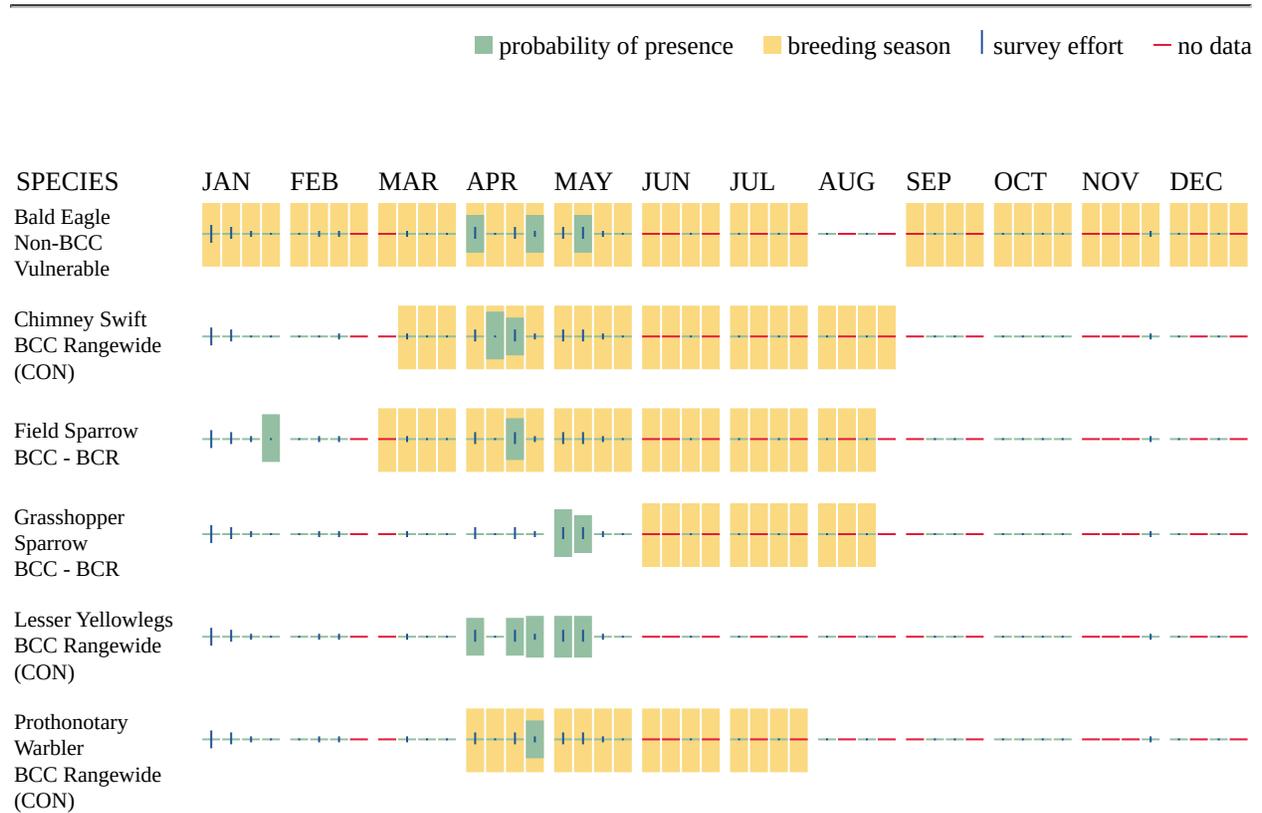
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

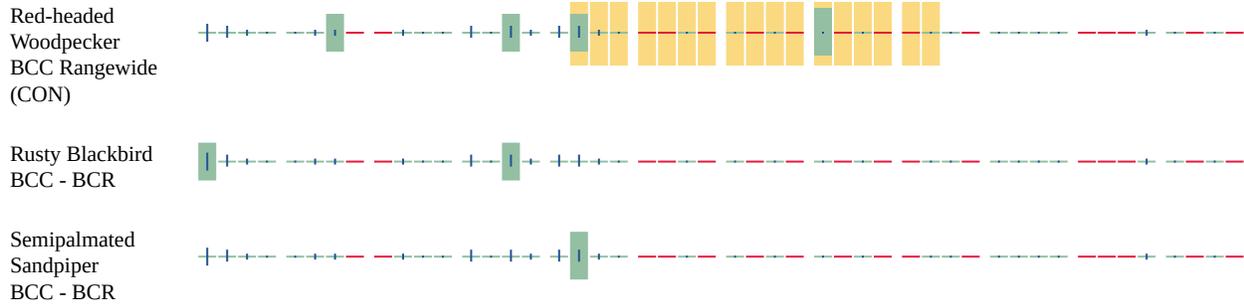
Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (-)

A week is marked as having no data if there were no survey events for that week.





Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide avoidance and minimization measures for birds
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

RIVERINE

- R4SBC

IPAC USER CONTACT INFORMATION

Agency: Kaskaskia Engineering Group, LLC
Name: Chad Kelly
Address: 477 South Third Street
Address Line 2: Suite 280
City: Geneva
State: IL
Zip: 60134
Email: ckelly@kaskaskiaeng.com
Phone: 6303329157

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration



United States Department of the Interior



FISH AND WILDLIFE SERVICE
 Indiana Ecological Services Field Office
 620 South Walker Street
 Bloomington, IN 47403-2121
 Phone: (812) 334-4261 Fax: (812) 334-4273

In Reply Refer To:

01/12/2026 15:09:10 UTC

Project code: 2024-0045619

Project Name: SR 65, HMA Pavement Replacement, DES 2001922 and 2400688

Subject: Not Likely to Adversely Affect Concurrence verification letter for the 'SR 65, HMA Pavement Replacement, DES 2001922 and 2400688' project under the December 13, 2024, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat, Northern Long-eared Bat, and Tricolored Bat.

To whom it may concern:

This letter records the determination of effects to federally listed (or proposed) bat species anticipated to result from the SR 65, HMA Pavement Replacement, DES 2001922 and 2400688 (the Project). This determination is based upon information you entered into the assisted determination key (Dkey) associated with the above referenced Programmatic Biological Opinion/Conference Opinion (PBO/PCO) in the U.S. Fish and Wildlife Service's (Service) Information for Planning and Consultation (IPaC) system on the date listed above to verify that the Project may rely on the concurrence provided in the PBO/PCO to satisfy requirements under section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (16 USC 1536), as amended.

Ensuring Accurate Determinations When Using IPaC:

The Service developed the IPaC system and this Dkey in accordance with the ESA and based on the PBO/PCO. All information submitted by the project proponent into IPaC must accurately represent the full scope and details of the Project.

Failure to accurately represent or implement the Project as detailed in the Dkey invalidates this letter. Answers to certain questions in the Dkey commit the project proponent to implementation of conservation measures that must be followed for the ESA determinations to remain valid. Carefully review this letter, your ESA requirements are NOT yet complete.

Determinations:

Based on the information you provided (Project Description shown below), you have determined that the Project is within the scope and adheres to the criteria of the PBO/PCO, including the adoption of applicable avoidance and minimization measures. Based on your IPaC submission and the PBO/PCO, the Project is consistent with the following effect determinations:

Species	Listing Status	Determination
Indiana Bat (<i>Myotis sodalis</i>)	Endangered	NLAA

The tricolored bat is proposed for listing as endangered under the ESA, but not yet listed. For actions that may affect a proposed species, agencies cannot consult, but they can confer under the authority of section 7(a)(4) of the ESA. Such conferences can follow the procedures for a consultation and be adopted as such if the proposed species is listed. Should the tricolored bat be listed, agencies must review projects that are not yet complete, or projects with ongoing effects within the tricolored bat range that previously received a no effect or not likely to adversely affect (NLAA) determination from the key to confirm that the determination is still accurate.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Project does not meet the criteria for a NLAA determination under the PBO/PCO. **If the Service does not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Project under the terms of the NLAA concurrence provided in the PBO/PCO.** This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO/PCO.

If the Project is modified, or new information reveals that it may affect the Indiana bat, northern long-eared bat, or tricolored bat in a manner or to an extent not considered in the PBO/PCO, further review to conclude the requirements of ESA section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities:

If your initial bridge, culvert, or structure assessment failed to detect Indiana bat, northern long-eared bat, or tricolored bat use or occupancy, yet bats are later detected prior to, or during construction, promptly notify the local Service Field Office within 2 working days of the discovery. In addition, please document whether incidental take occurred, and if so, the type (i.e. kill or harm) and amount (i.e. number of individuals) and submit documentation to the local Service Field Office within 5 working days from the completion of the bridge, culvert, or structure construction (use Appendix E - Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form in the [User's Guide](#)). In these instances, potential incidental take of Indiana bats, northern long-eared bats, or tricolored bats may be exempted provided that the take is reported to the Service. In these instances, potential incidental take of Indiana bats, northern long-eared bats, or tricolored bats may be exempted provided that the take is reported to the Service.

If the Project may affect any other federally listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Field Office is required for those species/designated critical habitat. If the Project has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please advise the lead Federal action agency to contact this Service Field Office

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Proposed Threatened
- Whooping Crane *Grus americana* Experimental Population, Non-Essential

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

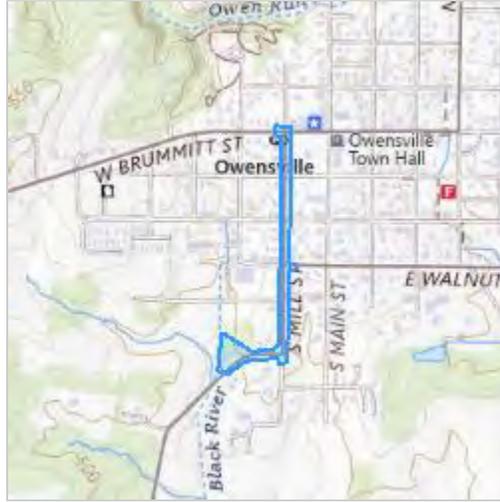
NAME

SR 65, HMA Pavement Replacement, DES 2001922 and 2400688

DESCRIPTION

The proposed state project is located on SR 65, from 0.26 mile south of SR 168 (S. Limits Owensville) to SR 165, in Gibson County, Indiana. The proposed project will have full depth pavement replacement on SR 65 from approximately 400 feet west of Mill Street to the intersection of SR 165 - under DES 2001922. The remaining south portion of the project limits will be milling and repaving 1.5 inches of surface. The full depth pavement replacement will include new curb and gutter, sidewalks, curb ramps, and an enclosed drainage system - under DES 2400688. A 10-foot parking lane will be added on both sides of SR 65 from Warrick Street to SR 168, with a curb and gutter separating the parking lane from the sidewalk. The curve design speed will be adjusted to 20 miles per hour (MPH) via roadway geometrics at Mill Street and SR 65. The design at SR 168 and SR 65 will include a 30-foot radii with 10:1 tapers and a two-foot offset. The storm sewer on SR 65 within the project limits will be redesigned to be enclosed and include replacement of the trunkline and the addition of inlets. A new detention pond is also proposed near the project's southern terminus on the west side of SR 65. There is suitable summer bat habitat within the project limits. Approximately 1.1 acres of trees will be cleared within 100 feet of the existing roadway in the southwest quadrant during the inactive season. Dominant species of trees to be removed include silver maple (*Acer saccharinum*) and eastern black walnut (*Juglans nigra*). INDOT personnel from the Vincennes District stated on January 16, 2024, that a review of the USFWS database did not indicate the presence of any endangered bat species in or within 0.5 mile of the project area. No permanent lighting changes are anticipated; however, temporary lighting changes are possible due to the potential for nighttime construction. No mitigation will be required. Construction is anticipated to begin in Fall of 2026.

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.27093925,-87.69265883711087,14z>



DETERMINATION KEY RESULT

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the Indiana bat, northern long-eared bat or tricolored bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 et seq.) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the Programmatic Biological Opinion/Conference Opinion for Transportation Projects in the Range of the Indiana bat, northern long-eared bat, and tricolored bat, dated December 13, 2024.

QUALIFICATION INTERVIEW

1. Which Federal Agency is the lead federal agency the action?

A) Federal Highway Administration (FHWA)

2. Does the Action Area intersect the species list area of the Indiana bat?

Automatically answered

Yes

3. Is the project within 0.5 miles radius of an entrance/opening to any known Indiana bat hibernaculum?

No

4. [Semantic] Is any portion of the action area within a 0.5 mile radius of an entrance/opening to any known NLEB or TCB hibernacula?

Note: The map queried for this question contains proprietary information and cannot be displayed. If you need additional information, please contact the Field Office listed in the letterhead of this letter.

Automatically answered

No

5. Does your project's activities include raising the road profile above the tree canopy in documented habitat for the Indiana bat, NLEB, or TCB?

Note: For the definition of documented habitat, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>

No

6. Is your project located within a karst area?

No

7. Will the project include bridge, culvert, or structure removal, replacement, and/or alteration activities?

Note: For definitions of bridge, culvert, and structure, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>.

No

8. Do your project's activities involve tree removal/trimming, temporary lighting, new/additional permanent lighting, ground disturbance, percussives that involves noise/vibration above existing background levels, vibrations, or slash pile burning?

Yes

9. Is there suitable summer habitat for the Indiana bat, NLEB, or TCB within the project action area?

Note: See the Service's summer survey guidance for current definitions of suitable habitat [<https://www.fws.gov/midwest/endangered/mammals/inba/inbasummersurveyguidance.html>].

Yes

10. Have P/A surveys for the Indiana bat, NLEB, or TCB been conducted within the suitable summer habitat located within your project action area? This refers to mist-netting or acoustic surveys, not bridge assessments.

Note: See the Service's survey guidance <https://www.fws.gov/midwest/endangered/mammals/inba/inbasummersurveyguidance.html>

No

11. Will the project involve the removal or trimming of trees within suitable habitat for the Indiana bat, NLEB, or TCB?

Yes

12. Will any tree removal or trimming occur during the bat pup season?

Note: For more information about bat pup seasons please visit https://www.fws.gov/sites/default/files/documents/2024-10/2024_usfws_rangewide_ibat-nleb_survey_guidelines.pdf

No

13. Will the removal or trimming of trees occur **within documented habitat** for the Indiana bat, NLEB, or TCB?

Note: For the definition of documented habitat, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>

No

14. Will all tree removal or trimming occur within 100 feet of the road or rail surface?

Yes

15. Does your project include activities involving the temporary or permanent exclusion of Indiana bats, NLEBs, or TCBs from a bridge/culvert or structure that is occupied by a large number of covered bats (>5)?

Note: exclusion is conducted to deny bats' entry or reentry into a bridge/culvert or structure. To be effective and to avoid harming bats, it should be done according to established standards. Coordinate with the local Service Field Office for additional assistance.

No

16. Does your project involve the use of temporary lighting within Indiana bat, NLEB, or TCB suitable habitat?

Note: For the definition of lighting, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>

Yes

17. Will the use of temporary lighting be conducted during the Indiana bat, NLEB, or TCB active season?

Note: For the definition of active season, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>

Yes

18. Will temporary lighting be directed away from Indiana bat, NLEB, or TCB suitable habitat?

Yes

19. Will the project substantially increase baseline light conditions via the use of permanent lighting (replacement or new/additional) in suitable habitat.

No

20. Will your project include percussive activities?

Note: Refer to Stressor #2 Noise/Vibration on page 109 of the PBO/PCO.

No

21. Will the project involve the removal or trimming of more than 20 acres of Indiana bat, NLEB, or TCB suitable habitat per 5-mile section of road/rail?

No

22. Will the removal or trimming of trees occur within 0.5 miles of a known Indiana bat, NLEB, or TCB hibernaculum?

No

23. Will the removal or trimming of these trees occur during the active season?

Note: For the definition of active season, refer to Appendix A: <https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat>

No

24. Will the removal or trimming of trees occur **beyond 100 feet** of the existing road/rail surfaces?

No

25. Does the Action Area intersect the species list area of the Indiana bat?

Automatically answered

Yes

26. Does the Action Area intersect the species list area of the tricolored Bat (TCB)?

Automatically answered

No

PROJECT QUESTIONNAIRE

1. How many acres of trees are proposed for removal/trimming **outside of documented habitat** for the Indiana bat, NLEB, or TCB within 100 feet of the existing road/rail surfaces during the active season (**outside the pup season and not between Dec 15th-Feb 15th in Zone 1 of the NLEB and TCB YR active areas**) (LAA)?

Note: If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

1.1

2. How many acres of trees are proposed for removal/trimming **outside of documented habitat** for the Indiana bat, NLEB, or TCB within 100 feet of the existing road/rail surfaces during the pup season (**trees must be <9 in DBH, and not between Dec 15th-Feb 15th in Zone 1 of the NLEB and TCB YR active areas**) (LAA)?

Note: If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0

3. How many acres of trees are proposed for removal/trimming **either outside or within documented habitat** for the Indiana bat, NLEB, or TCB between 100-300 feet of the existing road/rail surfaces during the active season (**outside the pup season, and not between Dec 15-Feb 15 in Zone 1 of the NLEB and TCB YR active areas**) (LAA)?

Note: If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0

4. How many acres of trees are proposed for removal/trimming **either outside or within documented habitat** for the Indiana bat, NLEB, or TCB beyond 300 feet of the existing road/rail surfaces during the active season (**outside the pup season, and not between Dec 15th-Feb 15th in Zone 1 of the NLEB and TCB YR active areas**) (LAA)?

Note: If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0

AVOIDANCE AND MINIMIZATION MEASURES (AMMS)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GAMM1

Ensure all operators, employees, and contractors working in areas of Indiana bat, NLEB, or TCB suitable habitat are aware of all Transportation Agency environmental commitments, including all applicable AMMs.

LAMM1

Direct temporary lighting away from suitable habitat during the active season

TRTAMM1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to the extent practicable to avoid tree removal/trimming in excess of what is required to implement the project safely.

TRTAMM2

Ensure tree removal/trimming is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree removal/trimming to ensure contractors stay within clearing limits

TRTAMM3

Ensure tree removal/trimming is limited to the inactive season, occurs within 100 ft of the road/rail surface, and is outside of documented habitat for the Indiana bat, NLEB, and TCB

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING IBAT, NLEB, OR TCB

This key was last updated in IPaC on January 06, 2026. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) and may affect the federally listed endangered Indiana bat (*Myotis sodalis*), northern long-eared bat (*Myotis septentrionalis*), and/or federally proposed endangered tricolored bat (*Perimyotis subflavus*).

This decision key should only be used to verify project applicability with the Service's Programmatic Biological Opinion/Conference Opinion for Transportation Projects in the Range of the Indiana bat, northern long-eared bat, and tricolored bat, dated December 13, 2024. The programmatic consultation limited transportation activities that may affect the covered bat species and addresses situations that are both likely and not likely to adversely affect the covered bat species. This decision key will assist in identifying the effect of a specific project/activity and the applicability of the programmatic consultation. The programmatic consultation is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic consultation, or that may affect ESA-listed species other than the Indiana bat, northern long-eared bat, or tricolored bat, or their designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

Agency: Indiana Department of Transportation

Name: Ryan Falls

Address: 3650 South U.S. Highway 41

City: Vincennes

State: IN

Zip: 47591

Email: rfalls@indot.in.gov

Phone: 8125821387

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Federal Highway Administration